

**JOINTLY OWNED
BOULDER COUNTY-LAFAYETTE OPEN SPACE
MANAGEMENT PLAN**

covering the following properties:

“TWO CREEKS” OPEN SPACE

(Armstrong, Flagg Park, Haselwood, Lafayette Buffer Parcel,
Madrigal, McClain, Serrano, Stephenson-Nelson, and Mountain View Egg Farm)
and

WAREMBOURG-LAFAYETTE FARM



Further Amended:

Boulder County, County Commissioners – July 26, 2010

City of Lafayette, City Council – July 20, 2010

As Amended by:

Boulder County, County Commissioners - February 27, 2007

City of Lafayette, City Council - February 20, 2007

Originally Adopted by:

Boulder County, County Commissioners - December 7, 2004

City of Lafayette, City Council - November 16, 2004

Boulder County Parks and Open Space

Mission Statement

To conserve natural, cultural and agricultural resources and provide public uses that reflect sound resource management and community values.

Vision Statement

Mountain vistas, golden plains, scenic trails, diverse habitats, rich heritage...a landscape that ensures an exceptional quality of life for all.

City of Lafayette

Vision Statement

Lafayette's panoramic view of the Rocky Mountains inspires our view into the future. We value our heritage, our unique neighborhoods, a vibrant economy and active life-styles. We envision a future that mixes small town livability with balanced growth and superior technologies.

City of Lafayette Parks, Open Space and Golf

Vision Statement

The City of Lafayette's open space and trails system provides a balanced network of open lands, natural areas, wildlife corridors and habitat areas, view corridors, and greenways that preserves the City's natural, aesthetic, and community character and provides connections between neighborhoods, the natural environment, and community amenities in a manner that complements the policy and land use guidance of the City's Comprehensive Plan.

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ACRONYMNS and REFERENCES

“City” – The City of Lafayette

“County” - Boulder County Parks and Open Space

“Plan” – Jointly Owned Boulder County - Lafayette Open Space Management Plan

CDOW – Colorado Department of Wildlife

ESA – Endangered Species Act

LWCF – Land and Water Conservation Fund

MOA – Multiple Objective Areas (Prairie Dog Management)

NPD – No Prairie Dog area (Prairie Dog Management)

NRHP – National Register of Historic Places

NSH - Non-suitable Habitat (Prairie Dog Management)

OAHP – Office of Archeology and Historical Preservations

ROW – Right of Way

UDFCD – Urban Drainage and Flood Control

USFWS – United States Fish and Wildlife Service

SUMMARY

Totaling some 540 acres along Coal Creek in southeastern Boulder County, the properties jointly owned by Boulder County and the City of Lafayette provide scenic vistas and urban buffers, preserve riparian resources and wildlife habitat, provide recreation opportunities, and ensure the continuation of agriculture in the local area. One major natural feature defines the properties and ties them all together - Coal Creek.

The Two Creeks Open Space is made up of nine properties (Armstrong, Haselwood, Flagg Park, Lafayette Buffer Parcel, Madrigal, McClain, Serrano, Stephenson-Nelson, and Mountain View Egg Farm) and are managed by Boulder County Parks & Open Space (“County”). The City participates in the planning and funding of resource management projects on these properties. The other jointly owned property (Warembourg-Lafayette Farm) is under an agricultural lease managed by the County. Private farm operators use this property to produce agricultural goods and also perform much of the land management work.

Past resource management activities have consisted mostly of grassland restoration on Haselwood, Armstrong and Serrano. Weed control, prescribed fire, and native grass plantings have been utilized. Prairie dog management has also been a component of this restoration process.

The future management emphasis in the Two Creeks area will be on riparian and grassland restoration, while pursuing implementation of the Coal and Rock Creek Trails. The Trails will meander through the open space on its way north to Baseline Road/State Highway 7. The management focus of the Warembourg-Lafayette Farm property will be on agricultural improvements and riparian enhancements.

By maintaining positive relationships and embracing a good neighbor policy, the joint owners seek to preserve and enhance the conservation values of the area.

1. INTRODUCTION

The City of Lafayette (“City”) and Boulder County Parks and Open Space (“County”) have been acquiring lands along Coal Creek as open space since 1996 (*Figure 1*). Preservation of these lands provides relief among growing municipalities, protects significant wildlife habitat, preserves the rural character and agricultural lifestyle of the area, and provides opportunities for regional trail development.

The ten properties outlined in this plan were acquired with combinations of open space sales tax and general funds from the City and the County. All of the properties are jointly owned, with the exception of three: Flagg Park, Lafayette Buffer Parcel and 7 acres considered part of the Stephenson-Nelson property. The Flagg Park property is owned fee-simple by the City of Lafayette and leased to Boulder County; the Lafayette Buffer Parcel and the 7-acre part of Stephenson-Nelson are owned fee-simple by Boulder County. The management of these three parcels is included in this plan.

The City and County entered into a partnership to protect and permanently steward these important lands for current and future generations. To this end, we have undertaken the mandate of developing a management plan to aid in making decisions and guide property management over time. We wish to thank the many partners involved in protecting and planning for these properties, including the Haselwood, Serrano, and Warembourg Families, the Urban Drainage and Flood Control District (UDFCD), the City of Lafayette and its citizens, and all citizens of Boulder County.

Purpose of the Plan

The purpose of the Jointly Owned Boulder County-Lafayette Open Space Management Plan (“Plan”) is to provide management direction for natural, cultural, agricultural, and passive recreational resources that occur on properties owned jointly by Boulder County and the City of Lafayette. Specifically, the Plan aims to develop short, mid, and long-term actions that will improve and enhance resource quality. This Plan is based on the best available information and provides a foundation for long-term adaptive management of the respective properties and their resources. Completion and implementation of this Plan will ensure that the County and City meet the promises and challenges of their open space legacy.

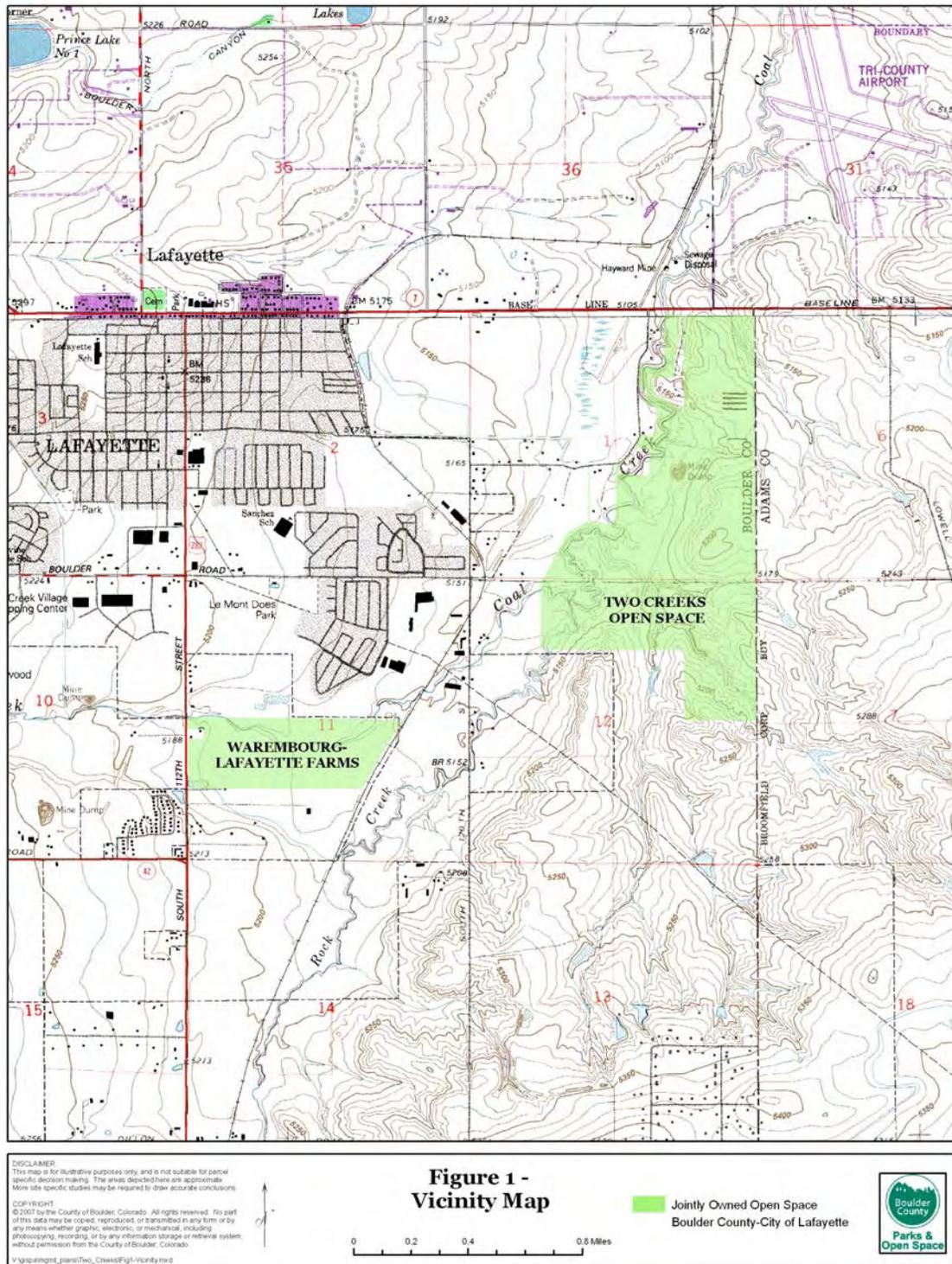
Report Organization

The Plan is organized into three major parts- *Introduction, Existing Conditions, and Management Direction*. The *Introduction* contains important background information and key information on the conservation values and goals of the project area. *Existing Conditions* contains property descriptions and resource evaluations. This section serves as the baseline inventory for the project area. The *Management Direction* section contains the resource management objectives and recommended action items that should be implemented.

How to Use the Plan

The Jointly Owned Boulder County-Lafayette Open Space Management Plan is a working document, which should change and evolve with the properties. As the City and County implement recommended actions, and as objectives and goals change, the Plan should reflect

Figure 1: Vicinity Map



those changes. At a minimum, the Plan should be reviewed and evaluated every fifteen to twenty years. It is entirely possible that new lands could be acquired or major changes take place that trigger the need for more frequent reviews and updates.

The Plan should be used to:

1. Understand the overall goals for the Coal Creek corridor and ensure that all actions support those goals.
2. Understand the specific resources on jointly owned properties within the Coal Creek corridor (refer to Section 2, *Existing Conditions*, for a summary of resource information).
3. Establish management direction and develop priorities (refer to Section 3, *Management Direction*, for recommended actions and implementation information).
4. Monitor the status of resources within the corridor and evaluate management success.

1.1. AREA DESCRIPTION

1.1.1. Location and Background

Two Creeks Open Space

The Two Creeks Open Space is located in southeastern Boulder County, Colorado, along Coal Creek near the city of Lafayette (*Figure 2*). The Two Creeks Open Space contains nine properties located in T1S R69W, Sections 1, 11, and 12:

- a 31-acre parcel referred to as the Armstrong Property,
- a 4.5-acre parcel referred to as the Flagg Park Property,
- a 59-acre parcel referred to as the Haselwood Property,
- a 3.75-acre parcel referred to as the Lafayette Buffer Parcel,
- a 1.5-acre parcel referred to as the Madrigal Property,
- a 2-acre parcel referred to as the McClain Property,
- a 20-acre parcel referred to as the Serrano Property,
- a 165.768-acre parcel referred to as the Stephenson-Nelson Property, and
- a 141.8-acre parcel referred to as the Mountain View Egg Farm Property.

Warembourg-Lafayette Farm

Lafayette and Boulder County also jointly own the 110-acre parcel referred to as the Warembourg-Lafayette Farm Property, located southwest of Two Creeks Open Space (*Figure 2*). This property is east of US Highway 287, just southeast of the South Public Road trailhead in Lafayette.

1.1.2. Landscape Setting and Physical Characteristics

Climate

With an average elevation of 5,236 feet, the climate of the Lafayette area can be described as high plains, continental climate, with light rainfall and low humidity. The climate is modified considerably from that expected of a typical high plains environment because of the nearby mountains. Winds are channeled from the Continental Divide down the Front Range and can be severe. Prevailing winds are generally from the west.

The average high temperature in July is 88°F, and the average low temperature in January is 14°F (Weatherbase 2002). Annual precipitation averages 16 inches. Relative humidity is about

30-35% in summer and about 40-50% in winter. Periods of drought are frequent, usually occurring in the fall and winter. The length of the growing season is approximately 140 days, with the average date of the first killing frost being September 28. The last killing frost occurs around May 11 (USDA 1975).

Topography

The area lies within the plains life-zone, about ten miles east of the Front Range of the Southern Rocky Mountains. The site topography is characterized by generally flat lands with some gently rolling terrain trending toward Coal Creek. Elevations of the subject properties range from about 5,100 to 5,240 feet.

Geology

The subject properties lie on the western edge of the Colorado Piedmont section of the Great Plains physiographic province. The area consists of northeast-trending mesas with a local relief of 100 to 300 feet. In general terms, the regional geology consists of sedimentary rocks of Late Cretaceous age including sandstones, claystones, and shales of the Laramie Formation (Spencer 1961).

The bedrock is overlain by alluvial, eolian, and colluvial (water, wind and gravity-carried) material deposited during the Quaternary Period. Bedrock generally dips gently to the east and is traversed by a series of northwest/southeast trending, high angle (near vertical) faults. The faults also lend themselves as channels for the many intermittent streams in the area.

Holocene and Pleistocene eolian deposits of well-sorted, fine to medium-grain sands and silts, usually less than 5 feet thick predominate the surrounding area. Near-surface eolium commonly forms a cap over older alluvial deposits. Together, the eolian and alluvial deposits form small terraces along the drainages, ranging in height from 5 to 8 feet above the creek bed. The Cretaceous Laramie and Upper Fox Hill Formations lie unconformably under the eolian and alluvial deposits. The Laramie Formation is an olive-gray to dark grayish-brown shale, siltstone, lignitic claystone and coal, which is interbedded with light-gray to light-brown sandstone. The upper member of the Fox Hills Formation is a massive light-gray to light-brown, crossed-bedded sandstone with mudstone partings and some locally thin coal seams.

The area along Coal Creek located in Section 1 is classified as a major geologic hazard area (Boulder County 1999). Potential problems include subsidence (abandoned coal mines), expansive soil or claystone, flooding, and landslides.

The subject properties lie within the Boulder-Weld Coal Field, which extends roughly from Marshall in Boulder County to Firestone in Weld County. Coal is found in roughly five zones in the lower portion of the Laramie Formation. Coal in this area is largely sub bituminous B (Spencer 1961). These coal deposits have low importance in the current National Coal Resource Assessment and it is unlikely that they will be utilized within the next 20-30 years because they are of lower quality than other coal available in nearby areas. Oil and gas resources are prevalent in the area as well.

Hydrology

All of the subject properties are located within the Coal Creek watershed. Coal Creek reaches its confluence with Rock Creek on the Stephenson-Nelson parcel. Coal Creek accounts for the majority of open water on the properties; however, the Warembourg-Lafayette Farm property contains irrigation ditches and a stock pond, Stephenson-Nelson contains a segment of Rock Creek and three other drainages, and the Mountain View Egg Farm (the Egg Farm) contains a small drainage.

In general, the direction of ground water flow on the subject properties is towards Coal Creek. Groundwater depth and flow direction is variable and is influenced by the time of year, the presence or absence of irrigation, and soil and aquifer characteristics. Depth to groundwater in the unconsolidated alluvium is estimated to be 10 to 20 feet (Hillier 1983). The bedrock aquifer consists of the sandstones of the Laramie Formation. Abandoned coal mine shafts under the area may also serve as conduits for groundwater flow.

Wetlands on the subject properties are generally associated with Coal Creek and consist of riparian vegetation supported by Creek flows. The Haselwood property contains a larger wetland that is described later in more detail. There are two permitted water wells on the Stephenson-Nelson property (*Figure 3*). The Egg Farm contains two small possible wetlands and a permitted well and cistern (*Figure 3*). Hydrologic features, including surface waters and wetlands in Two Creeks Open Space are depicted on *Figure 3*. Ditches and laterals on the Warembourg-Lafayette Farm property are identified on *Figure 4*.

1.1.3. Historic Ecology

During pre-settlement time, the wildlife of the area was characteristic of the faunal assemblage that extended over a vast expanse known as the High Plains Section of the North Temperate Biome. Shortgrass prairie dominated the region and probably supported bison during some seasons of the year. Grasslands are noted for their large numbers of plant eating animals, or herbivores, which include many insect, bird, and mammal species. As the urban area along the Front Range has grown, wildlife habitat and many wildlife species have been displaced.

The upland portions of the properties were once dominated by short and mixed grass prairie, which is characterized by species such as blue grama (*Condrosium gracile*), buffalograss (*Buchloe dactyloides*), sideoats grama (*Bouteloua curtipendula*), western wheatgrass (*Pascopyrum smithii*), little bluestem (*Schizachyrium scoparium*) and yucca (*Yucca glauca*). The historic fire interval for shortgrass prairie is about every eight to ten years; however, ranchers typically set fires in the grasslands more frequently to clear the area for improved cattle grazing. Remnant areas of this historic plains plant community still persist in surrounding areas. The majority of this vegetation community within Boulder County has been lost due to conversion of land for agriculture and urbanization.

The historic Coal Creek riparian vegetation community probably looked similar to what it does today, with perhaps a more robust shrub component and fewer weedy species.

Figure 3: Two Creeks Open Space – Infrastructure Map

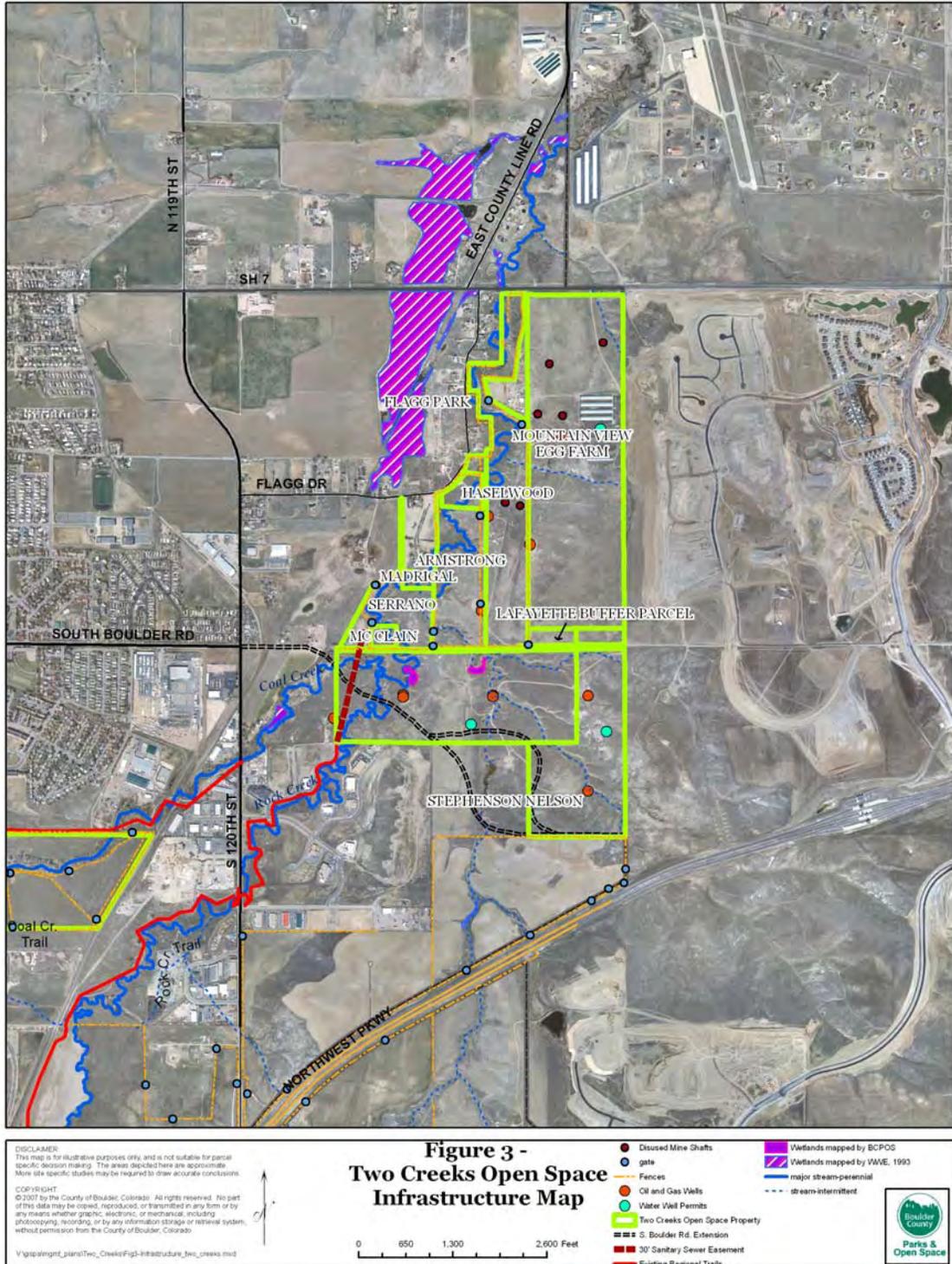
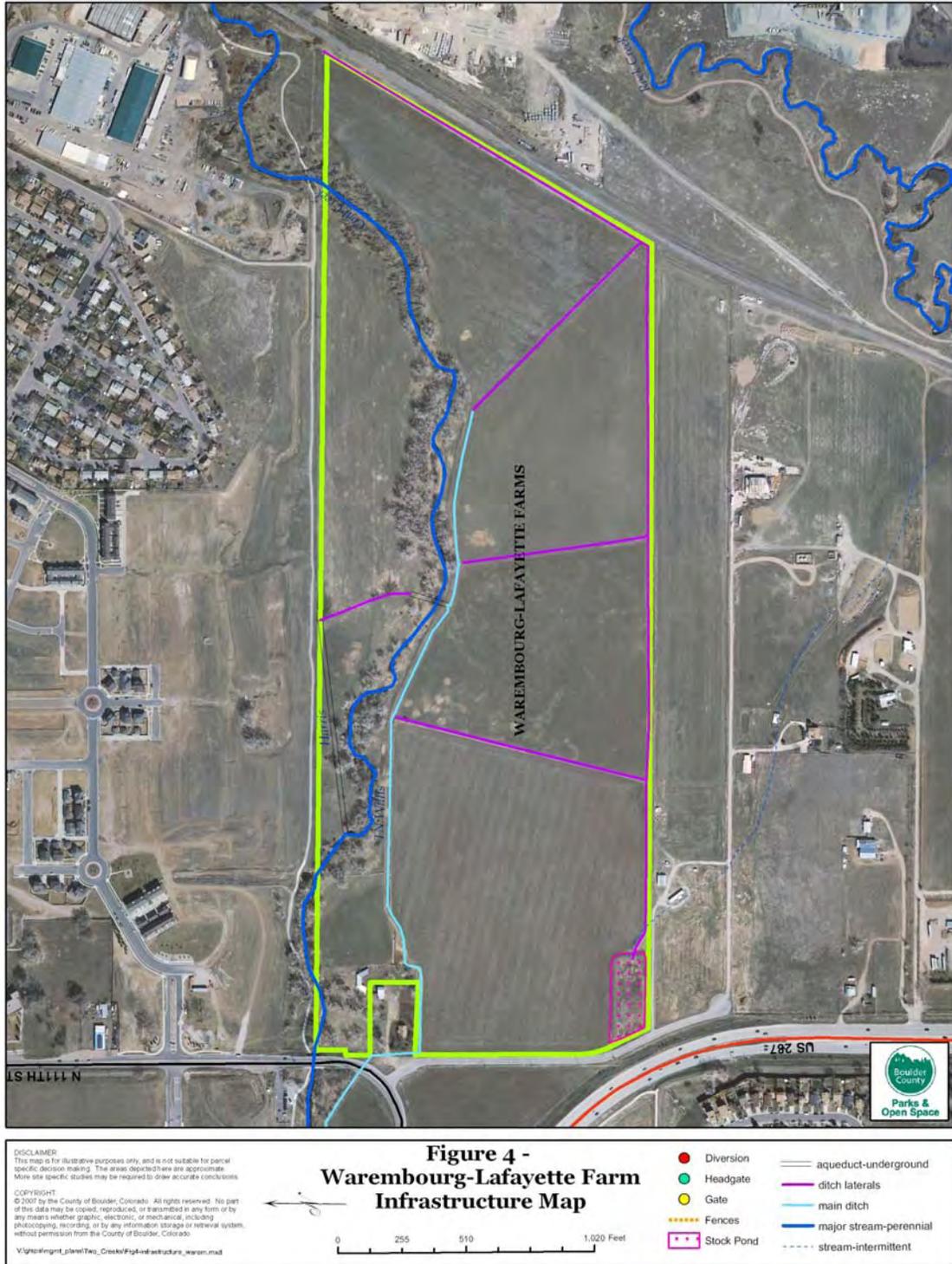


Figure 4: Warembourg-Lafayette Farm – Infrastructure Map



Conservation Values and Management Needs

Conservation values are those elements within the corridor and on the subject properties that represent the importance of resource preservation and sound management. Conservation values in the Coal Creek corridor and on the subject properties include the following:

- Scenic lands and open space
- Wildlife and wildlife habitat
- Significant plant communities, native riparian vegetation, and wetlands
- Water
- Education and interpretation opportunities
- Agricultural lands
- Cultural resources

In order to adequately preserve and enhance these conservation values, the following management needs were documented:

- Wildlife management
- Riparian restoration
- Upland restoration
- Weed control
- Cropland/livestock management
- Analysis/development of appropriate recreation opportunities

Evaluating management needs required for support of the conservation values is essential. Threats cannot be abated and opportunities capitalized upon unless needs are met. Meeting these needs is a formidable challenge and requires planning and diligence. Several issues that need to be considered for successful implementation are funding, landowner and agricultural lessee relations, law enforcement, and the constant interaction of balancing recreation, agriculture, and wildlife habitat.

1.1.4. Community Values and Priorities

Situated in unincorporated Boulder County, the subject properties are located to the east of the annexed boundary of Lafayette. The area's natural beauty and growing economy continue to attract new residents each year. According to the Colorado Department of Local Affairs, the population of Boulder County grew from 225,339 to 291,288 between 1990 and 2000, a 29 percent change. Lafayette also grew rapidly during this time period. Lafayette's 2002 population was 24,957 (City of Lafayette 2003a). The City's projected population at build-out is 33,450. The estimated population for Boulder County in 2025 is 415,323, almost double what it was in 1990. This population growth poses a threat to the natural environment.

The City and County have been actively acquiring and protecting lands around Lafayette and along Coal Creek for nearly a decade. The City and County seek to balance natural resource protection, recreational access, and agriculture in the corridor..

Three independent opinion surveys conducted for Boulder County provide some insight into community preferences about open space. According to the National Research Center (2002), 92 percent of respondents felt protecting wildlife habitat was very or fairly important; 85 percent

supported protecting farms and ranches; and 76 percent support nature study. Sixty-eight percent agree mildly or strongly with the existing County policy requiring dogs to be on leash when on open space property. Eighty-one percent support to some degree the use of herbicides on open space property for controlling noxious weeds.

A survey conducted by the Public Information Corporation (2002) showed that 93 percent of respondents said that protecting habitat for wildlife was fairly or very important. The next most important activities were hiking (91 percent stated it was fairly or very important) and preserving agricultural lands (89 percent felt it was at least fairly important). Horseback riding was shown to be least important with only 51 percent indicating this activity was at least fairly important.

In a 2010 Survey of Boulder County residents by Talmey-Drake Research and Strategy, Inc. found that 92% of respondents still found protecting habitat for wildlife was important. Providing trails was at least “somewhat important” to 86 percent of respondents. In terms of popular activities on open space, 89 percent said hiking was either very or fairly important.

A survey conducted for the City of Lafayette regarding open space showed that 63 percent of respondents preferred that open space land be kept natural, 20 percent indicated that some land should be used for more active uses, and 15 percent preferred a mixture of both. The City’s Master Plan Update (2003) identified the need to balance open space preservation and development of active parkland. The update also targeted the key issue of establishing connections to create a trails system, rather than many disconnected trail segments.

The Master Plan update supports these community preferences by protecting and enhancing significant resources within the corridor and providing compatible opportunities for recreation and environmental education. This foundation gave support to Lafayette completing the Open Space and Trails Master Plan in 2005, which provides the City with more specific guidance on future open space and trail needs and implementation strategies.

1.1.5. Vision, Goals and Management Protocols

Establishing a vision and goals for the Coal Creek corridor helps to provide a philosophical foundation on which to base the Plan. Goals are centered around five main resource topics and provide the basis for management actions related to resource issues and objectives. The vision and goals for the Coal Creek corridor are as follows:

Vision Statement

The Coal Creek corridor contains natural, cultural, and agricultural resources that provide unique opportunities for conservation and recreation. Through adaptive resource management, restoration, and appropriate recreational use, the jointly owned lands are an asset that will serve Lafayette and Boulder County residents well into the future.

Goals

- Vegetation- Protect and enhance native vegetation and restore degraded plant communities.
- Wildlife- Protect and enhance wildlife habitat and movement corridors.
- Cultural- Protect significant cultural resources and provide compatible interpretive opportunities.
- Agricultural- Preserve agricultural resources and practices where appropriate to ensure the legacy and rural character of the corridor.

- Visitor Use- Provide educational, interpretive, and passive recreational opportunities that are compatible with the conservation values of the corridor.

1.1.6. Management Protocol and Cost Share

Boulder County will continue to manage the subject properties in accordance with the guidance provided in this document. Routine management expenses and agricultural lease revenues shall continue to be the responsibility of the County. Non-routine costs and major capital expenditures shall be shared equally between the City and County, including, but not limited to, extraordinary prairie dog management efforts, recreational facility development, irrigation conveyance system improvements, and agricultural improvements. The City shall be responsible for all management (and related expenses) associated with prairie dog relocation efforts. The parties shall enter into an agreement reflecting these issues so that the language between the management plan and the existing reciprocal conservation easement(s) is consistent and conforming (*Appendix 3*).

Annual meetings should be held between the two agencies in order to discuss property improvements and needs and plan for any expenditures or other assistance that may be required in the following fiscal year. These meetings should occur prior to May 1 of each year (or another mutually agreeable schedule). Regular communication on issues that may have financial implications is important.

Water

Water rights on the Warembourg-Lafayette Farm are jointly owned and attached to the property. Any transfer of jointly owned water rights related to the properties addressed in this Plan shall require the consent of the City and the County and shall seek to enhance irrigation on prime agricultural lands, or contribute to dedicated in-stream flows. Payment of annual assessments and voting rights are described within Section 2.1, Property Inventory and Description.

Water rights on the Egg Farm are jointly owned and attached to the property. The water rights specific to the Egg Farm are groundwater rights. These rights are proposed for use on the property to support agricultural efforts. Any proposal to alter or move these rights will require the consent of both the City and the County.

According to the agreement with the UDFCD that applies to the Armstrong, Haselwood, McClain, Serrano, Stephenson-Nelson and Warembourg-Lafayette Farm properties, neither the City nor the County may dispose of or change the use of their interests in the floodplain portions of these properties without approval of the UDFCD.

Public Works

Many of the properties have existing easements or encumbrances that may affect future management activities. Rights-of-way for utilities, pipelines, ditches and laterals, and roads and railroads are common. An issue of particular note is the future extension of South Boulder Road. The City of Lafayette and City/County of Broomfield plan to extend this 4-lane road to the east. Transportation planning documents for this area identify the future road as a major arterial that will eventually serve the Anthem Development, a planned residential development in Broomfield.

In October 1997, the Board of County Commissioners expressed their concern about the road expansion and suggested that the road alignment be moved south of the confluence in order to protect natural resources and create a community buffer (Stewart 1997). In the Stephenson-Nelson purchase agreement between Lafayette and Boulder County, two alternatives for the South Boulder Road alignment were included as an exhibit. As a part of the agreement, the road must cross Coal and Rock Creeks above (west of) their confluence. The alternative alignments would pass through the southern boundary of the Stephenson-Nelson property (*Figure 3*).

No target date has been set for construction of the road extension. The trigger for additional planning and construction is when the Cities have the financial capabilities to construct the project. During the time Stephenson-Nelson was jointly purchased, Lafayette purchased 40 additional acres adjoining Stephenson-Nelson, in fee, located adjacent to the northeast corner of the Vista Business Park. Lafayette may sell this parcel at a later date to raise funds for the construction of the South Boulder Road extension.

The other public works project proposed to impact the Two Creeks Open Space is an extension of a sanitary sewer line for the City. The sewer line would run north-south, mainly through the Stephenson-Nelson property. The design and construction of this project is unknown (*Figure 3*).

Subsurface Mineral Rights

Mineral rights are severed on some of the properties and may be subject to the right of a proprietor to access and extract a vein or lode. Others may have an existing oil and gas lease or the condition that such a right may be executed. The surface owners (in this case the City and the County) should work with the mineral right owner(s) and its lessees on minimizing adverse impacts to the open space lands. In some instances, the joint owners may also want to consider purchasing the mineral right should this option exist. The joint owners shall embrace a good neighbor policy and strive to cooperate with adjacent landowners.

1.2. PLANNING PROCESS AND GUIDANCE

The original management planning process was initiated in January 2004, in which a planning approach and framework were developed. Resource evaluations were conducted in May and an open house was held in July 2004. Issues and preliminary management direction were discussed with project staff and at public meetings with both the City and County open space advisory boards in August. The draft Plan was completed and presented to both boards in October 2004 where they recommended the Plan for adoption.

The Plan was presented to the Lafayette City Council on November 16, 2004 where they voted unanimously to adopt the Plan. Their motion to approve the plan included the following language:

Motion- “To adopt the plan with the understanding that the language in the plan remain flexible enough for future discussion of two issues: prairie dogs and the extension of South Boulder Road; also to allow equestrian usage of the existing and future Coal Creek Trail through Two Creeks Open Space; and with the understanding that Boulder County will manage and maintain the trail and with the further stipulation that the agreement be reviewed annually; and, with regard to equestrian use of the trail, that the horses not be allowed off the trail and the trail be maintained to a certain level of cleanliness.” A

friendly amendment was submitted and accepted “to require that a copy of the plan be provided to ERO.”

On December 7, 2004, the Board of County Commissioners approved the Plan for final adoption.

An amendment to the original management plan was deemed necessary in 2006, in response to the joint acquisition of the 166-acre Stephenson-Nelson property. Planning for the amendment ensued in July 2006. A resource evaluation was conducted in June 2006 and an open house was held in September. As in the original management plan, the major issues in the amendment were discussed with County and City staff throughout the process.

The amendment summary was presented at public meetings for both the City and County open space advisory boards in November 2006, and endorsed the amendment in January and February 2007, respectively. The plan amendment was presented to the Lafayette City Council on January 20, 2007 where they voted unanimously to adopt the Plan amendment. On February 2007, the Board of County Commissioners approved the Plan for final adoption of the plan amendment and approved of staff incorporating it into a revised management plan.

The joint purchase of the Mountain View Egg Farm property in September 2009 led the parties to propose a further update to the Two Creeks Open Space Management Plan. Planning for the property began internally in December 2009. The public portion of the planning process began in March 2009. The addition of the Mountain View Egg Farm allows the parties to explore the expansion of agricultural opportunities, further develop a regional trail, and expand both wildlife protection and grassland restoration efforts.

Guidance

The Comprehensive Plans of the City and County both outline goals and policies that are relevant to the Jointly Owned Boulder County-Lafayette Open Space. Also, the 2005 Lafayette Open Space and Trails Master Plan provided specific management ideas for jointly owned property. These goals and policies are identified in *Appendix 1* and provide direction for natural resource planning and management, agriculture, and recreation.

The Lafayette Open Space Advisory Committee independently developed management policies and guidelines for the some of the subject properties (*Appendix 2*). These guidelines provided valuable guidance and input into developing a shared vision.

The Boulder County *Grassland Management Plan, Prairie Dog Habitat Element* provides specific guidance for maintaining appropriate habitat and for removing prairie dogs from unsuitable areas. Prairie dog management on the subject properties will be handled according to this plan. A summary of the plan can be found in *Appendix 4*.

Specific provisions from Intergovernmental Agreements (IGAs) apply to some of the properties. These provisions are summarized in *Appendix 5*.

2. EXISTING CONDITION

2.1. PROPERTY INVENTORY AND DESCRIPTIONS

The City and County own nine properties along Coal Creek to the east of Lafayette (*Figure 2*). These properties are known as Two Creeks Open Space and are described below:

2.1.1. Armstrong Property

Jointly owned with the City of Lafayette and located east of 120th Street and south of Flagg Drive, due south of Flagg Park. The 31.159-acre property contains 0.5-mile of Coal Creek and consists of upland grasslands and riparian woodlands. The northernmost 12 acres were mined for coal in the 1940s and 1950s. The upland portion of the site is currently being restored to native grassland.

Acquisition History

The City and County purchased the Armstrong Property in 1996 for \$206,505. Each paid half of the total purchase price and owns a 50 percent undivided interest. During the due diligence phase of the acquisition, it was discovered that an area 50-feet x 350-feet contained an illegal landfill. This outlot was excepted from the original purchase and was later conveyed to the County in April 1998. In October 1997, the size of the property was reduced by 1 acre as a result of a land exchange with a neighboring landowner. The purpose of the acquisition was to preserve riparian resources, agricultural lands, and for recreational use. In 1999, the UDFCD provided a reimbursement grant of \$45,630 for the acquisition of 14.04 acres of floodplain property.

Access

Access to the northern and eastern portions of the property is through Flagg Park (off of Flagg Drive) and across the bridge over Coal Creek. Access to the southwest portion of the property is gained through a temporary easement granted across private property to the adjacent south, which takes off from the southeast corner of the Haselwood property.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- An oil and gas lease by Julie Lupton to Martin Exploration Management Company dated January 20, 1988.
- All oil, gas, and other minerals have been reserved by Irvington Coal & Land Company.
- According to the terms of the UDFCD agreement, the City or County may not dispose of or change their use of the floodplain portions of the property from open space without approval from UDFCD.
- A covenant that prohibits the sale or donation of alcoholic beverages on the property.
- Right-of-way for County Road No.1 (Flagg Drive).

2.1.2. Flagg Park Property

Owned fee-simple by the City of Lafayette and leased to the County. The property is located on 12400 Flagg Drive. The property contains 4.65 acres and a section of Coal Creek. The property is a former landfill and was developed into a community park in 1979 with the help of grants from the Department of Housing and Urban Development (HUD) and the Land and Water Conservation Fund (LWCF). The property was leased to the County in 1977 for a period of 25

years at \$10/year, which expired on November 1, 2002. A new lease agreement is being negotiated. In addition to its traditional park use, the property is planned to be used as a trailhead for the Coal Creek Trail.

Acquisition History

The Flagg Park property was deeded to the City in 1964 by the Hurst Family. In November 1977, the County Commissioners entered into a lease agreement with the City and planned to develop the site as a natural park.

Access

Access to the property is from the park entrance off of Flagg Drive, east of 120th Street and south of Baseline Road.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- Any and all road, ditch, pipeline, railroad, or utility easement or right-of-way.
- A covenant relating to the acceptance of LWCF grant funds that allows outdoor recreation use only and prohibits indoor facilities on the property.

2.1.3. Haselwood Property

Jointly owned with the City of Lafayette and located east of Flagg Drive and the Armstrong property. The property consists of 59.373 acres and contains a mixture of upland grasslands, several small wetlands, and a riparian woodland. The property contains 0.5-mile of Coal Creek. The property was used for various agricultural and industrial uses over the last 100 years, including livestock grazing, railroad use, coal mining in the 1940s and 1950s, and oil and gas exploration. A 5.68-acre conservation easement is located to the adjacent east of the property. The upland portion of the property is currently being restored to native grassland.

Acquisition History

The City and County purchased the Haselwood Property in April 1998 for \$385,924.50. A conservation easement was purchased on 8 acres for \$10,280. Each paid half of the total purchase price and owns a 50 percent undivided interest. The purpose of the acquisition was to preserve riparian resources, agricultural lands, and for recreational use. In 1999, the UDFCD provided a reimbursement grant of \$46,637.50 for the acquisition of 14.35 acres of floodplain property.

Access

Access to the property east of the Creek is through Flagg Park (off of Flagg Drive) and across the bridge over Coal Creek.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- An oil and gas lease by James T. Haselwood to Petrogulf Energy Company dated July 24, 1981.
- An oil and gas lease by Todd Planning and Service Company to Petrogulf Energy Company dated November 7, 1981.
- Two oil and gas leases by Irvington Coal and Land Company to Petrogulf Energy Company dated November 7, 1981.

- An easement over the property recorded on August 21, 1996.
- A 20-foot wide easement for the repair and maintenance of the existing overhead power line and the existing underground power and gas lines from Flagg Drive.
- An easement for new water and sewer utilities from Flagg Drive.
- All oil, gas, and other minerals have been reserved.
- All oil and gas royalty payments were assigned to James T. Haselwood for the life of relevant leases.
- According to the terms of the UDFCD agreement, the City or County may not dispose of or change their use of the floodplain portions of the property from open space without approval from UDFCD.
- Right-of-way for the construction, operation, and maintenance of telephone lines.

2.1.4. Lafayette Buffer Parcel

Owned fee-simple by Boulder County. The property is located due north of the Stephenson-Nelson property. The property contains 3.75 acres, consisting of a 30 to 60 foot-wide strip of land on the southern border of the Serrano, Armstrong, and Haselwood properties. Near the eastern boundary of the Haselwood property, the parcel widens to 60 feet and runs east to the county line. Coal Creek traverses the property.

Acquisition History

The property was acquired by the County in 1934 and was declared open space by the Board of County Commissioners on June 24, 2004.

Access

Access to the property is from the east end of the parcel on a dirt road that runs west from Tennyson Road in Broomfield County.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- All coal, oil, gas, and other minerals were reserved by the Irvington Coal and Land Company.

2.1.5. Madrigal Property

Jointly owned with the City of Lafayette and located south of Flagg Drive, due north of the Serrano property. The property consists of 1.572 acres and contains a riparian forest and upland grasslands. Coal Creek traverses the property.

Acquisition History

The Madrigal Property was acquired by the City and County in October 1997. Adverse possession claims between the City/County and the Madrigal Family were settled through a land exchange that resulted in the conveyance of the 1.57-acre parcel.

Access

Access to the property is from the adjacent Serrano property, which is accessed from a strip of land that abuts Flagg Drive.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- The right of a proprietor of a vein or lode to extract and remove his ore.
- Rights or conditions regarding underground pipeline facilities recorded June 15, 1986.
- Terms and conditions of the Settlement Agreement between the City/County and Jane Madrigal recorded December 18, 1997; including, but not limited to, a covenant that prohibits the construction of any path or trail on the property north of Coal Creek.
- Right-of-way for Flagg Drive.

2.1.6. McClain Property

Jointly owned with the City of Lafayette and located south of Flagg Drive, surrounded on all sides by the Serrano property. The property consists of 2.3 acres and contains a small section of Coal Creek.

Acquisition History

The McClain Property was acquired by the City and County in December 1996 for \$45,000. Each paid half of the total purchase price and owns a 50 percent undivided interest. The purpose of the acquisition was to preserve riparian resources and for recreational use. In 1999, the UDFCD provided a reimbursement grant of \$16,728.26 for the acquisition of 1.71 acres of floodplain property.

Access

Access to the property is from the adjacent Serrano property, which is accessed from a strip of land that abuts Flagg Drive.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- An easement for power lines and poles traversing the southerly portion of the property.
- All coal as reserved by the Globe Coal Mining Company recorded March 27, 1922.
- All minerals as reserved by Joseph Koeppler recorded January 20, 1925.
- The fact that records show no means of ingress or egress to the property.
- A covenant that prohibits the sale or donation of alcoholic beverages on the property.

2.1.7. Serrano Property

Jointly owned with the City of Lafayette and located east of 120th Street and south of Flagg Drive, due west of the Armstrong property. The property consists of 20 acres and contains a section of Coal Creek. The property consists of a riparian forest and upland grasses and is temporarily leased for livestock grazing.

Acquisition History

The Serrano Property was purchased by the City and County in February 1996 for \$162,500. Each paid half of the total purchase price and owns a 50-percent undivided interest. Due to title issues relating to using a portion of the adjacent reserved parcel for access to the property, an escrow account holding \$3,750 of the purchase price and a deed for a 25-foot wide strip of land that would provide access to the property from Flagg Drive was established. The escrow agreement stated that the funds would be released and the deed recorded on October 31, 1996 unless otherwise agreed to in writing. The funds were released to Michael Serrano and the deed was recorded on February 6, 2001. The funds were not accepted by Mr. Serrano, returned to the County, and were then sent to the State escrow account according to statutory requirement.

The purpose of the acquisition was to preserve riparian resources, agricultural lands, and for recreational use. The Seller retained an easement that provides for livestock access to Coal Creek across the open space property. In 1999, the UDFCD provided a reimbursement grant of \$39,552.50 for the acquisition of 12.17 acres of floodplain property.

Access

Access to the property is from a strip of land (25 foot-wide) that abuts Flagg Drive. For the access to be used, the access must be constructed. The County should contact and coordinate with the adjacent landowner (Serrano) to examine alternatives and determine the most preferable route.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- In 2005, the agricultural lease on the Serrano property expired and a long-term lease was not re-issued.
- The Property can be used for temporary livestock grazing, as approved by County staff.
- An access easement to Coal Creek granted to Michael Serrano for the purpose of cattle watering. This easement shall terminate upon the death of Michael Serrano; or the death of Shavone or Venessa Serrano should he convey his interest to them.
- A 20 foot easement and right-of-way for gas pipeline granted to Martin Exploration Management Company recorded January 9, 1989.
- Reservation of all coal by the Globe Mining Company recorded March 27, 1922.
- Reservation of minerals by Joseph Koepler recorded January 20, 1925.
- Rights to a gas line that is situated in the western portion of the property.
- According to the terms of the UDFCD agreement, the City or County may not dispose of or change their use of the floodplain portions of the property from open space without approval from UDFCD.

2.1.8. Stephenson-Nelson Property

Jointly owned with the City of Lafayette and located east of 120th Street, directly south of the Lafayette Buffer parcel and north of the Archdiocese of Denver parcel owned by Boulder County. The property includes the confluence of Rock Creek and Coal Creek, riparian forest, and upland grasses.

Acquisition History

Boulder County and Lafayette jointly purchased approximately 165.768 acres of the Stephenson-Nelson property in 2005 from Roswell F. Taylor Jr. and Dorothy L. Stephenson, in care of Mark Stephenson. The total purchase price was \$4,182,091.20, of which the County paid \$2,091,045.60. The purchase price was calculated at \$25,900/acre for the 158.768 acres that were already annexed to Lafayette, and \$10,000/acre for the 7 acres that were not annexed.

The City and County exchanged reciprocal conservation easements over their 50% undivided interests in the 165.768 acres. The property purchase included the property's mineral rights; however, the sellers will receive the royalties from the oil and gas wells until 2010.

Lafayette purchased 40 additional acres in fee, located adjacent to the northeast corner of the Vista Business Park. Lafayette may sell this parcel at a later date to raise funds for the construction of the South Boulder Road extension.

Access

There are oil and gas roads that cross the property, allowing access from the west off of 120th Street or from the east, across the Anthem subdivision in Broomfield.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- The County granted an access easement to the City of Lafayette across the Lafayette Buffer Parcel that separates the annexed land from the 7-acre (unannexed) parcel.
- Lafayette may construct, operate and maintain one sewer line running generally in a North/South direction across the NE 1/4 of the NW ¼ of Section 12 (*Figure 3*).
- There are five oil and gas wells on the property under two leases that were executed in 1986 and 1991.
- Boulder County has the right of first refusal on the 40 acres adjacent to Stephenson-Nelson the northeast corner of the Vista Business Park, should Lafayette choose to sell.
- Lafayette may also construct an extension of South Boulder Road across the jointly-owned property. The extension, if built, would connect the S. Boulder Road intersection at 120th St. to Broomfield and the Anthem Planned Unit Development. The right-of-way (ROW) corridor may be up to 120 feet in width and, in the event that the alignment does not traverse the City's 40-acre parcel, the conveyance may include ROW to connect the north and east property line of the City's 40-acre parcel to the South Boulder Road extension.
- Lafayette & the County agreed to an anticipated alignment, included as Exhibit E of the PARTIAL ASSIGNMENT OF PURCHASE AGREEMENT AND AGREEMENT TO EXCHANGE RECIPROCAL CONSERVATION EASEMENTS (*Figure 3*). Any extension of South Boulder Road will not cross downstream of the confluence with Rock Creek and Coal Creek without the prior express written consent of County. Lafayette must pay County \$12,950.00 per acre of the jointly-owned property conveyed by County for this road ROW.
- This purchase included a 40 foot-wide easement across the Mountainview Egg Farms property to the north for vehicles, livestock, bicycles and pedestrian traffic, from State Highway 7 south to the Stephenson Nelson property.

Special Designations

Much of the central and western part of this property is designated as agricultural lands of local importance, according to the County comprehensive plan. West of Rock Creek, the property is classified with agricultural lands of national importance.

2.1.9. Mountain View Egg Farm Property

Jointly owned with the City of Lafayette and located south of Colorado State Highway 7 and east of the Haselwood property. The property consists of 144.7 acres and contains former agricultural lands, undeveloped grassland habitat, and a former egg production operation. The property is currently leased for grazing. The existing lease is in effect through April 2011 and may be continued at the discretion of the property owners.

Acquisition History

The 144.8-acre Mountain View Egg Farm was purchased by the City and County in September 2009 for \$5.2 million. The purchase included water rights to 35-acrefeet of groundwater per annum. The City and County split the purchase price such that the City paid 60 percent of the cost and the County paid 40 percent. The parties each own a conservation easement over the other's ownership interest. The purpose of the acquisition was to expand wildlife habitat, provide agricultural opportunities, and provide for recreational trail development.

Access

The main access to the site is from Colorado Highway 7 on the northern edge of the property. An oil and gas access road enters the property from the south along the Lafayette Buffer Parcel.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- An existing agricultural lease was acquired along with the property. The current lease continues to April 2011 at which time it may be renewed through the agreement of the City and County.
- 40-foot access easement conveyed to the County of Boulder.
- Terms, conditions, provisions, agreements, reservations, and obligations contained in the Agreement recorded October 29, 1974.
- A 16.5-foot wide utility easement not located exactly on the parcel.
- A CDOT owned Right of Way easement located on the northern border of the property.
- A 20' wide utility easement entering the property on the south and continuing north through all properties.
- An access easement 40' wide that lines the eastern edge of the property from north to south to the City of Lafayette and Boulder County.
- An oil and gas lease from the Rockwell Partnership to Martin Exploration. Covers the entire property
- A lease to Martin Exploration for oil and gas extraction.
- A pipeline easement to Panhandle Eastern Pipe Line Company.
- A general description of the area controlled by Panhandle Eastern Pipe Line Company.
- An underground natural gas pipeline.
- An oil and gas lease to USI Capital, Inc.
- An oil and gas lease to IFG Leasing, Inc.
- An oil and gas lease to Patina Oil & Gas Corporation.
- Another oil and gas lease to Patina Oil & Gas Corporation.
- A request for notification of any surface developments on the property from Noble Energy.
- Easement for the affects of the development of the Northwest Parkway by the Northwest Parkway Authority.

Special Designations

Portions of the property are considered Lands of Local Agricultural Importance in the Boulder County Comprehensive Plan. A small portion of the center of the site is designated in the Comprehensive Plan as a Coal Resource Area.

2.1.10. Warembourg-Lafayette Farm Property

Jointly owned with the City of Lafayette and located east of U.S. 287 and Public Road on 442 S. 112th Street. The property consists of 109.98 acres and contains irrigated agricultural fields, a stock pond, and a riparian forest. The property contains 0.5-mile of Coal Creek. The property is leased for the production of irrigated grass hay or alfalfa and for livestock grazing. A sign that indicates the property is jointly-owned as agricultural open space is located along the western boundary.

Acquisition History

The 110-acre Warembourg-Lafayette Farm Property was purchased by the City and County in June 2003 for \$3,020,240. The purchase included water rights and also a 1.29-acre house lot that was sold in December 2003 subject to a conservation easement. The City and County each paid half of the total purchase price and owns a 50 percent undivided interest. Each owns a conservation easement over the other's ownership interest. The UDFCD provided a grant of \$200,000 for the acquisition of the floodplain portion of the property. The purpose of the acquisition was to preserve riparian resources, agricultural lands, and for potential future recreational use.

Access

Access to the property is from S. 112th Street, which takes off of Public Road east of U.S 287. The access road is due south of the house lot.

Water Rights

- 58.33 shares in the Goodhue Ditch
 - adjudicated June 2, 1882; appropriation date May 1, 1873
 - pro-rata share of the ditch is 37.0 acre-feet based on ownership of 58 shares (0.64 acre-feet/share)
 - assuming 60 percent irrigation efficiency, yields 22.2 acre-feet, or 0.26 acre-feet per acre for 84.8 acres historically irrigated on the property
- 7 cubic feet per second (cfs) decreed to the Harris Ditch
 - adjudicated June 21, 1926; appropriation date April 30, 1876
 - assuming 60 percent irrigation efficiency, yields 1.92 acre-feet per acre for 84.8 acres historically irrigated on the property
- 0.8 cfs decreed to the Willis Ditch
 - adjudicated December 31, 1971; appropriation date December 20, 1953 for 2 cfs (of this 2 cfs, 1.2 cfs were abandoned and 0.8 cfs were made absolute on April 25, 1990)
 - composed wholly of water released from the Louisville Wastewater Treatment Plant
 - very junior in priority and available only in above average water years (no diversion records available)

The water rights are applied to the property from two head gates on Coal Creek. In 2000, Keith Bateman (irrigator) reported that he had plenty of water over the last ten years. A water valuation confirmed this report and stated that an adequate water supply is typically available (RMC 2000).

The water rights are owned equally by the City and County as tenants in common. The purchase agreement states that the County will manage the water rights, which has been interpreted to mean that the County is also responsible for paying assessments and voting ditch shares.

Current Leases, Easements, Encumbrances, and Rights-of-Way

- The Property is leased for crop production and livestock grazing. The lease agreement requires the Property to be managed consistent with a soil and water conservation plan.
- An oil and gas lease by Klubert and Helen Rose Warembourg to Robert C. Roehrs dated November 5, 1975.
- An oil and gas lease by Klubert and Helen Rose Warembourg to Todd T. Hutchings dated June 5, 1981.
- An oil and gas lease by Klubert and Helen Rose Warembourg to Martin Exploration Management Company dated March 18, 1992.
- An easement for oil and gas pipeline purposes granted to Public Service Company of Colorado.
- Reciprocal conservation easements were exchanged between the City and County and restrict the use of the subject property to open space.
- Reservation of minerals on the property.
- The Sellers reserved the royalties from existing oil and gas leases through June 25, 2028.
- According to the terms of the UDFCD agreement, the City or County may not dispose of or change their use of the floodplain portions of the property from open space without approval from UDFCD.
- Up to 100 feet of the eastern and western property boundaries were reserved for use as future trail corridors.
- The development of structures on the property is prohibited, except in connection with recreational trails.
- Right-of-way for the construction, operation, and maintenance of telephone lines.
- Right-of-way for South Public Road- Old U.S Highway 287.

2.2. NATURAL RESOURCES

The Coal Creek corridor contains important natural resources, including significant agricultural lands, riparian habitat, and a variety of important wildlife species. County staff and consultants have documented resources on properties within the corridor through wetland and vegetation inventories, rapid resource assessments, avian surveys, and baseline inventories. The following is a compilation of the significant natural resources found within the corridor, including soils, vegetation, wildlife, and water resources.

2.2.1. Soils

Eleven soil types from seven soil series occur on the subject properties (*Figures 5 and 6*). Capability classifications are discussed for those soils that are actively used for agricultural production. Tree suitability and limitations to recreational use are also discussed for those soils where these two activities are likely to occur.

Figure 5: Two Creeks Open Space – Soils Map

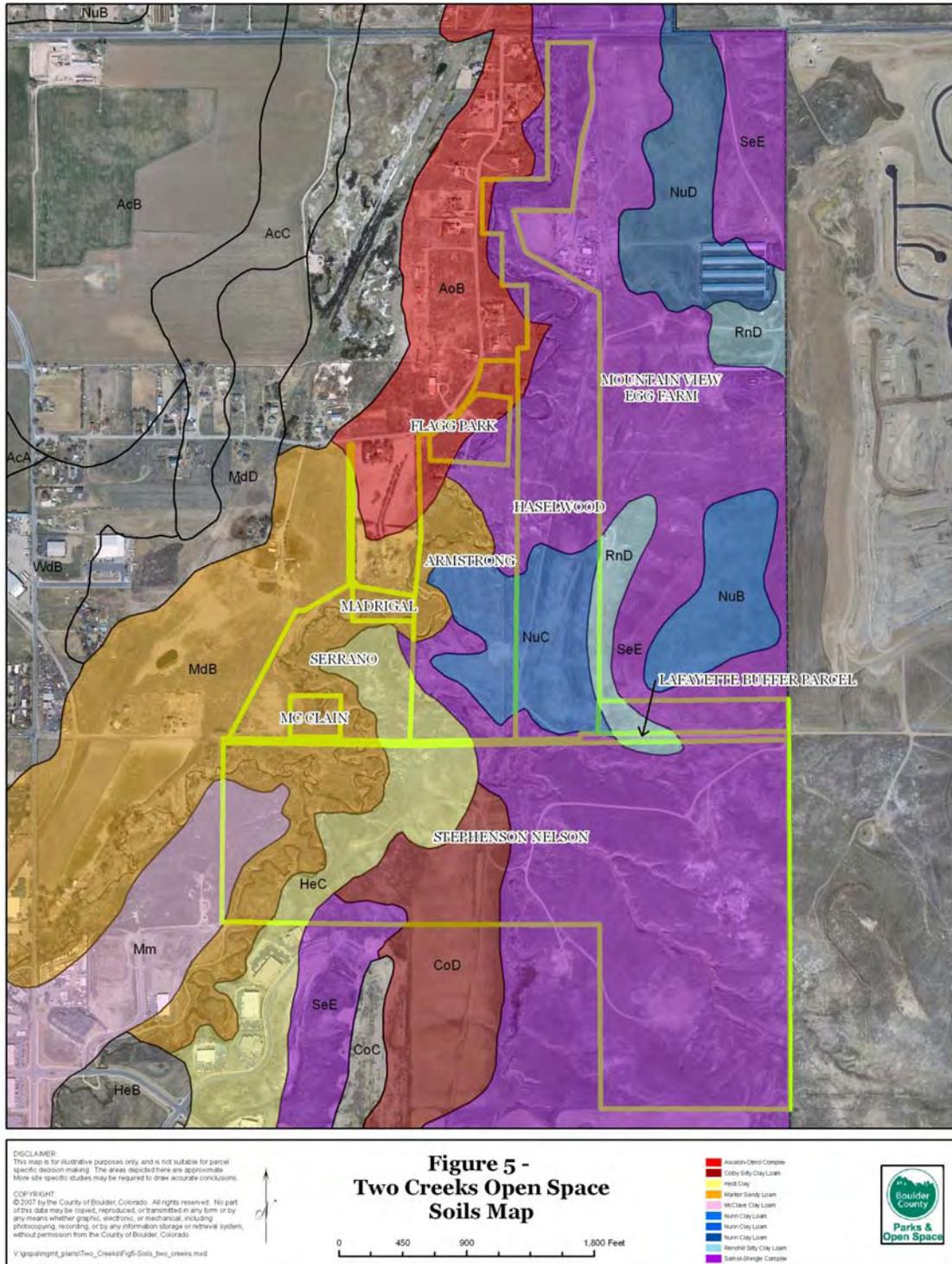
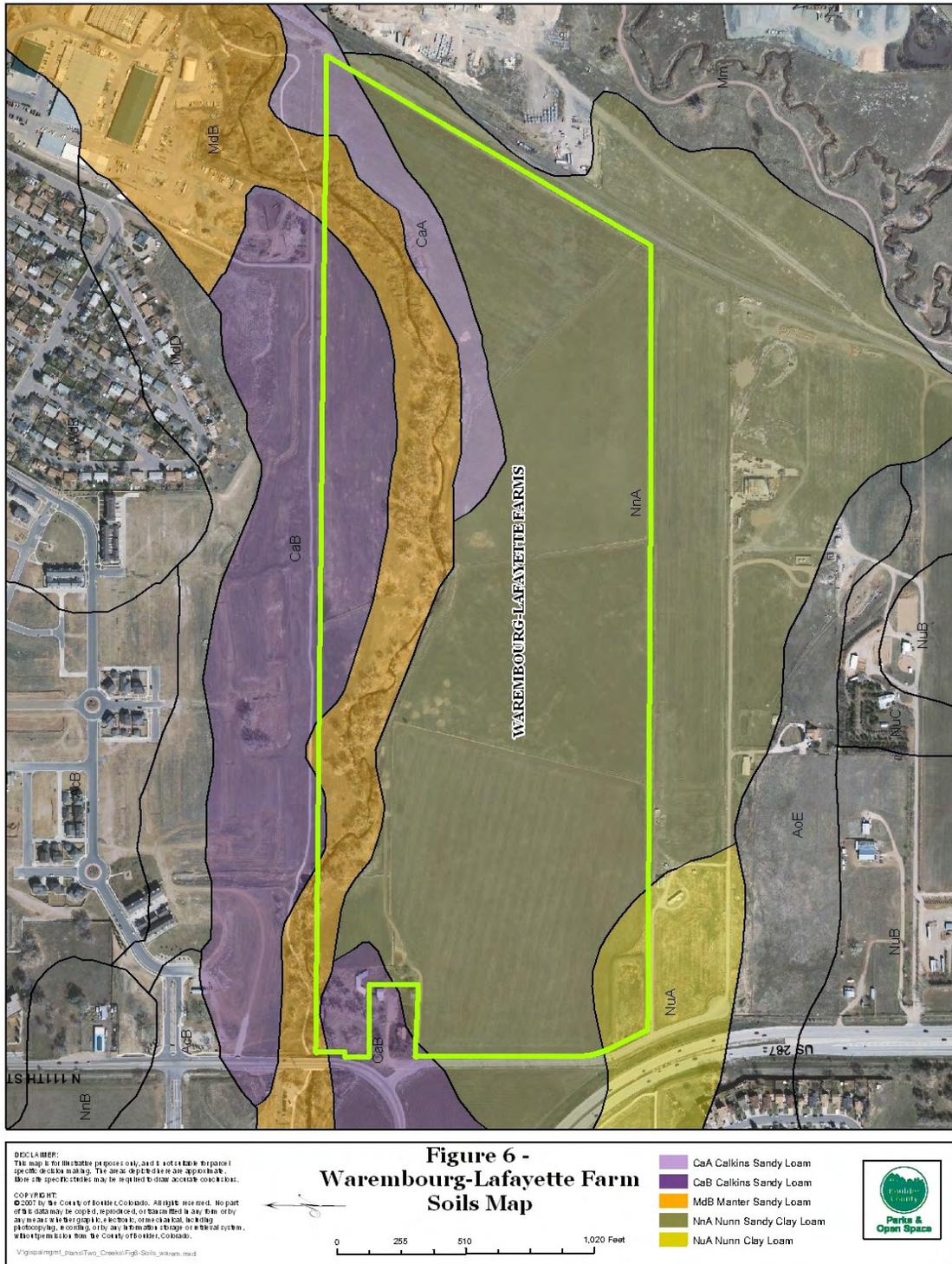


Figure 6: Warembourg-Lafayette Farm – Soils Map



Ascalon Series- This series is made up of deep, well-drained soils. These soils formed on terraces and uplands in loamy mixed alluvium and wind-laid materials. Slopes range from 0 to 20 percent. The native vegetation is mainly blue grama. The surface layer is made of grayish-brown sandy loam and is about 8 inches thick. The subsoil reaches to a depth of 26 inches. The substratum, to a depth of 60 inches or more, is strongly calcareous. These soils have moderate permeability and available water capacity is high. Roots can penetrate to a depth of 60 inches or more and is most often used for irrigated and dry cropland.

AoB- Ascalon-Otero complex, 0-3% slopes: This complex is comprised of Ascalon sandy loam (60%) and Otero sandy loam (30%) soils. Runoff is slow to medium on this complex and the erosion hazard is moderate. Maintenance of the organic matter content is important, particularly on areas where the surface layer has been removed by leveling.

Calkins Series- This series is made up of deep, somewhat poorly-drained soils. These soils formed in loamy alluvium on low terraces and bottomlands. Slopes are 0 to 3 percent. The native vegetation is mainly meadow grasses. The surface layer is made of grayish-brown sandy loam and is about 40 inches thick. Underlying this to a depth of 60 inches or more is coarse sandy loam with brown mottles. Soil reaction is neutral. These soils have moderate to rapid permeability and available water capacity is moderate to high. Roots can penetrate to a depth of 60 inches or more and the seasonal high water table is at a depth of 3 feet or less. These soils are used for irrigated crops and for pasture.

CaA- Calkins sandy loam, 0-1 % and CaB- Calkins sandy loam, 1-3% slopes: These soils occur on stream terraces and bottoms. Soils can be limy with slow runoff and slight to moderate erosion hazard. With an irrigated capability class of IIw-2, these soils are especially well suited to water-tolerant crops. To maintain maximum yields, the soils need to be drained or the water table controlled. Crop residue left on the surface during the windy spring months helps to control soil blowing and to increase the organic-matter content of the soil. Irrigating with short runs helps prevent over irrigation and the resulting extreme fluctuation in the water table. These soils are suited to irrigated pasture containing water-tolerant grasses such as tall wheatgrass, tall fescue, or slender wheatgrass. The addition of Alsike clover or alfalfa increases value of the forage. Nitrogen fertilizer increases vigor of the grasses. Good pasture management is necessary.

Colby Series - The Colby series is made up of deep, well-drained soils, with 5 to 9 percent slopes.

CoD - Colby silty clay loam (5 to 9 percent slopes): The soils formed on upland slopes in loamy, uniform wind-deposited material. Native vegetation usually includes short grasses. Runoff is rapid and the erosion hazard is high. In agricultural production, these soils are generally used for irrigated and dryland crops, as well as pasture.

Heldt Series- This series is made up of deep, moderately well drained soils. These soils formed on terraces and uplands in loamy alluvium. Slopes are 0 to 5 percent. The native vegetation is mainly short grasses. The surface layer is made of grayish-brown clay and is about 8 inches thick. The subsoil is a strongly calcareous clay-clay loam down to 36 inches. Soil reaction is moderately alkaline. These soils have slow permeability and available water capacity is high. Roots can penetrate to a depth of 60 inches or more. These soils are used for irrigated and dry

cropland, and for pasture. Where irrigated, water must be applied carefully to prevent waterlogging.

HeC- Heldt clay, 3-5% slopes: This soil has a surface layer of clay down to about 6 inches. Runoff is rapid and the erosion hazard is high. This soil is well suited to native grasses including blue grama (*Bouteloua gracilis*) and needlegrasses (*Stipa* spp.) In agricultural production, these soils are generally used for irrigated and dryland crops, as well as pasture. This soil takes in water slowly and is difficult to work. This soil has severe limitations for paths and trails due to the clay surface layer and is not suited to tree planting.

Manter Series- This series is made up of deep, well-drained soils. These soils formed on terraces and uplands in loamy eolian and outwash materials. Slopes are 0 to 9 percent. The native vegetation is mainly short grasses. The surface layer is made of brown sandy loam and is about 8 inches thick, with a subsoils about 10 inches thick. Underlying this to a depth of 60 inches or more is strongly calcareous sandy loam. Soil reaction is neutral at the surface, and moderately alkaline below the subsoil. These soils have both moderate permeability and available water capacity. Roots can penetrate to a depth of 60 inches or more. These soils are used for irrigated and dryland crops and for pasture.

MdB- Manter sandy loam, 1-3% slopes: This soil occurs on terraces and uplands. Runoff is slow to medium. The hazard of water erosion is moderate, and soil blowing is high. With an irrigated capability class of IIIe-4, this soil is suited to irrigated pasture. A suitable cropping system is alfalfa for 3-4 years followed by corn and small grain. Vegetable crops can be substituted for corn in the rotation. Row crops should be limited to no more than 2 years in the cropping rotation. Irrigation runs should be short due to erosion potential. Windbreaks should be established to help control soil blowing. Fields should not be bare during December through February, and crop residue should be left on the surface in order to minimize soil loss caused by blowing wind. Grain crops respond to nitrogen fertilizer and pasture crops respond to phosphorous. Use of manure increases organic matter and helps with water intake.

These soils are suited to irrigated pasture with species such as smooth brome grass and orchardgrass. The addition of Alsike clover or alfalfa increases value of the forage. In order to maintain production, pastures should be rotated and grazing should be limited so that a minimum of 4 inches of stubble is left on the soil. With good management and use of fertilizer, grass-legume mixtures can be used indefinitely for pasture or hay. This soil has slight limitations when used for paths and trails. Trees suitable for planting on this soil include Colorado blue spruce, green ash, hackberry, sandcherry, chokecherry, and American plum. Supplemental water is often needed.

McClave Series - The McClave series is made up of deep, somewhat poorly drained souls that formed on low terraces in loamy alluvium.

Mm - McClave clay loam (0 to 1 percent slopes): The native vegetation consists primarily of short grass prairie. Runoff is slow on this soil and the erosion hazard is slight. In agricultural production, these soils are used for irrigated crops and pasture.

Nunn Series- This series is made up of deep, well-drained soils. These soils formed on terraces and valley side slopes in loamy alluvium. Slopes are 0 to 9 percent. The native vegetation is mainly short and mid grasses. The surface layer is made of clay loam and is about 10 inches thick. The subsoil, about 20 inches thick, is a brown clay that grades to clay loam. It is noncalcareous in the upper part, but contains soft lime segregations in the lower part. The substratum is a strongly calcareous clay loam extending to a depth of 60 inches or more. In the surface layer, soil reaction is neutral. The subsoil ranges from mild to moderately alkaline. Permeability is slow and available water capacity is high. Roots can penetrate down to a depth of 60 inches. These soils are used for irrigated and dryland crops and for pasture.

NnA- Nunn sandy clay loam, 0-1% slopes: Runoff is slow on this soil and permeability is moderately slow. Although the erosion hazard is slight, this soil should be protected from blowing during periods of strong winds. Most of this soil is used for irrigated crops and some for irrigated pasture. With an irrigated capability unit of I, these soils are suited to all irrigated crops of the area. A suitable cropping system is alfalfa for 3-4 years, followed by corn, sugar beets, and small grain. Vegetables can be substituted for either corn or sugar beets in the rotation. Corn, sugar beets, and small grain respond well to applications of nitrogen fertilizer, and sugar beets and alfalfa respond to phosphorous. Leaving stubble or other crop residue on the surface helps to maintain tilth and to protect these soils from possible damage caused by soils blowing during windy periods in spring. If the soils are left bare for long periods during spring, a cloddy or ridged surface helps minimize wind damage. These soils are suitable for irrigated pasture.

NuA- Nunn clay loam, 0-1% slopes: Runoff and permeability are slow on this soil and the erosion hazard is slight. Most of this soil is used for irrigated crops and some for irrigated pasture. With an irrigated capability unit of IIs-1, these soils are suited to all irrigated crops of the area. A suitable cropping system is alfalfa for 3-4 years, followed by corn, sugar beets, small grain, and then alfalfa seeded with a small grain as a nurse crop. Vegetables can be substituted for either corn or sugar beets in the rotation. These soils are easier to work down for seedbed preparation if they are plowed in the fall. Occasional subsoiling when the ground is dry will temporarily open the soil to air, water, and plant roots. If intensive row cropping is planned, barnyard manure or a green-manure crop should be plowed under in order to maintain the rate of water intake and improve soil tilth. Crops grown on these soils respond well to applications of nitrogen and phosphorous fertilizer, and sugar beets and alfalfa respond to phosphorous. Use of crop residue helps improve tilth, so that these soils can be worked more easily. These soils are suitable for irrigated pasture.

NuC- Nunn clay loam, 3-5% slopes: This soil is limy at a depth of about 16 inches. Runoff is medium and permeability is slow. The erosion hazard is moderate. Most of this soil is used for irrigated and dryland crops and for pasture. With a non-irrigated capability unit of IIIe-7, these soils are suited to dry cropland (wheat-summer fallow) and pasture. Pasture grasses include crested wheatgrass, intermediate wheatgrass, pubescent wheatgrass, and Russian wildrye. To insure a full and vigorous stand of grasses, grazing should be limited until the grass has become established. This soil has moderate limitations when used for paths and trails due to the presence of a clay loam surface

layer. Trees suitable for planting on this soil include Colorado blue spruce, green ash, hackberry, chokecherry, and American plum. Supplemental water is often needed.

NuD- Nunn clay loam, 5-9% slopes: Runoff is rapid and permeability of the subsoil is slow. The erosion hazard is high. Most of this soil is used for irrigated and dryland crops and for pasture. With a non-irrigated capability unit of VIe-1, these soils are best suited to pasture. Pasture grasses include crested wheatgrass, intermediate wheatgrass, pubescent wheatgrass, and Russian wildrye. Blue grama and needle grass are native. Proper management of pasture is necessary. Grazing should be limited so that no more than half of the current year's growth is grazed. To insure a full and vigorous stand, newly seeded grasses should not be grazed during the first growing season. After the stand has been established, grazing should be limited so that a minimum of 3 inches of stubble is left. Trees suitable for planting on this soil include Colorado blue spruce, green ash, hackberry, chokecherry, and American plum. Supplemental water is often needed.

Renohill Series - This series is made up of moderately deep, well-drained soils. These soils formed on upland hills and ridges in loamy parent material weathered from shale and sandstone. Slopes are 1 to 9 percent. The native vegetation is mainly short and mid grasses. The surface layer is made of silty clay loam, is slightly calcareous, and is about 7 inches thick. The subsoil, about 23 inches thick, is slightly calcareous in the upper part, and strongly calcareous in the lower part. Soil reaction ranges from mildly to moderately alkaline. Permeability is slow and available water capacity is moderate. Roots can penetrate to a depth between 20 to 40 inches. These soils are used for irrigated and dryland crops and for pasture.

RnD- Renohill silty clay loam, 3-9% slopes- Runoff is rapid and the erosion hazard is high. Blue grama and needle grass are native. Trees suitable for planting on this soil include Colorado blue spruce, green ash, hackberry, chokecherry, and American plum. Supplemental water is often needed.

Samsil Series - This series is made up of shallow, well-drained soils. These soils formed on upland hills and ridges in clayey residuum weathered from shale. Slopes are 3 to 25 percent. The native vegetation is mainly short grasses. The surface layer is made of clay, is strongly calcareous, and is about 3 inches thick. The underlying material is about 9 inches thick and is strongly calcareous. Soil reaction ranges from mildly to moderately alkaline. Permeability and available water capacity are low. Roots can penetrate to a depth between 10-20 inches. These soils are used for dry pasture.

SeE- Samsil-Shingle complex, 5-25% slopes: Runoff is rapid and the erosion hazard is high. Gullies may contain outcrops of shale or sandstone. Needlegrass, western wheatgrass, and needleandthread are native. If these soils are in native range, they should be plowed only to reestablish stands of grass that have become depleted. For agricultural purposes, this soil is best suited to pasture. Limitations on the use of this soil for paths and trails are moderate to severe based on the presence of a clay surface layer and steep slopes. These soils are not suited for tree planting.

Flagg Park Soils History

In the spring of 1979, 4,500 cubic yards of fill dirt was imported to the site from the Erie Reservoir. In the fall of the same year, Flagg Park was developed using Land and Water Conservation Fund (LCWF) monies. The Department Natural Resources (DNR) Mined Land Reclamation Division conducted soils tests on the property in October 1983. They found only 4-6 inches of sandy soil over a hard clay pan subsurface. Measurements varied from a pH of 7.5-8.0 and there was little or no nitrogen found in the soil. In order to enhance vegetative growth at the site, the Mined Land Reclamation Division recommended a combination of soil ripping, the addition of topsoil and manure, addition of fertilizer to buffer the soil, and the use of alkaline and drought-tolerant species.

2.2.2. Significant Agricultural Land and Special Designations

Eastern Boulder County contains agricultural lands of national significance due to its soil resources and their production capability. These lands are considered prime farmland because of the soils and their associated irrigability. The Warembourg-Lafayette Farm property contains 'Agricultural Lands of State and National Importance' (*Figure 7*). Portions of the Two Creeks properties are designated as 'Agricultural Lands of Local Importance' in the Boulder County Comprehensive Plan. Although a few of the other properties contain eligible soils, only the Warembourg-Lafayette Farm property and the Egg Farm property have land that is actively irrigated and/or has water rights that are attached to the land. Protecting prime farmland from conversion to development is a formidable task and one that has already been accomplished for the property outlined in this Plan.

The Boulder County Comprehensive Plan classifies the subject properties as Open Corridor-Streamside, which further supports the notion of preservation of these lands. In the Two Creeks area, lands west of the Coal Creek are considered Agricultural Lands of National Importance, while portions of the properties east of Coal Creek are classified as Agricultural Lands of Local Importance. The entire Two Creeks area is classified as a Major Geologic Hazard Area. The County Comprehensive Plan also identifies the Coal Creek corridor as an Archaeological Travel Route and a Proposed Trail Alignment.

2.2.3. Vegetation

Three major vegetation types occur within the Coal Creek corridor: riparian forest, upland grasslands, and a few wetlands. Irrigated cropland is addressed in more detail in the Agricultural Resources section of the Plan. Vegetation communities are mapped in *Figures 8 and 9*.

While the Egg Farm generally consists of upland grasslands, it has not yet been mapped as part of the County's vegetation mapping plan. The Department's plant ecologist made preliminary visits to the site and found no significant vegetation requiring protection. Future mapping updates will be added to the existing maps when the project is complete.

Figure 7: Agricultural Lands Map

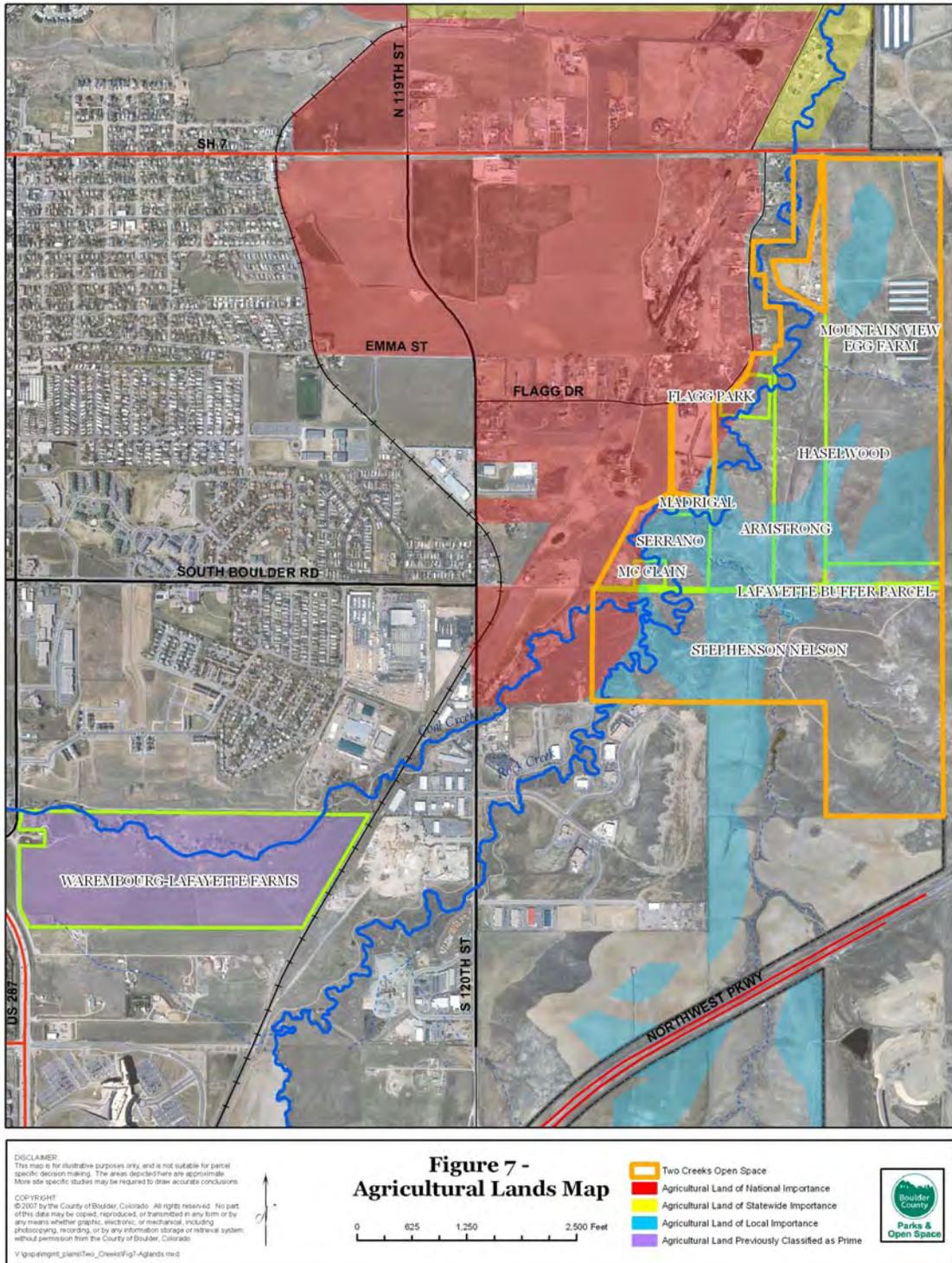


Figure 8: Two Creeks Open Space – Vegetation Map

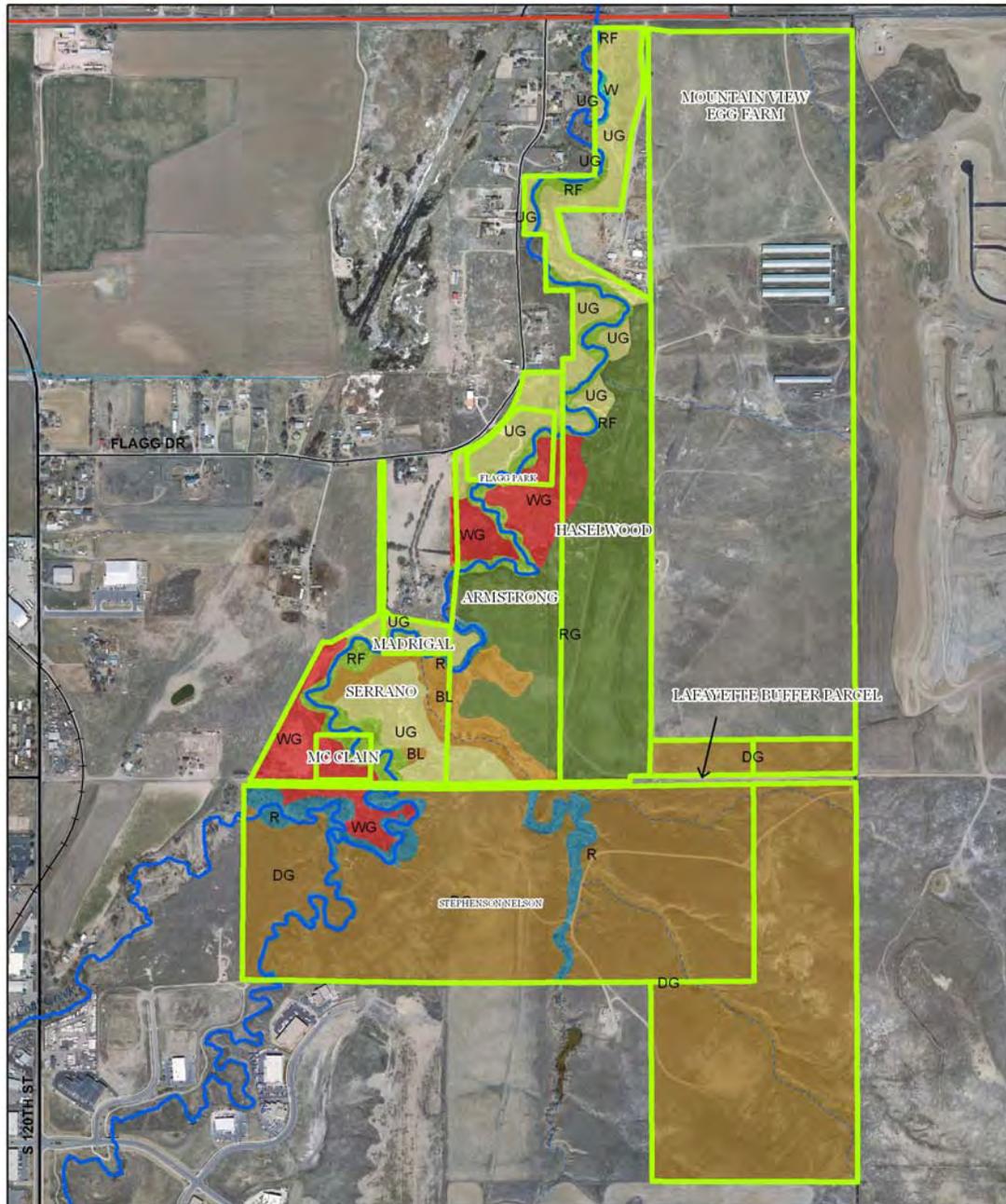


Figure 8 - Two Creeks Open Space Vegetation Map

DISCLAIMER: This map is for illustrative purposes only, and is not suitable for legal or financial purposes. The user should verify any information. All other information may be required for your specific requirements.

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http://www.boulder.gov/arcgis/arcgis/rest/info?layers=TwoCreeksOpenSpace_VegetationMap

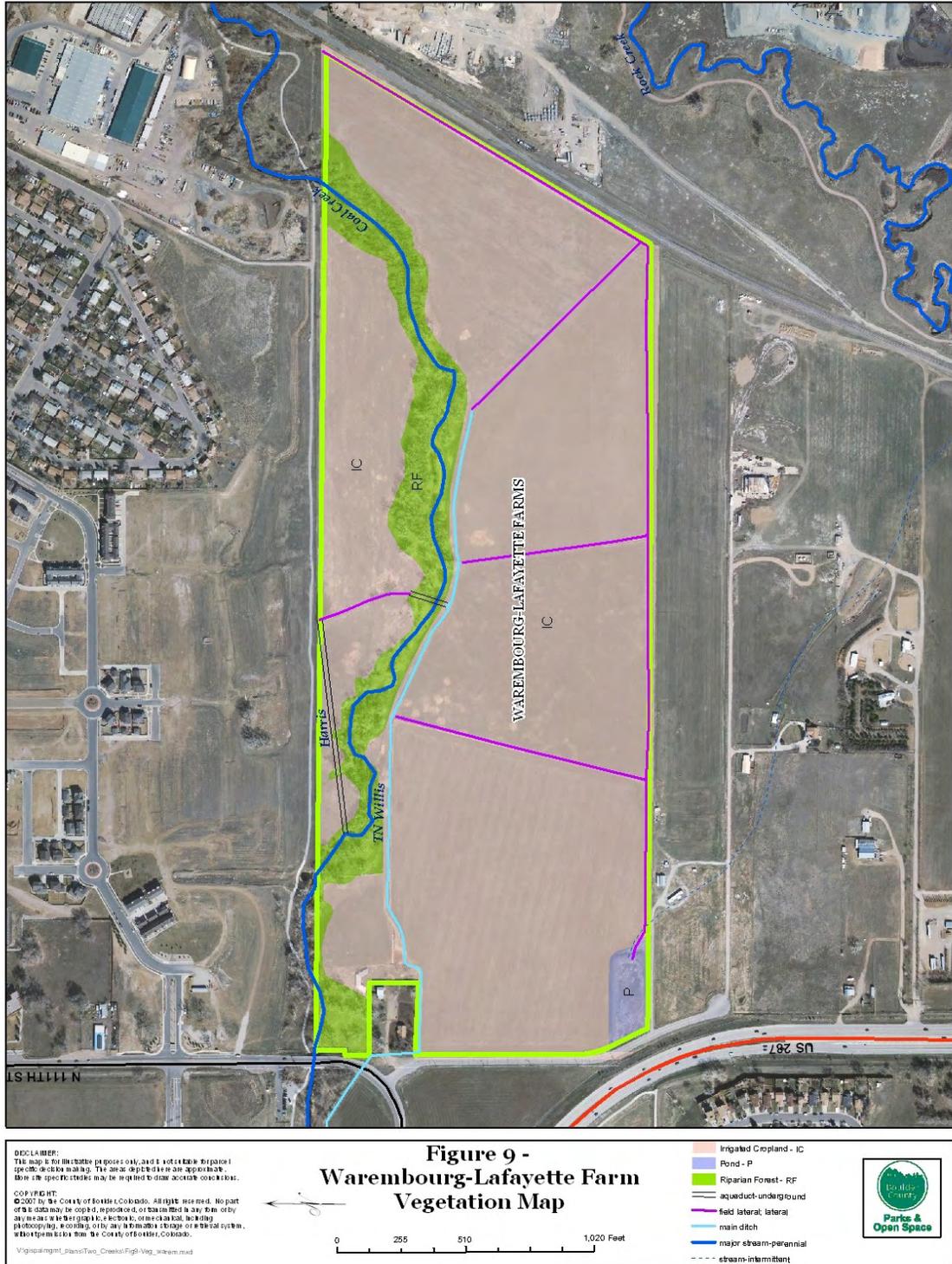
Legend:

- Disturbed Ground - BL
- Disturbed Ground - DG
- Reclaimed Grass - RG
- Riparian Forest - R
- Upland Grass - UG
- Wetland Grass - WG
- Wetland - W

Scale: 0 337.5 675 1350 Feet

Boulder County Parks & Open Space

Figure 9: Warembourg-Lafayette Farm – Vegetation Map



Riparian Forest

Riparian forests within the corridor provide important habitat for numerous wildlife species. Standing dead cottonwoods play an important role for raptors and cavity nesting birds.

The riparian forest community follows the Coal Creek channel throughout most of Two Creeks. The riparian forest is also along Rock Creek, south of the confluence, and in an intermittent drainage east of Rock Creek. The riparian forest community is dominated by various willow species along the stream channel, surrounded by cottonwoods on the riparian benches. Native tree species include plains cottonwood (*Populus deltoides*), narrowleaf cottonwood (*Populus angustifolia*), coyote willow (*Salix exigua*), peachleaf willow (*Salix amygdaloides*), and box elder (*Negundo aceroides*). Common non-native tree species in the Open Space include black locust (*Robinia pseudoacacia*) and Russian-olive (*Elaeagnus angustifolia*). Tamarisk (*Tamarix ramosissima*) may also be found in the Rock Creek riparian corridor. Parks and Open Space commissioned a Riparian Assessment of several streams in 2008. Several reaches were evaluated along Coal Creek in the Two Creeks area. All were rated Moderate in their functionality (Biohabitats 2009). The Assessment also made recommendations about improvements, clean-up, and restoration.

Beaver activity has damaged many mature cottonwood trees along Coal Creek, especially on the Armstrong property. This activity is detrimental to the recruitment and cover of native tree species.

Wetlands on the Property are associated with Coal Creek and Rock Creek and are described in the “Wetland” section below. The riparian understory is generally dominated by introduced grasses such as smooth brome (*Bromus inermis*) and meadow fescue (*Festuca pratensis*), and weedy species such as downy brome (cheatgrass) (*Anisantha tectorum*). Scattered native shrubs, such as snowberry (*Symphoricarpos occidentalis*) and wild rose (*Rosa woodsii*), complement the understory.

Upland Grasslands

Native and introduced grasses and forbs characterize upland grasslands within Two Creeks. Much of the eastern Stephenson-Nelson property is shortgrass prairie, with perennial bunchgrass and widely spaced shrubs. Much of the upland grasslands have been heavily grazed and has a large component of downy brome. This invasive annual grass is common in overgrazed grasslands.

Native shortgrass species present in Two Creeks include blue grama (*Bouteloua gracilis*), buffalograss, western wheatgrass, tansy aster (*Machaeranthera tanacetifolia*), scarlet globemallow (*Sphaeralcea coccinea*), needle-and-thread grass (*Hesperostipa comata*), tumblegrass (*Schedonnardus paniculatus*), yucca (*Yucca glauca*), prickly pear (*Opuntia polyacantha*), broom snakeweed (*Gutierrezia sarothrae*), and gray (rubber) rabbitbrush (*Chrysothamnus nauseosus*).

Introduced species include crested wheatgrass (*Agropyron cristatum*), Russian wildrye (*Psathyrostachys juncea*), and smooth brome.

Swales in Two Creeks contain more soil moisture and show greater density and vigor of native upland grasses. Wetter portions of the properties contain shrubs such as common snowberry (*Symphoricarpos occidentalis*) and wild rose (*Rosa woodsii*). Intermittent drainages that drain offsite to Coal Creek are vegetated with species that occur in moister soils such as smooth brome (*Bromus inermis*), curly dock (*Rumex crispus*), wild licorice (*Glycyrrhiza lepidota*), prickly lettuce (*Lactuca serriola*), and Canada thistle (*Cirsium arvense*). A few scattered plains cottonwood (*Populus deltoides*) and Russian olive (*Elaeagnus angustifolia*) can be found in these drainages (ERO, 2006).

Weedy species include Jim Hill mustard (*Sisymbrium altissimum*), kochia (*Bassia sierversiana*) and ragweed (*Ambrosia artemisiifolia*). Dominant noxious weeds include downy brome and cheatgrass, diffuse knapweed (*Centaurea diffusa*), field bindweed (*Convolvulus arvense*), musk thistle (*Carduus nutans*), Canada thistle (*Cirsium arvense*), Scotch thistle (*Onopordum acanthium*), and hoary cress (white top) (*Cardaria draba*).

Wetlands

Wetlands in the corridor consist entirely of streamside wetlands associated with Coal Creek and Rock Creek. Wetlands provide diverse habitats for a variety of wildlife species, help control flooding, and improve water quality. Mapped wetlands are shown on *Figure 3*.

Streamside wetlands are more dominant in the historical stream meanders and generally include willows and a few rushes. Moist, bottomland areas adjacent to Coal Creek and Rock Creek are dominated primarily by smooth brome and have a significant component of field horsetail (*Equisetum arvense*), Canada thistle. Other species include showy milkweed (*Asclepias speciosa*) and curly dock.

Coal Creek is dominated by Palustrine Forested Intermittently Flooded wetlands (NWI 1975). The reach of the Creek in the northeastern corner of the Warembourg-Lafayette Farm property contains Palustrine Emergent Seasonal wetlands. The stock pond on this property also contains wetland plants. A small portion of the Haselwood property contains a Palustrine Emergent Intermittently Flooded wetland.

A 1.4-acre wetland in the extreme northern portion of the Haselwood property is supported by seasonal flooding of Coal Creek and is dominated by cattail (*Typha latifolia*), arctic rush (*Juncus arcticus*), reed canarygrass (*Phalaroides arundinacea*), spikerush (*Eleocharis palustris*), and hoary cress (*Cardaria draba*) (Wright Water Engineers 1993).

There is a small possible wetland associated with a drainage on the northeastern edge of the Mountain View Egg Farm. This possible wetland is likely larger than in previous years due to the impact of increased runoff from the Anthem development.

Flagg Park Vegetative History

The site was originally used as a landfill, and in 1979, a loamy sand material was imported and placed over the clay cap in an effort to provide a medium for plant growth. The area was drill seeded in 1980 with perennial grasses and seedling trees were transplanted. In July 1983, a small grass fire occurred on the property due to illegal firecracker use. By the fall of 1983, plant

coverage was fair, but the grass was restricted to the planted rows and showed little evidence of spread or sod growth. Tree seedlings displayed varying degrees of vigor, with some dead and a few very healthy. Over time, the trees and grasses died off. The site was later revegetated.

Two Creeks Restoration History

Since 2000, the City and County have focused on restoring lands in the Two Creeks Open Space, east of Coal Creek. The two agencies have shared in the costs of restoration work and have used weed control, prescribed fire, and native plantings.

Approximately 33 acres of the Haselwood property (Block E, *Figure 10*) were seeded with native grasses in April 2000. The seed mix consisted of side oats grama, blue grama, buffalograss, and western wheatgrass. Weed control and prescribed fire were used on the property prior to the seeding. A large stand of kochia appeared in 2001 and the property was inadvertently grazed by a neighbor's horses over the course of the next two years. By the spring of 2003, the property was nearly devoid of vegetation. In June of that year, a cover crop of sorghum-sudan was planted and weed control was conducted. By August, the natives from the initial seeding established. The property was seeded with additional perennials in the fall of 2003, including the addition of prairie sage, fringed sage, tansy aster, scarlet globemallow, fourwing saltbush, winterfat, inland saltgrass, and rabbitbrush. Weed control was conducted in June and July 2004.

Ten acres of the central portion of the Armstrong property (Block B, *Figure 10*) were planted to a cover crop in 2001 that failed due to drought conditions. Cheatgrass dominated and few native grass species were present, although rubber rabbitbrush was prevalent. Another cover crop was planted in June 2003 that also failed due to unchecked competition that was the result of not spraying for weeds. Herbicide applications for cheatgrass and filaree control were conducted later that fall. The Armstrong and Serrano properties (Blocks B, C & D, *Figure 10*) were burned in March 2004 and some kochia control was initiated. By June, the weed conditions were dramatically improved.

Restoration Block A (*Figure 10*) on the Armstrong property has been treated for weeds (including mowing and herbicide application) but has not been burned or seeded. The portion of the Serrano property that was treated (Block C, *Figure 10*) currently contains a good cover of native grasses and will only be interseeded with native forbs and shrubs for species diversity and for the purpose of maintaining prairie dog tolerant cover.

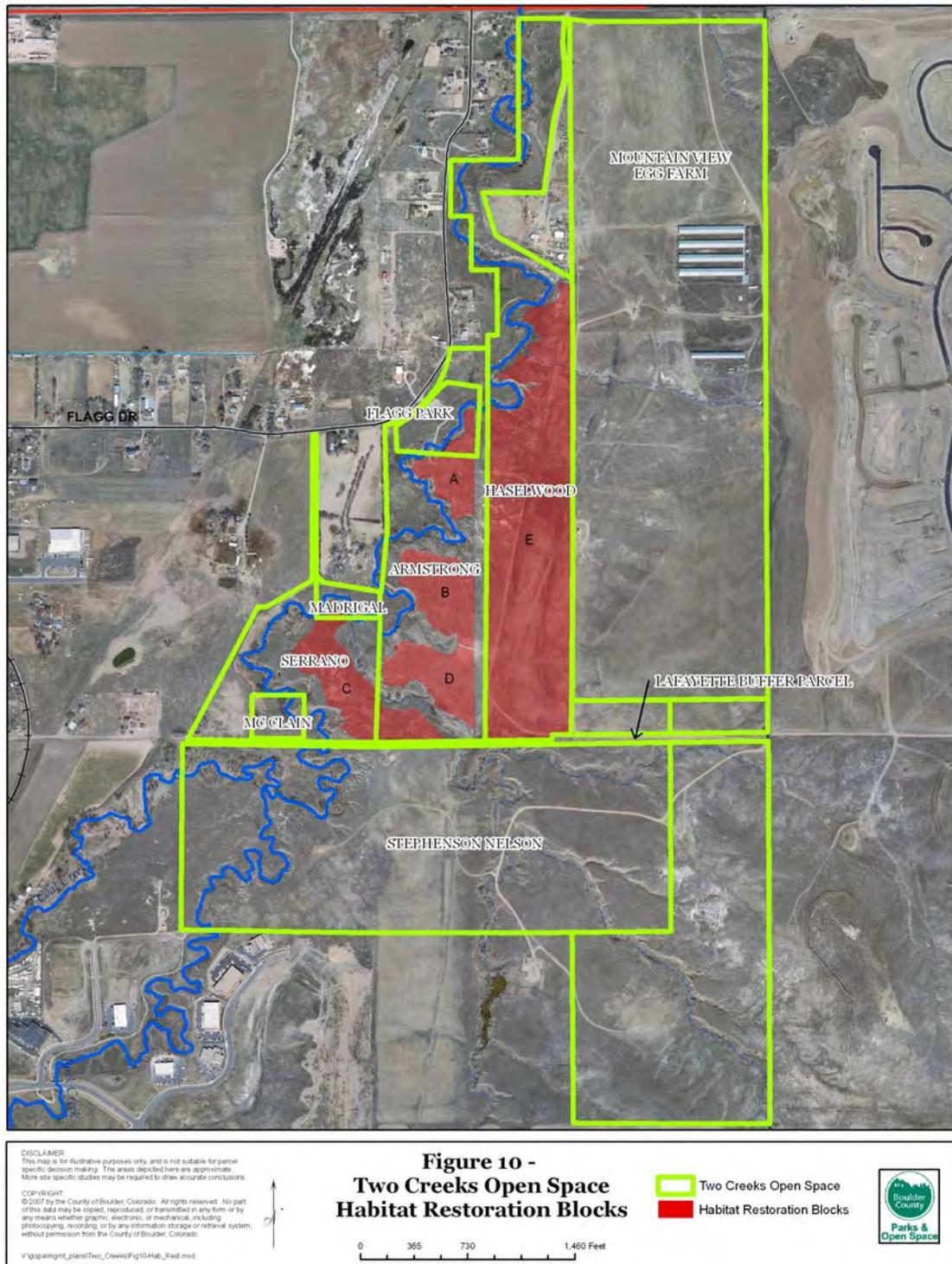
Restoration efforts ended in 2004. Two years of drought and the strong return of prairie dogs onto the restored areas resulted in an unsuccessful restoration effort. This scenario was useful as a lesson in how restoration can be carried out in areas with prairie dog populations. Any future restoration efforts will need to be designed to address this issue.

2.2.3.1. Noxious Weeds

Noxious weeds threaten native plant communities by displacing desirable native species. Alien plants that are highly invasive usually do not have natural pathogens and predators to keep their populations under control. Some non-natives, like diffuse knapweed, contain allelopathic

chemicals, which can suppress the growth of other species and allow the plant to grow in single-species stands.

Figure 10: Two Creeks Open Space – Habitat Restoration Blocks



According to the Colorado Noxious Weed Act (as amended 2004), landowners are required to manage noxious weeds on their lands. State listed noxious weeds are organized into three management categories:

- List A species- targeted for eradication and require specific management actions as well as coordination with the State Weed Coordinator within one year of detection. Not known to be present on the subject properties.
- List B species- benefit from management to stop their spread. List B species present on the properties include:
 - Canada thistle
 - common teasel (*Dipsacus fullonum*)
 - diffuse knapweed
 - field bindweed
 - hoary cress
 - musk thistle
 - redstem filaree (*Erodium cicutarium*)
 - Russian-olive
 - Scotch thistle (*Onopordum acanthium*)
 - tamarisk/ saltcedar (*Tamarix ramosissima*, *Tamarix parviflora*).
- List C species- widespread and are targeted for improved management and public education. List C species present on the properties include:
 - chicory (*Cichorium intybus*)
 - common mullein
 - downy brome/cheatgrass (*Bromus tectorum*)
 - puncturevine (*Tribulus terrestris*).

On the Two Creeks Open Space, six of the noxious weeds are designated as local noxious weeds by Boulder County in the Boulder County Weed Management Plan: diffuse knapweed, Canada thistle, common teasel, musk thistle, Scotch thistle and tamarisk. Other weed species observed were kochia, curly dock, and filaree. Downy brome dominates certain areas that were overgrazed.

The Warembourg-Lafayette Farm property contains four species listed as noxious weeds in the Boulder County Weed Management Plan: diffuse knapweed, Canada thistle, musk thistle, and Scotch thistle. The field north of Coal Creek has the highest concentration of noxious weeds. Diffuse knapweed is the most widespread and greatest in density. Stands of Canada and musk thistle are scattered, as well as infrequent stands of Scotch thistle. Stands of curly cup gumweed occur in the western portion of the field, and wild licorice on the east end. Both are dense enough to be detrimental to hay production or grazing. Scattered throughout the northern field is broadleaf plantain at a density that appears to be decreasing grass production. Moderate numbers of Russian-olive trees are present throughout the riparian zone. Light stands of chicory are found in wet areas of the field and in the riparian area. Other weed species infrequently observed includes common mullein, kochia, horseweed (marestail) (*Conyza Canadensis*), and meadow salsify (*Tragopogon dubius*).

2.2.3.2. Sensitive Plant Species

The Coal Creek corridor may provide potential suitable habitat for the Ute ladies'-tresses orchid (*Spiranthes diluvialis*) and Colorado butterfly plant (*Gaura neomexicana* ssp. *coloradensis*). Both of these species are listed as federally threatened under the Endangered Species Act (ESA), and are known to occur in wetland riparian habitats along Colorado's Front Range. While the Coal Creek corridor contains general environmental characteristics associated with these species, the soils and quality of habitat on the subject properties is not indicative of that which supports these rare plants.

Ute ladies'-tresses orchid

The Ute ladies'-tresses orchid occurs at elevations below 6,500 feet, almost exclusively in the Front Range of Colorado, in the following habitat types:

- Areas determined to be jurisdictional wetlands,
- Seasonally moist areas near springs, lakes, irrigation ditches, or perennial streams and their associated flood plains,
- Old stream channels and alluvial terraces,
- Sub-irrigated meadows,
- Areas supporting vegetation indicative of seasonally wet areas or areas dominated by vegetation considered to be facultative wet.

Colorado butterfly plant

The Colorado butterfly plant was listed as a threatened species on October 18, 2000; however, the U.S. Fish and Wildlife Service (USFWS) has not yet published formal survey guidelines for this species. The Colorado butterfly plant occurs in north-central Colorado, southeastern Wyoming, and western Nebraska on sub-irrigated alluvial soils on level or slightly sloping floodplains and drainage bottoms between 5,000 and 6,400 feet above sea level. Colonies are often found in low depressions or along bends in wide, active stream channels a short distance upslope of the actual channel. Typical habitats have vegetation that is relatively open and not overly dense or overgrown. On August 6, 2004, the USFWS proposed critical habitat for the species, which in Colorado was confined to only Weld County (USFWS 2004b).

2.2.4. **Wildlife**

The Coal and Rock Creek corridors provide habitat for a broad range of wildlife species. Common mammal species include whitetail deer (*Odocoileus virginianus*), coyote (*Canis latrans*), red fox (*Vulpes vulpes*), striped skunk (*Mephitis mephitis*), raccoon (*Procyon lotor*), fox squirrel (*Sciurus niger*), eastern cottontail (*Sylvilagus floridanus*), American beaver (*Castor canadensis*), and various mice and voles.

Numerous birds utilize the corridor for nesting and foraging habitat, including black-billed magpie (*Pica pica*), European starling (*Sturnus vulgaris*), brown-headed cowbird (*Molothrus ater*), common grackle (*Quiscalus quiscula*), American robin (*Turdus migratorius*), blue jay (*Cyanocitta cristata*), mourning dove (*Zenaidura macroura*), house wren (*Troglodytes aedon*), killdeer (*Charadrius vociferous*), belted kingfisher (*Ceryle alcyon*), great blue heron (*Ardea herodias*), downy woodpecker (*Picoides pubescens*), American goldfinch (*Carduelis tristis*), black-capped chickadee (*Poecile atricapillus*), northern flicker (*Colaptes auratus*), Canada goose (*Branta canadensis*), and mallard (*Anas platyrhynchos*).

Common raptor species identified within the corridor include red-tailed hawks (*Buteo jamaicensis*), American kestrels (*Falco sparverius*), and great horned owls (*Bubo virginianus*). These species are habitat generalists, and are less vulnerable to human impacts if sufficient habitat is available (Jones 1997). A red-tailed hawk nest is located in the northern portion of the Serrano property (Figure 11). A Swainson's hawk (*Buteo swainsoni*) roost was reported on the Haselwood property before 2007. Since that report there have been no further reports. Any roost in the area would be largely unaffected by any proposed changes to the plan as proposed amenities are located some distance from the reported roost site.

A preliminary management plan completed for Flagg Park in the mid-late 1980's noted observations of the following additional avian species: yellow-rumped warbler (*Dendroica coronata*), western meadowlark (*Sturnella neglecta*), red-winged blackbird (*Agelaius phoeniceus*), Brewer's blackbird (*Euphagus cyanocephalus*), house finch (*Carpodacus mexicanus*), and chipping sparrow (*Spizella passerina*). Mammals sighted include the black-tailed prairie dog (*Cynomys ludovicianus*) and common muskrat (*Ondatra zibethicus*).

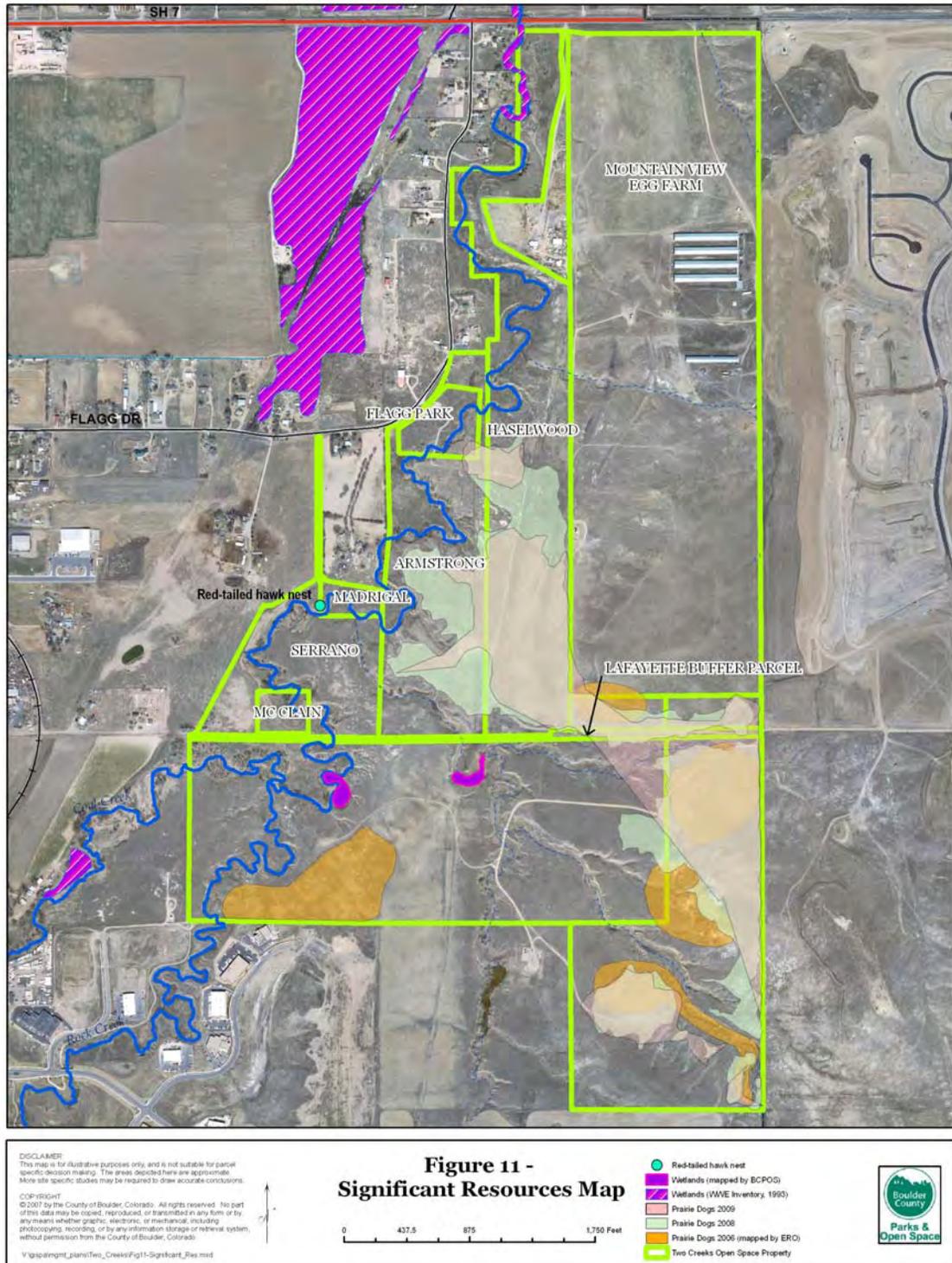
Recent field observations at Two Creeks Open Space yielded the following additional avian species not noted in the above paragraphs: orchard oriole (*Icterus spurius*), Bullock's oriole (*Icterus bullocki*), song sparrow (*Melospiza melodia*), vesper sparrow (*Pooecetes gramineus*), barn swallow (*Hirundo rustica*), northern rough-winged swallow (*Stelgidopteryx serripennis*), horned lark (*Eremophila alpestris*), turkey vulture (*Cathartes aura*), warbling vireo (*Vireo gilvus*), and western kingbird (*Tyrannus verticalis*). Please refer to Appendix 7 for a complete listing of the May 20, 2004 avian observations.

Fish that occur in Coal Creek include such species as fathead minnows, longnose dace, white sucker, creek chub, and green sunfish. These fish can tolerate the changing, low water conditions that are prevalent in plains streams. In a study of St. Vrain Creek tributary streams, the previously mentioned common species made up 86 percent of fish sampled. Uncommon, or environmentally sensitive, fish made up only 2 percent of all tributary species.

2.2.4.1. Threatened, Endangered, and Candidate Species Management

Passed in 1973 and reauthorized in 1988, the Endangered Species Act (ESA) regulates a wide range of activities affecting plants and animals designated as federally endangered or threatened. By definition, an endangered species is any animal or plant listed by regulation as being in danger of extinction. A threatened species is any animal or plant that is likely to become

Figure 11: Significant Resources Map



endangered within the foreseeable future. A candidate species is any animal or plant for which reliable information is available that a listing under the ESA may be warranted. There are no mandatory federal protections required under the ESA for a candidate species; however, it is advisable to voluntarily protect these species. The Act prohibits a number of activities involving endangered species.

No federally protected species occur on the subject properties.

2.2.4.2. Sensitive Wildlife Species

The Coal Creek corridor provides important habitat for several sensitive wildlife species, including the black-tailed prairie dog and several bird species.

Black-tailed prairie dog

Much of the Two Creeks Open Space is appropriate habitat for the Black-tailed prairie dog. Since the properties were acquired populations have fluctuated naturally (Figure 11), been managed through fencing and eradicated in some areas. Since 2007 the colonies have not been directly managed. In 2006, with the sighting of Burrowing owls on the open space it became vital to protect prairie dog habitat to provide habitat for the owls.

Conservation of the species is important because they are an important component of the grassland ecosystem and because agriculture and development pressures along the Front Range continue to displace them. Consequently, open space with appropriate habitat has become increasingly important for preserving viable colonies. Species such as burrowing owl (*Speotyto cunicularia*), prairie rattlesnake (*Crotalus viridis*) and mountain plover (*Charadrius montanus*) use prairie dog colonies for food, cover, or both. Prairie dogs also provide an important prey resource for predators including badger (*Taxidea taxus*), coyote, bald eagle (*Haliaeetus leucocephalus*), ferruginous hawk (*Buteo regalis*), and other raptors.

The County has a Prairie Dog Element in the Boulder County Parks and Open Space Grasslands Management Plan, which addresses prairie dog management on County properties (see Section 3.1.2– Wildlife).

Great blue heron

The Coal Creek corridor provides nesting and/or foraging habitat for a species of local conservation interest/concern, the great blue heron. The Boulder County Nature Association has identified the great blue heron as having isolated or restricted populations (found only at certain locations and/or have narrow habitat niches) within Boulder County. Great blue herons have been sighted regularly on the jointly owned properties.

Burrowing owl

Parks and Open Space employees and volunteers have observed burrowing owl nesting on Two Creeks, during nesting season, since 2005. Prior to 2005, the last burrowing owl was observed in 1986 about 0.25 miles to the northwest of the Two Creeks Open Space (Jones 2004). The burrowing owl is State-listed as a Threatened species. Nests have been observed on the Haselwood, Stephenson-Nelson property and on the Mountain View Egg Farm since 2005. Burrowing owls may not return to the same burrow each year but generally return to the same

area. The strong prairie dog presence on the Mountain View Egg Farm has supported the ability of burrowing owls to persist in the Two Creeks area. Burrowing owl activity has also been reported on Broomfield property to the east in the Anthem development, although some of that habitat is now a housing development.

Swainson's hawk

During the summer of 2004, Swainson's hawks congregated on and around the Two Creeks Open Space. A potential roost site on the Haselwood property requires further evaluation.

Preble's Meadow Jumping Mouse

The Preble's meadow jumping mouse (*Zapus hudsonius preblei*) (Preble's) is a federally threatened species that occurs in habitat adjacent to streams and waterways along the Front Range of Colorado and southeastern Wyoming. The USFWS determined that Preble's habitat generally occurs within the riparian zone, primarily defined by the 100-year floodplain, and adjacent uplands extending out about 100 meters. On December 18, 2003, USFWS determined that the mouse will remain listed as threatened under the ESA after evaluating three petitions to delist the species. The agency conducted a five-year review of the mouse's status in 2009 and proposed critical habitat for the species. Coal Creek was not included and remains unoccupied..

Habitat for the mouse has been identified along Coal Creek west of U.S. 36, but the quality of habitat in the project area is not considered suitable. No positive Preble's captures have been documented in the area. According to the Draft Boulder County Preble's Habitat Conservation Plan, the reach of Coal Creek on the subject properties is classified as Noncontiguous, Potential Restoration (Boulder County 2002).

2.2.5. Water Resources

Coal Creek and Rock Creek define many of the natural features found in the jointly owned properties. The hydrology associated with normal stream processes sustains the riparian and mesic plant systems. Seasonal high water and flooding events also contribute greatly to the diversity and functionality of plant and animal life.

The trend of utilizing water rights to extinction causes concern for sustaining riparian vegetation, wetlands, and wildlife. Should this come to bear in Coal Creek, it may present difficulties in preserving the conservation values of the corridor.

Coal Creek through the Warembourg-Lafayette Farm property contains a functional riparian area with access to bench floodplains. The Creek has a rocky bottom and sufficient bank vegetation.

In Two Creeks Open Space, the creek channels are entrenched and incised in many places and has scoured causing significant erosion. This activity is common for plains streams situated in fine soils. Although incised, the sinuosity of the Creeks and creation of new benches and floodplains is a sign of stream health.

2.3. CULTURAL RESOURCES

The Colorado Historical Society, Office of Archaeology and Historic Preservation (OAHP) conducted a search of the Colorado Inventory of Cultural Resources database for the project

area. This database contains information on documented federal or state studies or findings regarding any cultural resources. According to their search, OAHP identified four sites that occur within the vicinity of the subject properties.

In addition to these documented sites, the Haselwood property also contains a historic railroad bed that has not been formally documented or surveyed. This railroad bed is considered a significant historic resource and potentially eligible for the NRHP based on the presence of other field eligible railroad lines in the project area.

Coal mining activity was very prevalent in the project area during the late 1800s into the early-mid 1900s. Several coal mines underlie the project area, including the Baker Mine, Capitol Mine, Haywood Mine, Pallot Mine, Standard Mine, and Vaughn Mine. According to coal mine subsidence maps, portions of the Standard and Capitol Mines appear to be underneath Two Creeks Open Space (Colorado DNR 1986). These historic resources should be considered potentially significant. For example, the Baker Mine is significant because it is one of the earliest coal mining attempts in the County and because it may be associated with Jim Baker, a prominent frontiersman.

A Class III cultural resource inventory was conducted for 13.3 miles of proposed trail corridor along both Rock Creek and Coal Creek (Native Cultural Services 1989). The study's project area ranged from McCaslin Blvd. east to S. 120th Street. The proposed trail corridor located on the subject properties was not surveyed.

Other potential unidentified cultural resources may exist within the project area. The Boulder County Comprehensive Plan identifies the Coal Creek corridor as a travel route, but does not identify the area as archaeologically sensitive.

2.4. AGRICULTURAL RESOURCES

The Coal Creek valley has long been defined by its agricultural heritage. Early settlers used the water resources of the Creek and fertile soils to produce agricultural goods. Most of the land was used for the production of irrigated grass for hay and pasture. Row crops were also cultivated in some areas.

The Warembourg-Lafayette Farm is leased for agricultural production. The Warembourg-Lafayette Farm property consists of irrigated cropland in five major fields. The northern field is grass that is used mostly for pasture, but is sometimes hayed. The four fields to the south of Coal Creek consist of (described from east to west) wheat, alfalfa, grass/alfalfa, and grass. Previous cropping systems included corn and barley.

Agricultural improvements on the Warembourg-Lafayette Farm property include headgates, diversion structures, field laterals, a stock pond, fencing, and a U-shaped loafing shed/barn (*Figure 4*). The stock pond provides reliable water storage so fields can be irrigated for two to three days. Numerous concrete piles exist on the property that was left over from previous Creek armoring work. The outlet pipe at the eastern end of the stock pond is rusted and has collapsed. Fencing on the northern boundary of the property is in disrepair and invites trespassing from the adjacent recreational trail.

The Serrano property consists of upland grass and is used for pasture. Agricultural improvements include fencing, corrals, and a horse-feeding trough.

The Mountain View Egg Farm, was, until approximately 8 years ago, an active egg production farm. Furthermore, evidence on the site points to agricultural development on the southeastern edge of the property. Currently, there is a grazing lease on the entire property. The lease remains in affect through April 2011 and may be renewed at that time with the agreement of the City and County. The Egg Farm was purchased along with 38 acre-feet of groundwater as well as a 50,000-gallon cistern.

Buildings associated with the egg production operation remain on the Mountain View Egg Farm. Removal of these buildings and associated trash should be carried out in accordance with the purchase agreement between the City of Lafayette and Boulder County. If however, an alternative use for the buildings can be found, it should be considered with respect to impacts on wildlife, conservation values, and grassland restoration.

County agricultural staff work with lessees to develop plans that guide grazing management, crop production, and irrigation management. Ditch maintenance and fencing often require special attention.

2.5. RECREATION FACILITIES

Parks and Open Space

Much of the Coal Creek corridor has been protected as open space from west of the Town of Superior through the City of Louisville and Lafayette. Preservation of these lands as open space provides wildlife habitat, helps to maintain water quality, and allows for recreational access through the corridor. The jointly owned properties are important links in providing continuous trail access and preserving the natural heritage of the area. The Warembourg-Lafayette Farm property is in active agriculture, while the other properties serve as undeveloped natural areas with limited recreational use. The Flagg Park property serves as a community park and contains a parking lot for about eight vehicles, five picnic tables, four benches, and a garbage can.

Trails and Greenways

The Coal Creek-Rock Creek Trail system is the dominant recreational feature in the project area. Both the Boulder County Comprehensive Plan (1999) and the Coal Creek-Rock Creek Trail Corridor Master Plan (1991) identify trail alignments on the subject properties along Coal Creek. The goal is to create a continuous recreational trail through Louisville and Lafayette into Weld County. Currently, the Coal Creek Trail is complete up to 120th Street, which is its eastern terminus. Private property lies between 120th Street and the Serrano property, preventing a continuous ownership pattern and trail corridor along Coal Creek (*Figure 3*).

The purchase of the Mountain View Egg Farm coincided with an effort by the City of Lafayette, Boulder County, and the Town of Erie to complete the eastern link of the Coal Creek Trail through a grant from the Great Outdoors Colorado program. The Mountain View Egg Farm provided an opportunity to move the Coal Creek Trail alignment from the highly erosive banks and sensitive habitat of Coal Creek through the Haselwood property. This opportunity has been

contemplated by trail planners since 2004. The grant was applied for and awarded in 2010, with a spending deadline of 31 December 2012.

Just west of the Warembourg-Lafayette Farm property at Public Road is a trailhead that contains 10 paved parking spaces, a kiosk with various signs, a garbage can, and two benches. The Flagg Park property is approximately 1.5 miles northeast of the Public Road trailhead and will serve as a future trailhead. In 2003, a bridge over Coal Creek was constructed in order for this property to be used as a trailhead for the Coal Creek Trail system. This bridge is rated for use by vehicles and also provides access to the Haselwood and Armstrong properties.

The Warembourg-Lafayette Farm property contains two 100' wide corridors that were reserved for possible future trail use: one on the eastern property boundary that parallels the BNSF rail line and one that parallels the western property boundary (*Figure 4*). However, after the property was acquired, the house lot was sold with the condition that the trail corridor along the western boundary would not encroach on the residential lot. Locating a trail within the adjacent road right-of-way on the west side of the lot was not believed to have been an option since it was thought that it would indeed encroach on the lot. Consequently, the trail corridor was reserved in a fashion that meandered around the lot within the property to ultimately reach the northwest corner of the property.

It is now believed that the City owned right-of-way is actually some distance west of the house lot fence line and allows for a trail without encroaching on the lot. Regardless, the preference of the current lot owner is to have a trail that runs across just the front of his lot rather than around three sides (Kern 2004).

Based on recent discussions with the City of Lafayette, it is anticipated that a future trail will be constructed within the 30-foot road right-of-way to the east of S. 112th Street and Public Road. This alignment runs in front of the house lot but is entirely within the road right-of-way. The objective is to connect any future development south of the Warembourg-Lafayette Farm property with the Coal Creek Trail system at Public Road.

3. MANAGEMENT DIRECTION

3.1. NATURAL RESOURCES

3.1.1. Vegetation

The Coal Creek corridor contains three distinct vegetation community types: riparian, upland grasslands, and various types of wetlands. In general, the riparian communities have an overstory of mature plains cottonwood trees and an understory composed of non-native shrubs and grasses. Upland grasslands on Two Creeks Open Space are in the process of being restored to native shortgrass prairie and contain numerous areas dominated by cheatgrass and other noxious weeds. Wetlands are generally limited to the immediate riparian zone.

3.1.1.1. Riparian Areas

The riparian area at the confluence of Coal and Rock Creek is vitally important for habitat values associated with the Wildlife Preserve management area. Trails and/or bridges in the vicinity of the confluence should be planned and constructed to minimize impact on the riparian habitat.

Removal of Russian-olive, young crack willow throughout the corridor, and a small pocket of tamarisk should be a priority. This will enhance conditions for younger native cottonwoods and willows along the Creek and improve wildlife habitat. Restoration of the shrub layer with native species such as American plum, chokecherry, coyote willow, and golden currant would greatly increase wildlife value and help to prevent bank erosion.

Protection of existing mature trees from beaver activity is needed. New tree plantings should also be protected to ensure their survival.

The Creek is incised and its banks are severely eroded in many places on the subject properties. Although this may be a function of natural channel migration, a few areas may present hazards to visitors or endanger recreational facilities over time. The future alignment of a regional trail on the subject properties should take this into consideration. Stabilization and mitigation needs should be further evaluated. The use of hard materials such as riprap can help anchor the bank and prevent additional scouring. Utilizing bio-engineering techniques in conjunction with hard materials can improve the functionality and benefit to wildlife; however, the depth to groundwater will be a limiting factor for plants and will likely prohibit this practice.

There is some concern over the trend of utilizing water rights to extinction due to its impact to riparian vegetation and wetlands. This issue should be monitored to make sure that adequate water resources are available to support the conservation values of the Coal Creek corridor.

3.1.1.2. Uplands

Restoring uplands on the Two Creeks Open Space is a priority. Weed control and revegetation projects have been underway since 2000 (*Figure 10*). Efforts to establish a competitive stand of native shortgrass prairie should be continued, including ongoing weed control and possible interseeding. Several isolated areas still dominated by cheatgrass and other weeds should be treated and included in the restoration effort. Prescribed fire, weed control, and seeding should

be utilized. Once established, this native plant community will require ongoing weed control, mowing, and prescribed fire to mimic historic environmental conditions.

Short-term management activities on upland areas of Stephenson-Nelson will include intensive weed control. Seeding should not begin until staff can evaluate the progress of existing native species growth in the areas with intensive weed control and grazing control activities. After the effectiveness of the controls is evaluated, seeding the upland area with native species should be considered. Prairie dog management in the upland areas should be sensitive to these restoration efforts and manage colonies in a manner that supports revegetation to the extent possible.

3.1.1.3. Grazing Response

The number, distribution, and timing of grazing animals has an impact on vegetation. Plant communities that are important for riparian functionality and to wildlife, such as bank vegetation, unique wetland vegetation, and upland shrubs, should be monitored for grazing impacts. Vegetative conditions at the Warembourg-Lafayette Farm should be monitored and factored into grazing plans.

3.1.1.4. Noxious Weeds

One of the most serious and fastest growing problems in the West today is the spread and establishment of invasive non-native plants. Noxious weed infestations have contributed to the loss of productivity and ecological functions on both public and private lands, seriously impacting agriculture, native plants and wildlife. Weeds are rapidly becoming the most pressing management issue for many private landowners and public land managers.

Integrated weed management, employing mechanical, cultural, biological, and chemical control techniques, should be used. The most important part of any weed management program is prevention. Weed infestations should be monitored and vigorously addressed to prevent dispersal and the need for future control actions. Certified weed-free materials should be used in recreational developments and reclamation projects. Restoring and maintaining healthy plant communities, in particular grasslands, and reducing human impacts and use patterns can prevent weed invasion. The use of prescriptive grazing can be very beneficial for weed management by allowing animals to graze weeds and reduce competition with native plants.

The establishment of favorable plant species is important for providing competition to weed species. Grass species in particular are important, as they are tolerant of selective weed management practices such as mowing and herbicide applications. In many areas, retaining non-native perennial grass species such as smooth brome and crested wheatgrass provides desirable competition to noxious weeds. Weed management should be performed in conjunction with the goal of establishing a healthy stand of native perennial grass.

The Boulder County Noxious Weed Management Plan (as amended in 2004) provides additional direction and identifies noxious weeds of local concern. Thirteen species are listed as County noxious weeds. According to the plan, diffuse knapweed, Mediterranean sage, Dalmatian toadflax, Canada thistle, musk thistle, and yellow toadflax are the most serious noxious weeds on County lands because of their extent and aggressive nature. Only five of these species have been

noted on properties within the Two Creeks: diffuse knapweed, tamarisk, Canada thistle, musk thistle, and Scotch thistle.

The County should continue to map and monitor noxious weeds. Weed monitoring should occur annually while more intensive mapping should be performed about every five years.

Two Creeks Open Space

Lands east of Coal Creek are currently undergoing restoration, with parts planted to native species, parts planted to a cover crop, and the remaining area slated for restoration in the near future. Within Two Creeks, kochia is the most widespread species. A mid-summer herbicide application over about half (9 acres) the grass-planted area killed the kochia, leaving half the seeded area untreated and with a dense stand of kochia. This will make for an interesting test of the impact of a competitive annual species such as kochia in a restoration process and the effects of herbicides on other species.

Cheatgrass has been controlled over much of the site through a fall 2003 application of Plateau, combined with a spring 2004 prescribed burn. Diffuse knapweed and the thistle species are infrequent and can be controlled by spot spray herbicide applications or hand pulling.

Warembourg-Lafayette Farm

The weed of greatest concern on the property is diffuse knapweed. A spring application of Transline (clopyralid), or any pre-mix herbicide containing Transline, provides very effective control of diffuse knapweed as well as the thistle species. The best recommendation for control of the wide spectrum of weed species present in the northern field is a mix of herbicides with differing modes of action, such as Curtail (clopyralid + 2,4-D) tank-mixed with Escort (metsulfuron). This treatment is labeled for use in a hay meadow/pasture near water. Repeat applications may be necessary until the seed soil bank is depleted. The perennial pasture grasses should show a favorable response to the removal of competitive broadleaf species, and increase in vigor and density. This grass response should prevent or suppress subsequent weed occurrences.

3.1.2. Wildlife

The Coal Creek and Rock Creek corridors provide habitat for a range of wildlife, including some significant species. Although no federally protected species occur on the properties, species that require special management attention include the following:

Black-tailed prairie dog

Prairie dogs on the subject properties are managed according to the County's updated 2005 *Grassland Management Plan, Prairie Dog Habitat Element*. This plan attempts to balance wildlife, ecological, and agricultural resource concerns. Most of the properties in Two Creeks Open Space have been classified as Multiple Objective Areas (MOA), with Non-suitable Habitat (NSH) mainly in the riparian areas and drainages. Areas classified as MOA often require more intensive management. To accomplish grassland restoration, prairie dogs must be excluded from the site until native grasses are established. Once a competitive stand of grass is established, prairie dogs should be allowed to co-exist on the site.

The Flagg Park property is owned by Lafayette and is not addressed in the County's *Prairie Dog Habitat Element*; however, as a managed park site and trailhead, prairie dogs are not compatible, and should be treated as a No Prairie Dog area (NPD).

In the original 2004 Management Plan, the Haselwood property was planned for a future prairie dog receiving site for the City of Lafayette. A mutual understanding and agreement in principle between the City and County for this use had been in place for several years. Both agencies had worked to restore the site's habitat with the intention of relocating prairie dogs to the property once it has a stand of grass adequate to support animal activity. Habitat restoration on the Haselwood property was successful; however, the site is currently occupied by prairie dogs relocating from the east because of massive development activities related to the Anthem Development in Broomfield. This migration into the Haselwood property and Two Creeks may limit or preclude Lafayette's ability to relocate prairie dogs onto this property.

If relocations were to occur, the City and County shall notify adjacent landowners prior to such relocations, according to the guidelines established in the County's Grassland Management Plan, *Prairie Dog Habitat Element*. Once prairie dogs are allowed to inhabit the site, conditions should be monitored and animal density may need to be managed. Barriers may also need to be constructed to prohibit prairie dog migration onto adjacent lands. Given adjacent land uses and the increasing urbanization of lands to the south and east of the site, the management of effective prairie dog habitat will be difficult. The City shall be responsible for any and all management efforts associated with prairie dog relocations. The goal is to retain a viable grassland ecosystem on the site.

Private lands to the south of the Armstrong property appear to provide a conduit for regular migration of prairie dogs onto the property. A prairie dog barrier was installed on the south boundary of the Armstrong and Haselwood properties to prevent further migration; a barrier on the eastern boundary of the Haselwood property may be needed in the future.

As a part of the next *Prairie Dog Habitat Element* update, the Stephenson-Nelson property should be classified as a Multiple Objective Area (MOA), except in the drainage areas, which should be classified as No Prairie Dogs (NPD). The MOA designation will provide flexibility to actively manage prairie dogs in order to meet other management objectives, such as habitat restoration and revegetation.

The Mountain View Egg Farm should be designated as MOA. The area of the property proposed for agricultural management should be designated in the *Prairie Dog Habitat Element* as NPD or a No Prairie Dog area due to the conflicts between agricultural development and prairie dog colonies.

Based on the conditions and productive agricultural resources on the Warembourg-Lafayette Farm, this property is a No Prairie Dog (NPD) area. Agricultural production will be the focus of this property, which is not compatible with prairie dog habitation.

Burrowing owl

The burrowing owl is State-listed as a Threatened species. Nests have been observed on the Stephenson-Nelson property and other properties in the Two Creeks area. Burrowing owl activity has also been reported on Broomfield property to the east in the Anthem development. Broomfield plans on locating a prairie dog colony in the Anthem development directly east of the Stephenson-Nelson property. Broomfield is constructing a large berm to dissuade the prairie dogs from migrating west (Dunleavy, 2007).

Inventory is ongoing to locate the extent of burrowing owl nests on the Two Creeks open space properties, followed by annual monitoring to confirm site fidelity and general activity. This monitoring has detected burrowing owls in every season since 2006.

Prairie dog management activities, such as removal or control, would not be conducted on a specific coterie where burrowing owls have established a nest. Prairie dog management activities would be done during the non-nesting season (October – April) to minimize impacts on areas with known owl activity. CDOW recommends no human encroachment or disturbance within 75 yards of burrowing owl nest sites from April 1 through July 31, although tolerance limits to human activity may vary (CDOW 2007).

Preble's meadow jumping mouse

Although certain properties within the corridor have been previously trapped with negative results and received USFWS clearance, this clearance is valid only for one year from the date of survey. Additional surveys and research may be needed to determine whether or not Preble's have reoccupied an area. The City and County should remain informed of the status of the species in the area.

Should Preble's ever be documented in the project area, habitat can be preserved through protection and enhancement. Excluding grazing from riparian corridors with fencing and incorporating diverse native riparian plantings of forbs and grasses, especially coyote willow, are beneficial practices. Willows can be readily sprigged along the Creek where adequate moisture exists. Ditches that connect to the Creek also provide Preble's habitat. Upland shrubs can be added in patches within about 100 meters of the Creek and/or ditches for use as hibernation sites.

Great blue heron

Cottonwood gallery forests that could serve as potential great blue heron rookeries should be maintained; as well as fish populations in Coal Creek that support heron feeding.

Raptors

Both red-tailed and Swainson's hawks are utilizing the subject properties. Table 1 shows buffer and seasonal restriction offered as guidance from the Colorado Division of Wildlife to protect

Table 1: Raptor Buffer and Seasonal Restriction Guidelines, 2007

Species	Buffer Distance from Nest to Surface Occupancy* <i>(beyond that which historically occurred in the area)</i>	Seasonal Restriction to Human Encroachment
Red-tailed hawk	1/3 mile Note: Some members of this species may tolerate human habitation to within 100 yards of the nest	March 1 to July 15
Swainson’s hawk	1/4 mile Note: Some members of this species may tolerate human habitation to within 100 yards of the nest	April 1 to July 15

*surface occupancy includes both human habitation as well as non-human habitation, examples of which would be oil and gas wells, tanks, roads, tracks, trails, etc.

A raptor survey should be completed before implementing the trail project to confirm nest use. Should nesting activity be verified, construction activity near nesting sites should be postponed until after young have fledged. Once the trail is complete and open to public access, the red-tailed hawk nest may be abandoned and the bird displaced to other nearby suitable habitat. Or, because of the species’ high nest site fidelity and its generalist behavior, it may continue to utilize the site. Some red-tailed hawks tolerate human habitation to within 100 yards of their nest (CDOW 2007). While it is important to protect nest sites, equal attention should be paid to protecting important foraging areas.

Development to the south and east of the project area is influencing the overall ecological function and value of the Two Creeks Open Space. With increasing land development, the relative effectiveness and use of habitat for sensitive species such as the Swainson’s hawk will likely decline. The Swainson’s roost site should be further observed to determine whether or not it is routinely used as a prime hunting location. If it is, then efforts should be made to preserve its function and utility.

3.2. MANAGEMENT AREAS

Management areas define specific zones that are suitable for visitor use, appropriate conservation, and resource preservation. Management area designations for the subject properties are described below and depicted on *Figure 12*.

3.2.1. Recreation/Visitor Use Area

These areas are suitable for public recreation and typically contain developed recreation facilities such as parking lots, trailheads, restrooms, picnic areas, shelters, and interpretive facilities.

These areas can accommodate higher levels of visitor use and density of trails/facilities and often have more evidence of previous human use and impacts.

The Flagg Park property currently contains such facilities and is proposed to be modified and enhanced for use as a regional trailhead for the Coal Creek Trail.

3.2.2. Agricultural Area

These areas include properties where soil and water resources are best utilized, conserved, and managed through sustainable agriculture. Crop production and livestock grazing are common uses. In general, public use is not compatible with ongoing agricultural operations. There are circumstances where visitor use can be accommodated with little or no injury to the agricultural operation, but dispersed recreational use is not appropriate.

The Warembourg-Lafayette Farm currently contains such resources and is proposed to be operated as a sustainable farming operation. The Egg Farm property also includes areas that are appropriate for agricultural uses.

3.2.3. Natural Area

These areas contain resources that are not especially sensitive but contribute to the natural heritage of an area and/or provide valuable wildlife habitat. Conditions of these areas are variable, but they generally contain significant natural values or have potential for restoration of natural ecosystems. "Natural areas" have the dual goal of conserving resources while allowing for compatible recreation, which may include activities such as trail use and interpretation. In order to prevent visitor use impacts to non-target areas, trail corridors and interpretive features should be well planned, delineated, and managed. Policies such as on-trail travel, prohibition of off-trail access, dogs on leash, and nighttime closures are appropriate tools to maintain the conservation values of these areas. "Natural areas" may also be utilized for agricultural uses, but agricultural production is not the focus.

Within the Two Creeks area, all or parts of the following properties are designated as with natural area management: Armstrong, Haselwood, Lafayette Buffer Parcel, Madrigal, Mountain View Egg Farm, and Stephenson-Nelson. Much of the Two Creeks area has been or is planned for native grassland restoration activities. Two Creeks also offers opportunities for trail development.

3.2.4. Wildlife Preserve

The "Wildlife Preserve" management area is a relatively undisturbed area that contains significant wildlife habitat, sensitive natural communities, or other significant natural resources, relative to the surrounding landscape, that are buffered from development and other disturbances. Areas designated as "Wildlife Preserve" are not suitable for through-trails or other intensive public uses. Limited visitor access facilities may be suitable for the purposes of education and interpretation, such as a spur trail.

The management area designation was identified by combining two sensitive habitat areas that were described in the City of Lafayette's 2005 Open Space and Trails Master Plan and in the existing management plan for the jointly owned properties. Lafayette's Master Plan outlined an area around the Coal and Rock creek confluence as a Wildlife Preserve Area. The Serrano

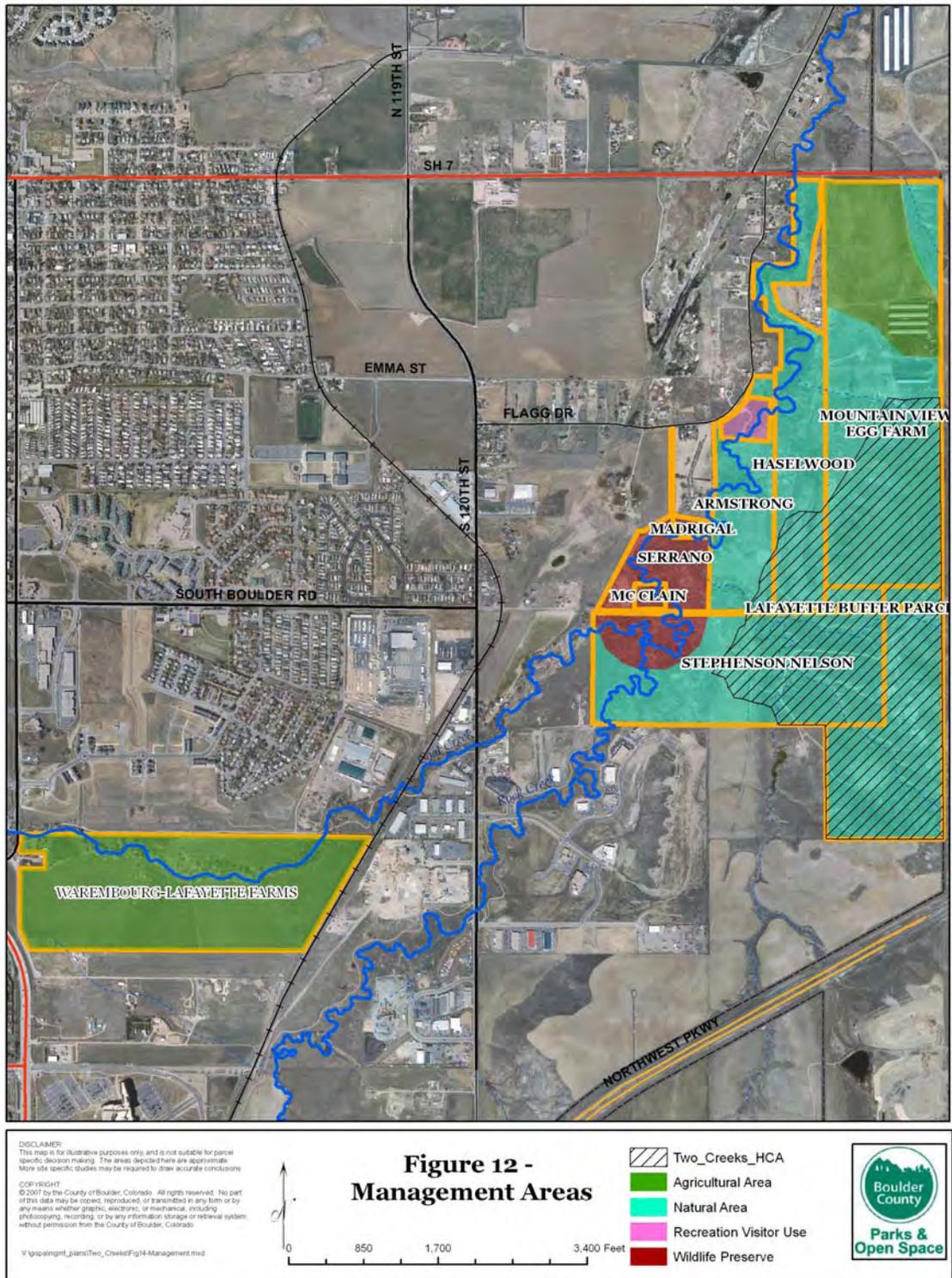
property was identified as having high wildlife values for the riparian area and potential raptor activity in the current management plan, including the current red-tailed hawk nest.

3.2.5. **Habitat Conservation Area**

The Burrowing Owl is a threatened species in Colorado. The prairie dog colonies east of Coal Creek on the Stephenson-Nelson, Mountain View Egg Farm, and Haselwood properties are suitable habitat for the Burrowing Owl. Since 2006 owls have been observed in different areas of each of these properties. In order to differentiate between Natural Areas and areas in which prairie dog colonies will be protected and trail construction will be prohibited, the area shown on Figure 12 will be protected as a Habitat Conservation Area.

The HCA will allow BCPOS to manage the area to promote prairie dogs and burrowing owl habitat. In order to foster both, restoration of the area will be severely limited and access off trail will be prohibited.

Figure 12: Management Areas



CULTURAL RESOURCES

The Coal Creek corridor contains historic and archaeological resources that are remnants of past human influence. A few of these occur on the subject properties and require special attention.

Protection

Cultural resource protection begins with resource identification, primarily through surveys of the built environment and possibly any prehistoric sites. An analysis is then conducted to determine their significance and potential eligibility for the National or State Registers or as a local landmark. Once this assessment is achieved, needs and strategies can be developed based upon the collected information and future use of the site.

Due to the inexact location of identified cultural resources on the subject properties, further investigation is required if any significant disturbance, other than the current use, is proposed. For example, the construction of new trails on the properties should be preceded by a site survey to identify and avoid any significant cultural resources. Particular attention should be paid to the Haselwood property where archaeological resources are known to exist within close proximity of the proposed trail alignment. Similarly, the Serrano, McClain, Madrigal, and Armstrong properties contain segments of Coal Creek where archaeological resources may be present and potential trail development should be initiated with care. Avoidance of any archaeological resources is the preferred action.

Interpretation

Once cultural resources are identified, these resources might provide opportunities for education and interpretation. Should interpretation be pursued, it is important to design access such that the resource is adequately protected and vandalism is prevented. Before access and visitation can be offered, appropriate action (such as fencing and signage) must be initiated in order to ensure public safety. Implementation of the Coal Creek Trail provides a unique opportunity to offer trailside signage that interprets both natural and cultural features. The City and County should coordinate with the County Transportation Department to evaluate and implement interpretive plans for the subject properties. Also, see the topic suggestions for coal mining history under Recreation and Visitor Services section of this plan.

3.3. AGRICULTURAL RESOURCES

County agricultural lands are leased to private operators and are managed for agricultural production. Consistent with current County rules and regulations, these properties are closed to the public. Where appropriate, agricultural lands can accommodate recreational trail development. Should agricultural lands be used to implement the Coal Creek Trail, all efforts should be made to limit the loss of productive land and the impact on agricultural operations, including fencing and irrigation management.

Crop Management

The long-term focus of the Warembourg-Lafayette Farm property is the production of grass for hay and pasture. Vigor and production in several of the fields is declining and they need to be rehabilitated; in particular, the grass field to the far east. The alfalfa fields need to be plowed and rotated into small grains.

Grazing Management on Warembourg-Lafayette Farm

Past grazing management can be described as seasonal grazing coordinated with hay production. Livestock was typically pastured in the fields from winter into early spring, at which time they were removed for production and harvest of hay. Stock was then brought back on the land to pasture after grass dormancy in the fall.

Current grazing practices can be described as adaptive grazing management that is guided by forage production and environmental conditions. Fencing is used by agricultural resource managers to control the timing, number of cattle, and size of pastures in order to control plant utilization and vigor. Growing season grazing requires close management and an adequate rest and recovery period between uses. Grazing can also be used to improve ecological conditions. Prescriptive grazing may be used to manage weeds and promote healthy vegetative conditions. Certain leveled portions of the property are still in hay production to support the grazing lease.

The fence line on the northern property boundary should be replaced, as should the fence line that borders the eastern edge of the field closest to the homestead. The fence line that parallels the eastern boundary fence (which was previously used to delineate a pipeline right-of-way) should be removed and this area should be utilized as part of the farm operation.

The Agricultural Resources Division develops and annually reviews farm management plans with its lessees and works towards improvements in productivity and land condition.

Irrigation Management

Managing and maintaining water conveyance systems is a necessary component of farm and ranchland management. Ditches must be monitored for performance and water loss and often require mowing or burning to eliminate water-consuming vegetation. Head gates and laterals must also be tested and cleaned for proper performance.

At the Warembourg-Lafayette Farm property, several irrigation improvements are needed. Now that the new farm access road has been established, the culvert and field lateral near the access gate should be moved to the south side of the new road. A new structure that is capable of blocking debris from entering the Willis headgate is needed. The outlet pipe in the stock pond needs to be replaced.

Water conveyance into the ditch on the south side of Coal Creek west of Highway 287 is problematic and needs to be evaluated. Perhaps it may be possible to move the ditch diversion upstream enough to improve water conveyance; or it may require a legal change of diversion to remedy the situation.

Market Farm Operations

Boulder County Parks and Open Space has been working with smaller local farm operations for the since 2006 in accordance with the County Commissioners directive to increase local food production. The soils at the Mountain View Egg Farm have been deemed suitable for the development of a Market Farm operation. The 38-acre feet of water purchased with the property also makes this a desirable location for a market farm operation. The development and management of the farm will be in accordance with the practices of Boulder County Parks and

Open Space's Agricultural Resources Division. The area delineated for market farm operations is shown in Figure 12.

3.4. RECREATION AND VISITOR SERVICES

3.4.1. Trails – Two Creeks Open Space

Trail development within the corridor was first established through the Coal Creek-Rock Creek Trail Corridor Master Plan (1991). This document recommended a trail alignment running along Coal Creek during most of its length. In the 2007 revision to this plan, an alternative alignment using the uplands east of Coal Creek from Flagg Park to Highway 7 was indicated as a possibility. With the purchase of the Mountain View Egg Farm the possibility of completing the 10-foot wide multi-use trail from its current terminus at 120th to Highway 7 became a possibility.

In April of 2010, Boulder County, the City of Lafayette, and the Town of Erie received a grant through the Great Outdoors Colorado (GOCO) program to complete the trail from 120th to Vista Parkway in Erie. The partners largely relied on the 1991 plan for the alignment. However, Parks and Open Space and the City of Lafayette developed a series of alternatives to avoid the erosive banks and difficult topography north of Flagg Park and south of Highway 7 (Figure 13).

Egg Farm Trail Options

The purchase agreement for the Mountain View Egg Farm indicates that future uses might include habitat preservation, agricultural opportunities, and recreational trail development. Balancing those different purposes is the challenge of this management plan. Fortunately, the many different purposes reflect the same challenges as exist through the rest of the Two Creeks Open Space.

The team developed six different trail options. The goal of these different options was to minimize impacts on habitat restoration and preservation efforts as well as to limit impacts on proposed agricultural efforts while still linking Highway 7 to Flagg Park.

Feasibility Study

With those limitations in mind the final hurdle to connecting the north and south sides of was how to cross Highway 7. Based on the budget for construction, the planning team discarded the possibility of crossing the road via an overpass. This left two possibilities for crossing: Using the underpass created by the bridge over Coal Creek and Using an existing "cattle crossing" on the northeastern edge of the Mountain View Egg Farm. These two options had been reviewed as part of the revision to the Two Creeks Plan in 2007. At the time a very basic feasibility study found that the Coal Creek crossing was the preferred option.

In the intervening three years, many factors have changed. The Anthem development's increased size and changes to the traffic patterns on Highway 7 led to community concerns regarding the original assessment. In response, staff proposed a feasibility study of the two crossing options. In order to compare the two options the following comparisons were made:

- Environmental Concerns
- Construction Costs
- Topology and drainage issues

- Ownership

Preferred Alignment

The details of the feasibility study can be found in Appendix 9 of this document. To a great extent the result of the feasibility study dictates the preferred trail option through the Egg Farm. Therefore, below is a short conclusion of the feasibility study and the preferred trail alignment (*Figure 13*)

Coal Creek Crossing

The Coal Creek Crossing of Highway 7 would use the existing bridge. The trail would exit the Mountain View Egg Farm on the northwestern corner of the property and use a small amount of road right-of-way in order to reach the Haselwood property west of the Egg Farm. At this location the trail would begin a series of switchbacks in order to get to the bank of Coal Creek. Once at the bank the trail would require a significant amount of engineering to stay at a height that would avoid flooding damage and not significantly impact the riparian corridor. The trail would have impacts on wildlife in and around the crossing but due to the fact that the crossing uses a highway bridge there is already significant disturbance at the site. Construction and use will have long-term impacts on vegetation surrounding the crossing however, many of the construction impacts can be mitigated through replanting.

Cattle-Crossing

The cattle-crossing on the eastern edge of the Egg Farm property has the significant advantage of requiring little to no highway right-of-way to make a trail connection. The trail would enter the drainage gully through a series of switchbacks to get down to the level of the crossing. Once at the level of the crossing the drainage would need to be rerouted into an auxiliary pipe structure that could carry runoff from the Egg Farm and the Anthem development across highway 7. This would allow the cattle-crossing to be used as the trail underpass without fear of flooding and sedimentation. Once on the north side of Highway 7 the trail would need to quickly exit the gully created by the drainage. On the north side the Department would either need to acquire a trail easement or a fee simple right to develop the trail across privately held land in Weld County.

Recommended Alignment

Based on the engineering and environmental feasibility study conducted by the contractor and Boulder County staff, the preferred crossing is Alignment A on the western portion of the Egg Farm (*Figure 14*). Therefore the preferred alternative is the crossing at Coal Creek. The actual alignment and the engineering required to complete the crossing will be verified in the field. However, the general alignment will be followed.

Future Trail Connections

Connecting communities is the goal of the Regional Trail program. The development of an agricultural element on the Mountain View Egg Farm does impact how and when those connections can be made. Boulder County Parks and Open Space and the City of Lafayette will make every effort to work closely with the City and County of Broomfield in order to create

Figure 13: Two Creeks Trail Plan

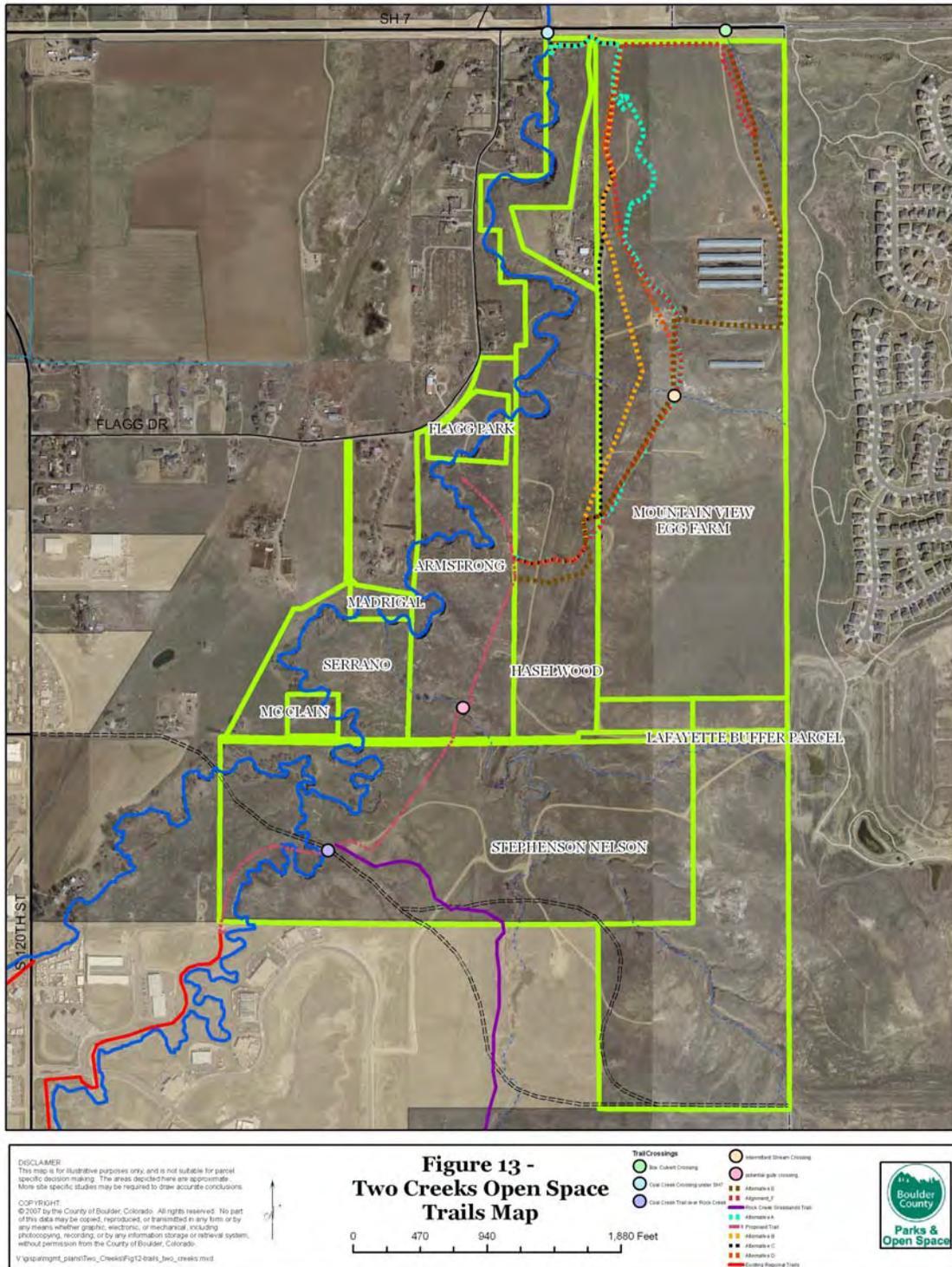
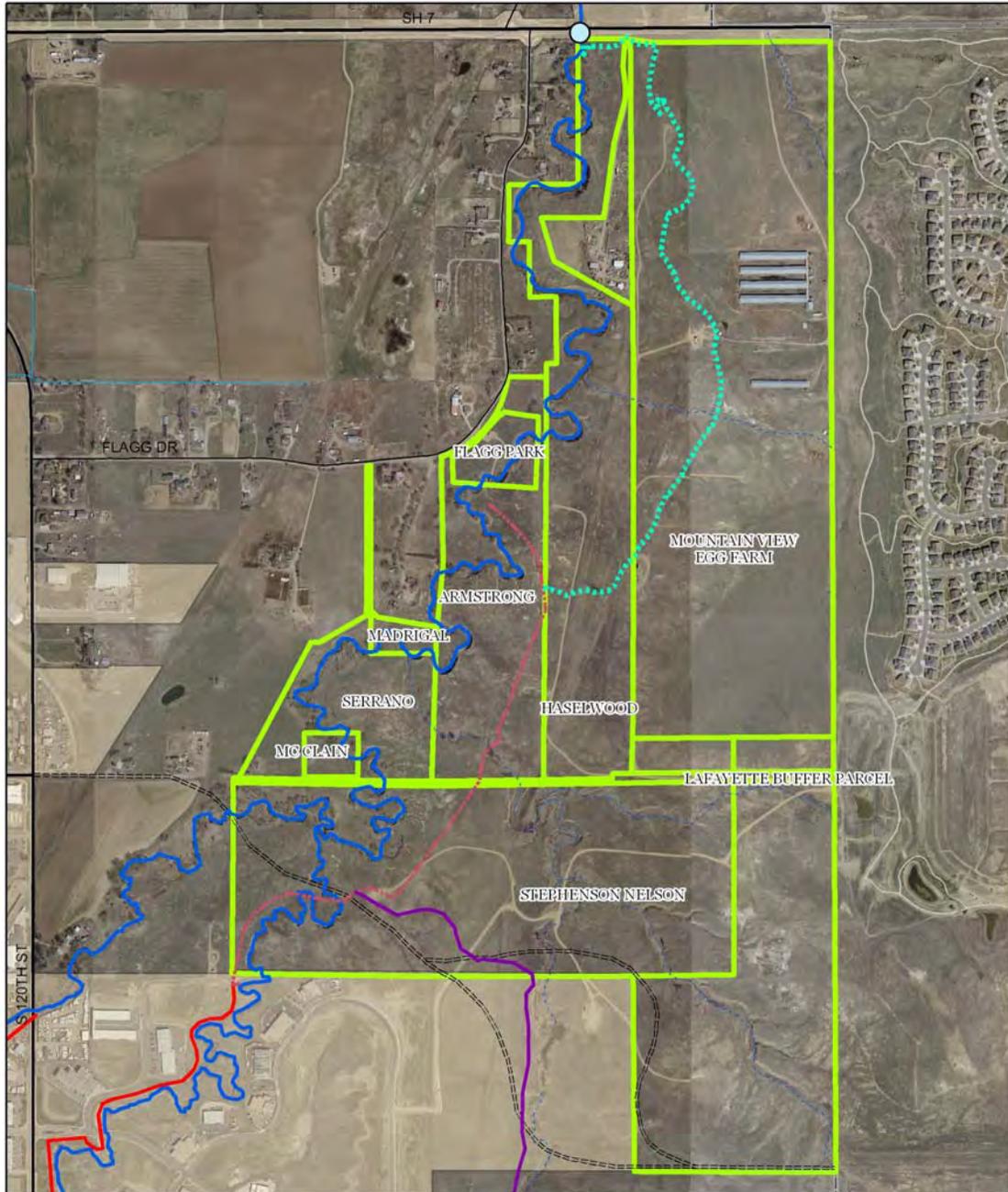


Figure 14: Recommended Alignment

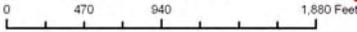


DISCLAIMER
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Figure 14 - Recommended Alignment and Crossing



- Old Creek Crossing under SH 7
- Alignment
- Proposed Trail
- Rock Creek In-streams Trail
- Existing Regional Trails



connections to existing trails when Broomfield determines it would like to become a partner in the ownership of the Mountain View Egg Farm.

Flagg Park

Flagg Park is designated as a regional trail trailhead. The current facilities are inadequate for this purpose and will need to be improved when adequate funding is available. Features that will need to be added to address these issues include:

- Additional parking
- Horse-trailer parking
- Restroom Facilities
- Information Kiosk

Due to issues relating to a former landfill site, the current parking area cannot easily be expanded. Expansion will need to occur north of Flagg Park on the jointly owned Armstrong Property. Any expansion should adhere to Boulder County Parks and Open Space design and development practices.

Proposed changes on the site should be focused on increasing the capacity of the site to give Boulder County residents access to the Coal Creek Regional Trail. As a trailhead at a regional trail, parking should be expanded only enough to meet perceived need. Other amenities should be limited as this is not a destination trailhead but an access point to a regional trail. Prior to development of the site amenities, Boulder County and the City of Lafayette shall develop a Memorandum of Understanding to define roles in managing and developing the site.

Connecting Rock Creek Trail to Flagg Park

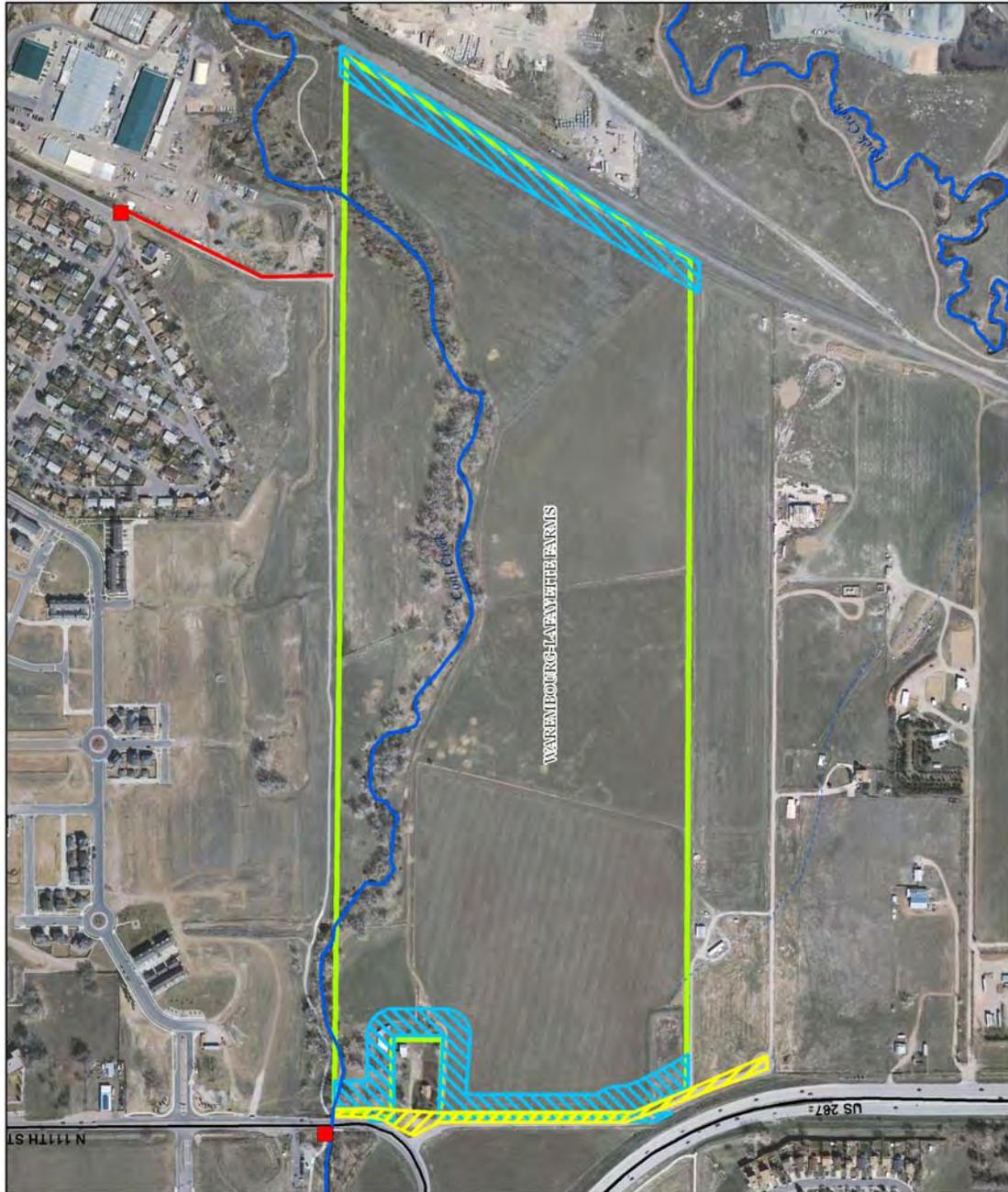
The Rock Creek trail would continue north from the terminus in Vista Business Park, onto the Stephenson-Nelson property and veer east, north of the large bend in the Creek, to cross Rock Creek. The intent with this crossing was to choose a site that is buildable and that coincides with the future South Boulder Road extension crossing. The trail crossing was a recommendation staff brought forward after detailed field reconnaissance allowed staff to evaluate the feasibility of trail construction in this area.

After crossing Rock Creek, the trail would bend east and north between the riparian area and an oil and gas access road. The trail would then traverse north into the Armstrong property, cross the intermittent stream gully and head toward the eastern boundary of the property. The trail would then head north near the eastern boundary of Armstrong and veer west to connect to the existing trail and bridge in Flagg Park.

3.4.2. Trails – Warembourg-Lafayette Farm

Trails in the vicinity of the Warembourg-Lafayette Farm property have the goal of connecting into the Coal Creek Trail system. *Figure 15* shows an alignment that is located within the road right-of-way adjacent to the Warembourg-Lafayette Farm property, which is the preferred route. This alignment parallels the western property boundary and may require a minimal amount of farm acreage to go out of production to accommodate the trail. Trails on or near the property should be initiated with the goal of minimizing impact to the agricultural operations, including

Figure 16: Warembourg-Lafayette Farm – Trails Map



**Figure 15 -
Warembourg-Lafayette Farm
Trails Map**

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0 255 510 1,020 Feet

-  Proposed Trail Corridor
-  Reserved Alternate Trail Corridors 100'
-  Existing Trails
-  Trailheads



irrigation systems. The City and County should also coordinate and work with the house lot owner to implement an alignment that makes the most sense for all parties.

3.4.3. Allowable Uses

Allowable uses on the Coal Creek Trail system are defined according to the rules and regulations of the lead management agency. For the subject properties, this is the County. Southwest of the Two Creeks Open Space, in areas that are within the corporate limits of Lafayette, the City maintains the trail and the City's rules and regulations apply.

- All recreation activity in the Two Creeks Area are restricted to designated trails only, and includes pedestrian activities, dogs on-leash and equestrian uses.
- Equestrian use will be allowed on specific segments of the Coal Creek Trail. Equestrian use will be allowed on Rock Creek Trail and on the recommended alignment shown in Figure 13. Lafayette does not allow equestrian use on their managed Open Space properties, so signage and education will be necessary to make this use work effectively on this section of trail. Equestrian use/activity should be reviewed annually, or at such time that either party shall request such review, to ensure it is working properly.

The goal is to establish a regional trail system that provides for multiple uses and connects Flagg Park with Rock Creek Farm. Once the entire system is in place and serves as a regional trail corridor, the County should assume responsibility for management and maintenance of the trail for its entire length from Flagg Park to Rock Creek Farm. This will facilitate consistent emergency response and allow equestrian use according to the County's rules and regulations.

The County shall be responsible for routine maintenance, but the City and County should consider cost sharing major improvements where the trail traverses annexed City lands. Refer to *Appendix 3* for information on a previous agreement with Lafayette regarding Coal Creek Trail maintenance responsibilities within the city. In the interim, the two agencies should continue to manage their respective portions and provide for maintenance and enforcement.

3.4.4. Visitor Safety

If visitors will be allowed to access previously closed portions of the subject properties, several issues that pose safety concerns need to be addressed. The following list summarizes the major concerns and findings:

- Flagg Park- eliminate access from the south to the steep Creek bank with fencing.
- Flagg Park- repair/replace split-rail fence and install reflectors to warn motorists of the sharp curve.
- Flagg Park- removal of the silt fence from under the bridge.
- Flagg Park and Haselwood- metal and concrete trash should be removed.
- Armstrong and Haselwood- fence the oil and gas wells for visitor safety and to prevent vandalism (in coordination with the oil and gas company).
- Armstrong and Haselwood- removal of the N-S fence that runs along the boundary between the two properties.

- Armstrong and Serrano- removal of garbage and debris (old tires and couches) along Coal Creek.

Fencing is a management tool that can help delineate boundaries, focus access onto open space land from public trailheads, reduce casual access points from private land, control trespass onto private land, and control grazing animals. Fencing may also have adverse impacts on animal movement. In places where fence is constructed for visitor management or resource protection purposes, wildlife-friendly fencing such as smooth wire or post and dowel should be used. Other properties have fencing that is potentially dangerous to visitors and should be removed.

Once the Coal and Rock Creek trail is ready for construction in the Two Creeks Open Space, the following visitor safety concerns should be addressed before opening the trail connection to the public:

- Remove fencing on the north boundary of the Stephenson-Nelson property.
- Require safety warnings and guidelines posted in Two Creeks about the presence, or potential presence of coyotes and mountain lions.
- Describe safety concerns of recreationists using the oil and gas access roads in Two Creeks, primarily the conflict between motorized and non-motorized uses. Using the access roads for recreational purposes would also violate the on-trail use policy.
- Design fencing options along the trail through the Mountain View Egg Farm that maintain livestock control, and visitor safety and ease of use.

3.4.5. Accessibility

Developing facilities that are accessible to persons with disabilities is important and required by federal law. At Flagg Park, an accessible path that connects the main trail with the accessible picnic area should be constructed (*Figure 14*). The Coal Creek and Rock Creek Trails will be built to ADA standards, where feasible.

3.4.6. Interpretation and Outreach

Natural and cultural resources that occur within the corridor offer opportunities for interpretation and education. Themes ranging from riparian and wildlife resources, the Rock and Coal Creek confluence, historic agricultural and mining activities, and the use of fire for site restoration would be good candidates for educational topics. Future construction of the Coal Creek trail through the Two Creeks area should consider creating spur trails and educational signs near the creek, and one spur trail near the confluence.

Implementation of the Coal Creek Trail provides an excellent opportunity for interpretation through trailside signage. Signage should be consistent throughout the trail system.

Outreach efforts should focus on interaction with coyotes and mountain lions, trespass and illegal dumping issues. Signs should be posted informing the public about confronting wildlife. Owners of adjacent residential and commercial properties should be contacted and given information regarding the point of contact for the subject properties.

3.4.7. Volunteer Opportunities

The City and County both offer many volunteer opportunities for interested citizens and organizations, ranging from land maintenance activities to providing services to visitors. Volunteer opportunities include:

3.4.8. Emergency Services

Emergency response is provided by a host of agencies, organizations, and fire protection districts. These activities are initially coordinated through a call to the Boulder County Sheriff's Department, Dispatch Division. From here, depending on the nature of the emergency, appropriate response agencies are contacted.

Law Enforcement

Primary law enforcement responsibility for the subject properties rests with the Boulder County Sheriff's Department, as the properties are located within the unincorporated county. Commissioned Sheriff's Deputies are assigned full-time to patrol open space properties, as are County Open Space Rangers who have limited commissions and enforce County rules and regulations only.

Built segments of the Coal Creek Trail southwest of the subject properties are located within the municipal boundary of Lafayette, and as such, enforcement and emergency response for the trail system is the responsibility of the City. Once the trail is extended east of 120th Street and into the unincorporated county, the County will be responsible for enforcement in those areas. Implementation of the Coal Creek Trail through the subject properties will change law enforcement and resource protection needs and patrol duties should be adapted accordingly. It is expected that the County will be the lead enforcement entity for the trail from Flagg Park all the way down to Rock Creek Farm. Until that time, the two agencies are responsible only for those areas that are under their corporate jurisdiction. However, mutual aid agreements provide for efficient response to the area.

The Warembourg-Lafayette Farm and Serrano properties are under agricultural lease and are closed to the public. Visitation to these properties by law enforcement staff is minimal and patrol is primarily limited to "drive-by" inspections. Law enforcement staff relies heavily on contact and communication from the agricultural operators/lessees to be informed of any problems or potential violations.

The Flagg Park property has a history of vandalism and after-hours drinking that requires maintenance and law enforcement attention.

Each of the properties needs to be assigned a street address for emergency response purposes. Flagg Park is the only property that has an official street address. The County Parks and Open Space Department should work with the County Land Use Department to accomplish this task.

Fire Protection

Fire potential on the subject properties is generally limited to wildland fire, probably in the form of a grass fire. Agricultural ditch burning may also be a consideration. Primary fire protection responsibility rests with the Lafayette Fire Department, as the subject properties fall within its

initial attack jurisdiction. County has staff trained in wildland fire response and can assist with coordination and firefighting resources.

3.4.9. Management Protocol

The City and County should monitor the South Boulder Road extension and the sanitary sewer line and should seek to minimize any impact to open space properties.

3.4.9.1. Rules and Regulations

The following general rules and regulations, which apply to all County properties according to Resolution 2004-101, are applicable to the Jointly Owned Boulder County-Lafayette Open Space:

- Properties that are open for public use are open from sunrise to sunset. Overnight camping is prohibited.
- Collecting, removing, destroying, or defacing any natural or manmade objects within parks and open space is not permitted.
- Discharging or carrying firearms, crossbows, fireworks, or projectile weapons of any kind is not permitted (except law enforcement officials and as allowed by the Board of County Commissioners to carry out a wildlife management program).
- Ground fires are not permitted. Fires may only be built in established grills and fireplaces in picnic areas. Fires may be prohibited entirely by order of the Board of County Commissioners, the Boulder County Sheriff, or the Director of Parks and Open Space by posting of special notices or notification through the press.
- Feeding, disturbing, trapping, hunting, or killing wildlife is not permitted (except as allowed by the Board of County Commissioners to carry out a wildlife management program).
- Motorized vehicles are not permitted (County, emergency, and agricultural lessees on official business are excepted; exceptions may also be granted to persons with disabilities, by written permission from the Parks and Open Space Department, for the use of single-rider, motorized vehicles adapted for recreational use by people with disabilities).
- All dogs or other domestic animals shall be restrained by a leash, cord, rope or chain and under physical control of a person, except as otherwise provided for or specially posted.
- Swimming, diving, wading (except as required for fishing), ice skating, ice fishing or boating (operating a vessel or a single-chambered device) in or on any lake, pond or stream, unless the area is specifically posted to allow such activity, is prohibited.
- Trail right-of-way rules shall be obeyed and trail use by bicyclists shall be limited to officially designated trails and roads only.
- Vehicles must be parked in designated areas only.
- It is unlawful to place rock bolts, install gates, establish or construct trails or other facility for public or private use without the written permission from the Parks and Open Space Department.
- The Parks and Open Space Department may temporarily close areas to public use for repairs or due to wildlife, vegetation, and/or public safety concerns. It shall be unlawful for the public to enter such areas.

- The Board of County Commissioners may adopt use restrictions on trails or park and open space areas for resource protection or public safety. Such restrictions shall be posted and it shall be unlawful to violate these restrictions.
- Open space lands being managed for agricultural purposes, whether by the Parks and Open Space Department or by a lessee, are closed to the public unless trails or other related facilities are designated by the Board of County Commissioners.
- It is unlawful to possess alcohol higher than 3.2% in any park or open space area.
- Geocaching, including the placement or retrieval of geocache items, in any park or open space area is prohibited.
- Activities that unduly interfere with the health, safety, and welfare of the users or the neighbors in the area, or that create a nuisance or hazard to the use and safety or persons using or neighboring such areas are prohibited. Disorderly conduct (including amplified sound) shall be prohibited.
- Fishing is permitted in accordance with the Colorado Wildlife Commission's land and water regulations, except in areas designated and posted with special county parks and open space regulations.

In addition to the general rules and regulations outlined above, the following specific rules and regulations apply to the subject properties:

- All recreation activity in the Two Creeks Area are restricted to designated trails only, and includes pedestrian activities, dogs on-leash, biking, and equestrian uses.
- Equestrian use is allowed on the Coal Creek Trail only north of the signed intersection with Rock Creek Trail. Horses are required to stay on-trail in this segment of Coal Creek Trail through Two Creeks Open Space.
- Equestrian use is not permitted north of the Highway 7 underpass.

3.5. RESOURCE MONITORING

Resource monitoring is conducted to determine if management objectives are being achieved. Monitoring provides information about changes that are occurring on the subject properties and helps inform decisions about future land management activities. The monitoring of specific resources is performed on a periodic basis in relation to resource sensitivity. Some monitoring takes place through routine staff activities, while others take place annually or every few years. The following monitoring activities are recommended for the subject properties:

Prairie dog survey	-----Annual-----	Boulder County Parks and Open Space (BCPOS)
Weed monitoring	-----Annual-----	BCPOS/Lessee
Weed inventory	-----Every 5 yrs.-----	BCPOS
Breeding bird survey	-----Every 3-5 yrs.---	Volunteer
Burrowing owl/Raptor	_____ Annual _____	BCPOS/Volunteer
Riparian plant inventory	-----W/in 5 yrs.-----	BCPOS
Grassland restoration	-----Ongoing-----	BCPOS
Cropland	-----Ongoing-----	BCPOS/Lessee
Flagg Park infrastructure	-----Ongoing-----	BCPOS/Lafayette
Coal Creek Trail maintenance	-----Ongoing-----	BCPOS/Lafayette

3.6. SUMMARY OF MAJOR RECOMMENDATIONS

Property	Action Item
General	<ul style="list-style-type: none"> -Pursue trail easement that allows for continuous regional trail implementation -Conduct cultural resource survey for proposed trail corridor -Design and construct Coal Creek Trail; create/install trailside interpretive signage -Update Grassland Management Plan with recommended prairie dog classifications -Execute an agreement between the City and County concerning mgmt. expenses, etc. that supercedes the reciprocal conservation easement -Obtain official street addresses for all properties -Conduct annual reviews of budget needs and equestrian use -Update management plan every five years
Armstrong	<ul style="list-style-type: none"> -Weed management -Upland restoration -Trash removal -Native tree protection -Fence removal (Armstrong-Haselwood boundary) -Prairie dog management according to the <i>Grassland Management Plan</i> -Map and monitor prairie dog colonies -Russian-olive removal -Posting of boundary signs -Fencing of oil/gas well
Flagg Park	<ul style="list-style-type: none"> -Trash removal -Weed management -Russian-olive removal -Removal of silt fence -Develop trailhead according to site plan
Haselwood	<ul style="list-style-type: none"> -Trash removal -Weed management -Grassland restoration -Prairie dog management according to the <i>Grassland Management Plan</i> -Map and monitor prairie dog colonies; evaluate prairie dog barrier -Notify adjacent landowners prior to prairie dog relocations -Russian-olive and tamarisk removal -Posting of boundary signs -Fencing of oil/gas well

Property	Action Item
Madrigal	-Weed management
McClain	-Weed management -Trash/fence removal
Mountain View Egg Farm	- Trash removal - Remove buildings/Re-use in accordance with Management Goals - Weed management - Construct Trail - Grassland Restoration - Maintain Burrowing Owl habitat -Continue Burrowing Owl monitoring
Serrano	-Weed management -Grassland restoration -Russian-olive removal -Trash removal -Monitor raptor nest -Move southern fence to property line when replaced
Stephenson-Nelson	-Weed management -Grassland restoration -Russian-olive removal -Trash removal -Monitor burrowing owl and raptor activities - Map and monitor prairie dog colonies
Warembourg-Lafayette Farm	-Weed management -Exercise water rights; pay assessments and vote ditch shares -Ditch maintenance; install debris screen on Willis headgate -Install new culvert and field lateral -Replace outlet pipe at stock pond -Evaluate changing the point of diversion on Coal Creek west of Hwy. 287 -Rehabilitation of grass fields -Russian-olive removal -Fence repair/replacement/removal

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Appendix 1: Relevant Goals and Policies

Those **goals in the Boulder County Comprehensive Plan** (as amended, 1999) of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

Environmental Management

- B.5 Wetlands which are important to maintaining the overall balance of ecological systems should be conserved.
- B.6 Unique or critical environmental resources [identified pursuant to Goals B.1, B.3, B.4 and B.5] shall be conserved and preserved in a manner which assures their protection from adverse impacts, with the private sector, non-county agencies and other governmental jurisdictions being encouraged to participate.
- B.7 Productive agricultural land is a limited resource of both environmental and economic value and should be conserved and preserved.
- B.9 Riparian ecosystems, which are important plant communities, wildlife habitat and movement corridors, shall be protected.

Parks and Open Space

- C.3 Open space shall be used as a means of preserving the rural character of the unincorporated county and as a means of protecting from development those areas which have significant environmental, scenic or cultural value.
- C.5 The private sector, non-county agencies, and other governmental jurisdictions should be encouraged to participate in open space preservation and trails development in Boulder County.

Community Facilities

- E.1 Preservation and utilization of water for agricultural purposes within the county shall be encouraged.

Natural Hazards

- L.1 Inappropriate development in natural hazard areas should be reduced as much as possible or eliminated in order to minimize potential harm to life, health, and property.

Agricultural Resources

- M.1 Agricultural enterprises and activities are an important sector of the Boulder County economy and the county shall foster and promote a diverse and sustainable agricultural economy as an integral part of its activities to conserve and preserve agricultural lands in the county.

Those **policies in the Boulder County Comprehensive Plan** (as amended, 1999) of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

Geology

- GE1.01 The county shall strongly discourage intensive uses in Major Hazard Areas.

Natural Hazards

- NH1.04 The level of risk from natural hazards should be reduced through positive county action such as guiding development away from areas prone to natural disturbances, mitigating existing development from hazards, and considering the impact on ability to provide emergency services.

Environmental Resources

- ER2.07 The County shall identify and work to assure the preservation of critical wildlife habitats, Natural Areas, environmental conservation areas and significant agricultural land.
- ER2.08 The County shall use its open space program as one means of achieving its environmental resources and cultural preservation goal.
- ER6.05 Management of riparian areas shall encourage use or mimicry of natural processes, maintenance or reintroduction of native species, restoration of degraded plant communities, elimination of undesirable exotic species, minimizing human impacts, and development of long-term ecological monitoring programs.
- ER8.01.01 The County shall work with landowners and other entities to promote sound conservation practices and, where appropriate, to establish cooperative management plans.

Open Space

- OS2.01 The county shall identify and work to assure the preservation of Environmental Conservation Areas, critical wildlife habitats and corridors, Natural Areas, Natural Landmarks, significant areas identified in the Boulder Valley Natural Ecosystems Map, historic and archaeological sites, and significant agricultural land.
- OS2.03 The County shall provide management plans and the means for the implementation of said plans for all open space areas that have been acquired by or dedicated to the county.
- OS2.03.01 The foremost management objectives of individual open space lands shall follow directly from the purposes for which the land was acquired.
- OS2.03.02 Management of county open space lands shall consider the regional context of ecosystems and adjacent land uses.
- OS2.03.03 Management of individual open space lands, including those under agricultural leases, shall follow good stewardship practices and other techniques that protect and preserve natural and cultural resources.
- OS2.05 The County, through its Weed Management Program, shall discourage the introduction of exotic or undesirable plants and shall work to eradicate existing infestations through the use of Integrated Weed Management throughout the county on private and public lands.
- OS5.01 Boulder County shall, in consultation with affected municipalities, utilize open space to physically buffer Community Service Areas, for the purpose of ensuring community identity and preventing urban sprawl.
- OS5.02 The County shall utilize Intergovernmental Agreements with one or more municipalities to encourage the preservation of open space lands and the protection of the rural and open character of the unincorporated parts of Boulder County.
- OS5.04 The County shall use its open space acquisition program to preserve agricultural lands of local, statewide, and national importance. Where possible, purchase of conservation easements, purchase of development rights, or lease-back arrangements should be used to encourage family farm operations.
- OS8.03 In developing management plans for open space areas, Parks and Open Space staff shall solicit public participation of interested individuals, community organizations, adjacent landowners and the Parks and Open Space Advisory Committee. Plans shall be reviewed by the Parks and Open Space Advisory Committee, including public comment,

and recommended for adoption after public hearing by the Board of County Commissioners.

Agriculture

- AG1.01 It is the policy of Boulder County to promote and support the preservation of agricultural lands and activities within the unincorporated areas of the county, and to make that position known to all citizens currently living in or intending to move into this area.
- AG1.02 The County shall foster and encourage varied activities and strategies which encourage a diverse and sustainable agricultural economy and utilization of agricultural resources.
- AG1.03 It is the policy of Boulder County to encourage the preservation and utilization of those lands identified in the Agricultural Element as Agricultural Lands of National, Statewide, or Local Importance and other agricultural lands for agricultural or rural uses. The *Boulder County Comprehensive Plan Agricultural Element Map* shall include such lands located outside of the boundaries of any municipality or the Niwot Community Service Area.
- AG1.07 The County shall continue to actively participate in state, federal, and local programs directed toward the identification and preservation of agricultural land.
- AG1.11 The County shall encourage that water rights historically used for agricultural production remain attached to irrigable lands and shall encourage the preservation of historic ditch systems.
- AG1.12 The county shall continue to discourage the fragmentation of large parcels of agricultural land and to encourage the assemblage of smaller parcels into larger, more manageable and productive tracts.

Those **goals and policies** identified in the **City of Lafayette's 2003 Comprehensive Plan** that are of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

Open Space

- Goal I.1: Provide a balanced system of open lands, natural areas, wildlife corridors and habitat areas, trails, and greenways using a variety of conservation methods to meet both the needs of Lafayette's citizens and the City's resource protection goals.
- Policy I.1.3: Pursue grants for open space land acquisition, intergovernmental agreements and other mechanisms to leverage funding to create open space buffers between communities.
- Policy I.1.5: Manage open space areas consistent with designated and intended uses.
- Policy I.1.10: Create trail connections through and between Lafayette's open space properties in order to provide an integrated and complete trail system within the community.

Parks and Recreation

- Goal H.1: Provide Lafayette residents with parks, trails, and high quality recreational opportunities while contributing to community identity by establishing an interconnected "green" physical framework.

Environmental

- Goal J.1: Protect the health, safety, and welfare of the citizens through adequately mitigating environmental hazards and by eliminating, reducing, or preventing air, water, light, and noise pollution.
- Goal J.2: Conserve environmental resources to insure the most efficient use of such resources.
- Goal J.3: Preserve and conserve unique or distinctive natural and man-made features in recognition of their irreplaceable character and importance to the quality of life in the City of Lafayette.
- Policy J.1.1: Restrict development in the 100-year floodplain. No structure should be located, constructed, extended, converted, or altered without full compliance with Lafayette's Floodplain Standard.
- Policy J.1.6: Consider the reduction or prevention of inappropriate noise as identified and regulated in the Municipal Code during the development review process or other land use planning.
- Policy J.3.1: Protect from destruction or harmful alteration historic landmarks (as defined by the City's Historic Preservation Ordinance [Section 47-2]), archaeological sensitive areas, and other landmarks as determined by the city. Preservation of these sites will be protected through the planning of compatible surrounding land uses. Proposals with potential adverse impact will be dealt with on a case-by-case basis. Evaluation of impacts may be required as part of the development review process.
- Policy J.3.4: Recognize the importance of agricultural lands outside the Urban Growth Boundary as cultural and natural resources. Support efforts to preserve and protect adjacent agricultural lands through conservation easements, Intergovernmental Agreements (IGA's), participation in the County's Transferable Development Rights program, and other efforts and mechanisms.
- Policy J.3.6: Establish a prairie dog migration program/procedures to require all landowners, both public and private, to address prairie dog colonies when proposed development impinges on existing prairie dog habitat.
- Policy J.3.7: Protect wildlife habitats and wetlands.
- Policy J.3.8: Ensure that proposed development and redevelopment appropriately responds to existing topography to avoid excessive site grading and/or retainage.
- Policy J.3.9: Cooperate with Boulder County in establishing a wetlands management plan to avoid degradation of critical wetlands located within the Planning Area.
- Policy J.3.10: Address wildlife issues as appropriate, such as prairie dog relocation including habitat improvement, criteria for accepting animals, and determining future relocation areas to avoid encroachment issues.
- Policy J.3.11: Incorporate significant wildlife habitat and corridors, community buffers, view corridors, and stands of unique native vegetation as designated by the City into open space reserves.

Those **recommendations identified in the City of Lafayette's Parks, Recreation and Trails Master Plan Update 2003** that are of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

- Recommendation A.5. (Parkland, p. 10) Assure water rights for all water currently available to a parcel to be acquired are included in the acquisition negotiations.

- Recommendation C.1. (Trails, p. 12) The addition of new trails should complete the Coal Creek and Rock Creek “spines” and create a trails *system*, improving the ability of all residents to access existing and future regional trail systems.

Those **recommendations identified in the City of Lafayette’s 2005 Open Space and Trails Master Plan** that are of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

- Completion of the Coal Creek trail to the northeast towards Erie
- Completion of the Rock Creek trail to the south towards Boulder County's Rock Creek Farm
- Rails-to-trails connection between Coal Creek and the Great Park, looping back to Coal Creek north of Highway 7

Those **objectives identified in the City of Lafayette’s 2005 Open Space and Trails Master Plan** that are of particular relevance to the Jointly Owned Boulder County-Lafayette Open Space include:

- Objective 1.1 Open Space Conservation: Preserve and protect lands that benefit wildlife and represent the open space values of the greater Lafayette community.
- Objective 1.2. Inter-jurisdictional Collaboration: Collaborate with Boulder County, the City and County of Broomfield, and the Cities of Louisville and Erie on the acquisition and management of open space areas of mutual interest.
- Objective 1.4. Conservation Tools: Consider ways to pursue open space acquisitions using a variety of tools, including fee title purchases, conservation easements, trail easements, bargain sales, and others.
- Objective 2.1. Land and Resource Management: Develop and update management plans and maps for existing open space properties that are consistent with the original acquisition purposes.
- Objective 2.2. Weed Management: Continue to and implement and evaluate a comprehensive weed management strategy for open space areas and trails.
- Objective 2.3. Agricultural Stewardship: Develop a strategy for the management and long-term stewardship of agricultural lands in the open space system.
- Objective 2.4. Prairie Dog Management: Implement and support the City’s policy for the management, relocation, and, where necessary, removal of prairie dogs on open space lands.
- Objective 2.5. Wildlife Preserve Designation: Establish a Wildlife Preserve designation for portions of open space lands that provide high-quality, rare, or otherwise sensitive wildlife habitat.
- Objective 2.6. Riparian Habitat Preservation: Protect the quality, continuity, and ecological integrity of riparian habitat areas in Lafayette.
- Objective 2.7. Ecological Restoration and Enhancement: Identify and implement opportunities to restore and enhance native plant communities on open space lands.
- Objective 3.1. Trails System: Develop a trails network on open space lands and throughout the City that provides reasonable, environmentally sensitive access to open space resources and community destinations.
- Objective 3.3. Trailheads: Develop additional trailhead facilities to provide access to and information about Lafayette's open space and trails system.

- Objective 3.4. Facility Maintenance: Maintain open space facilities, including trails and trailheads, to ensure user safety and long-term viability.
- Objective 3.5. Dogs on Open Space: Maintain existing dog restrictions on open space lands while supporting creative strategies and facilities that will allow dog owners to enjoy appropriate open space lands without infringing on the enjoyment of others or degrading wildlife habitat.
- Objective 3.6. Permitted Open Space and Trail Uses: Public use of open space and trails should continue to be limited to non-motorized, passive uses.
- Objective 3.7. Off-Trail Use of Open Space: Allow reasonable off-trail use of open space while designing trails and other facilities to minimize the need for open space users to leave designated trails.
- Objective 3.8. Education and Outreach: Increase the awareness and appreciation of the environment, its natural systems, and open space resources through the education and outreach guidance outlined in the Education and Outreach Master Plan.
- Objective 3.9. Historical Interpretation: Increase the awareness of historical sites and structures within the City's open space system.
- Objective 5.1. Good Neighbor Approach: Manage existing open space lands and pursue additional open space land in a manner that is cognizant and respectful of the privacy and rights of neighboring landowners.
- Objective 5.3. Security: Design and manage trails and other open space facilities in a manner that protects the safety and security of open space users and adjacent landowners.
- Objective 5.4. Wildlife Conflicts: Cooperate with adjacent landowners and trail users to minimize wildlife conflicts.

Appendix 2: LOSAC Property Management Guidelines

The following Management Guidelines were provided to Boulder County prior to the adoption of the Management Plan in 2004.

SERRANO PROPERTY

Property Description and History

The Serrano Property is a 20-acre parcel of land purchased from Michael Serrano in 1996. The land had been in the Serrano family since 1958, when it was purchased from Edward C. Arnold. Earliest records indicate that Violet A. Smith owned the property from 1933 to 1954. The property has been used historically as grazing land and is located south of Flagg Drive and east of 120th Street.

The Coal Creek Corridor meanders through the 20-acre parcel. Mr. Serrano maintains a residence on the acreage to the east of the open space. The purchase agreement contains a clause that allows Mr. Serrano to lease back the 20 acres for grazing until 2001. The City has agreed to a two-year extension of the lease. If the lease were not renewed, the cattle would be allowed to access the creek along an easement that Mr. Serrano retained at the time of the sale.

Use

The Open Space Advisory Committee recommends that the Serrano Property be used as a trail connection, riparian corridor and wildlife habitat. As such, the committee recommends that limited improvements be made to this property. Trail connections with the Coal Creek/Rock Creek trail system constructed according to policies established by the Coal Creek/Rock Creek Trail Foundation. Public use is not encouraged until the trail is developed because the property is bordered by either private property or open space (Armstrong property) which provides no public access.

Provisions of Use

In addition to the *General Guidelines for Use of Lafayette Open Space*, the following rules are recommended for the Serrano Property: Due to livestock grazing, pets will be allowed under leash control only. Rules for use of the trail should be consistent with those established for the Coal Creek/Rock Creek Trail System.

Citizen Suggestions

Citizen suggestions for the Serrano property have not been solicited as yet.

Site Preparation for Public Use

Top Priority Recommendations

- Limit grazing where legally possible.
- Monitor grazing impacts and easement use (determine width of easement along creek).

Near-Term Recommendations

- Confer with Boulder County regarding the grazing lease, which is up for renewal in 2001.

Long-Term Recommendations

- Construct connecting segment of Coal Creek/Rock Creek Trail.
- Replace non-native species with native trees according to Division of Wildlife recommendations.

Immediate Maintenance and Management Issues

- Monitor weed populations in or near the livestock easement.

Ongoing Maintenance and Management Issues

- Conduct trail maintenance according to Coal Creek/Rock Creek Trail management policies.

FLAGG PARK

Property Description and History

Flagg Park is a 4.65-acre site, located in east Lafayette, south of Baseline Road, along Flagg Drive. The park is owned by the City of Lafayette, but maintained by Boulder County. Flagg Park serves as a wildlife habitat for species within the Coal Creek corridor. It offers public parking, and several benches for relaxation and wildlife viewing. The park was acquired by the City of Lafayette in 1965. Prior to becoming a park, it was used as a landfill from 1947 through 1972.

Use

The Open Space Advisory Committee recommends that Flagg Park continue to function as a public park, an area for musing, and as a preservation site for wildlife within the Coal Creek riparian zone. It is further recommended that Flagg Park be a future trail connection within the Coal Creek/Rock Creek Trail System of Boulder County.

Provisions of Use

In addition to the General Guidelines for Use of Lafayette Open Space, the following rules are recommended for the Flagg Park Property:

- Pets will be allowed under leash control only.
- Rules for use of the future trail should be consistent with those established for the Coal Creek/Rock Creek trail system.
- No boating, fishing, swimming, or hunting.

Citizen Suggestions

Citizen suggestions for Flagg Park have not been solicited as yet.

Immediate Maintenance and Management Issues

- Monitor weed control, as practiced by Boulder County maintenance crews.
- Clean area of debris, including trashes and glasses.

Ongoing Maintenance and Management Issues

- Maintain future trail according to the Coal Creek/Rock Creek Trail Plan

ARMSTRONG PROPERTY

Property Description and History

The Armstrong Property is a 31-acre parcel, purchased in 1996 from Steven Armstrong. The property is contiguous to the Coal Creek corridor, and is located to the south of Flagg Drive in the unincorporated area of eastern Boulder County. The northern 12 acres were mined for coal in the 1940's and early 50's, creating a potential for irregular settling of the land in this area. The northern portion of the property is also adjacent to a dumpsite that was operated by Lester and Martha Hurst from the mid-1950s until 1970. This former dumpsite is now Flagg Park, a County-maintained open space property.

Use

The Open Space Advisory Committee recommends that the Armstrong Property be used as a trail connection, riparian corridor and wildlife habitat. As such, the committee recommends that limited improvements be made to this property.

Trail connections with the Coal Creek/Rock Creek trail system constructed according to policies established by the Coal Creek/Rock Creek Trail Foundation.

Public use is not encouraged until the property is clearly delineated from adjacent properties.

Provisions of Use

In addition to the General Guidelines for the Use of Lafayette Open Space, the following rules are recommended for the Armstrong Property:

Rules for use of the trail should be consistent with those established by the Coal Creek/Rock Creek Foundation trail system.

Citizen Suggestions

Citizen suggestions for the Armstrong property have not been solicited as yet.

Site Preparation for Pubic Use

Top Priority Recommendations

- Clean up and removal of dumped items in the creek corridor.

Near-Term Recommendations

- Post signs that distinguish open space from private property.

Long-Term Recommendations

- Construct connecting segment of Coal Creek/Rock Creek Trail.

On-Going Maintenance and Management Issues

Trail maintenance, taking into account the Coal Creek/Rock Creek Trail management policies.

McCLAIN PROPERTY

Property Description and History

The McClain Property is 2.3 acres of land located approximately 1800 feet to the south of Flagg Drive and adjacent to the Serrano Property in an unincorporated area of eastern Boulder County. Coal Creek crosses the northeastern portion of the property. It was historically used as a holding area with a barn for horses. The horse barn was demolished in the late 1980's. The property and surrounding properties are currently zoned A (Agricultural) by Boulder County.

Use

The Open Space Advisory Committee recommends that the McClain property, in conjunction with adjacent properties, be used as a trail connection, riparian corridor and wildlife habitat. As such, the committee recommends that limited improvements be made to this property.

Trail connections with the Coal Creek/Rock Creek trail system constructed according to policies established by the Coal Creek/Rock Creek Trail Foundation. Public use is not encouraged until the trail is developed because the property is bordered by either private property or open space which currently provides no public access.

Provisions of Use

In addition to the General Guidelines for Use of Lafayette Open Space, the following rules are recommended for the McClain Property:

- Pets allowed under leash control only, as per general guidelines.
- Rules for use of the trail should be consistent with those established for the Coal Creek/Rock Creek trail system.

Citizen Suggestions

Citizen suggestions for the McClain property have not been solicited as yet.

Site Preparation for Pubic Use

Long-Term Recommendations

- Construct connecting segment of Coal Creek/Rock Creek Trail.

- Post signs that distinguish open space from private property.
- Replace non-native plant species with native species according to Division of Wildlife and Boulder County recommendations.

On-Going Maintenance and Management Issues

Trail maintenance according to Coal Creek Rock Creek Trail management policies.

HASELWOOD PROPERTY

Property Description and History

The Haselwood property consists of 59 acres. A 5.68-acre conservation easement lies to the adjacent east. The land and easement were purchased jointly with Boulder County in April of 1998. The property is located south of Baseline Road and east of Flagg Park in eastern Boulder County. The western boundary of the property generally follows Coal Creek. It extends for approximately one mile, north south, and varies from 150 to 660 feet in width. The property was mined for coal during the 1940's and 1950's. During the 1950's and 1960's, the Hurst family maintained a dumpsite in the vicinity of the property. It has remained in rural residential and agricultural use since the early 1980's. The Haselwood property, and surrounding private property, are zoned Agricultural by both Boulder and Adams Counties. A residence, several outbuildings and other structures are located on the conservation easement. The US Fish and Wildlife Service has mapped potential wetland areas along Coal Creek through the property.

The Haselwood property has been used for various agricultural, residential and industrial purposes over the past 90 years. Historic uses of the property include: livestock grazing; agricultural facilities and structures; railroad line; coal mining; oil and gas exploration; residential development; and dumping of agricultural, residential and mining waste.

There is limited public access available to the property. Coal Creek and its embankment form a considerable barrier on the western perimeter of the property. Adjacent private property precludes access to the property from the south and east. It is possible to access the property from Baseline Road, however, the property boundary is not recognizable, and such access may be unsafe due to high traffic speeds and volumes on the road.

Use

The Open Space Advisory Committee recommends the property be maintained as a riparian habitat, prairie dog relocation site, and future connection for the Coal Creek Trail and the Weld County Legacy Trail.

Provisions of Use

- Public use currently is not recommended.

Citizen Suggestions

Citizen suggestions for the Haselwood property have not been solicited as yet.

Site Preparation for Pubic Use

Top Priority Recommendations

- Work with Prairie dog relocation consultants to develop prairie dog plan
- Work with Boulder County toward resolution of access issues.

Near-Term Recommendations

- Research oil well production schedule, reclamation standards, Gerrity emergency plan, and impact of, drainage from the chicken farm.

Long-Term Recommendations

- Construct segment of the Coal Creek Trail.
- Post signs denoting public property.

Immediate Maintenance and Management Issues

- Refer to General Guidelines for Maintenance and Management of Lafayette Open Space.
- Control weeds according to policies of the City of Lafayette Parks and Recreation Department and Boulder County Open Space.

On-Going Maintenance and Management Issues

- Monitor vegetation and prairie dog population
- Strictly enforce codes regarding dumping and polluting.
- Study of oil and mineral rights issues may dictate future maintenance and management.

MADRIGAL OPEN SPACE

Property Description and History

Madrigal Open Space is a 1.57-acre property located in eastern Lafayette, north of the Armstrong property, East of the Haselwood property, and south of Flagg Park. It was purchased from Jane Mireles Madrigal in August of 1998.

Use

The Open Space Advisory Committee recommends that Madrigal Open Space be used as a trail connection in the Coal Creek/Rock Creek trails plan, a riparian corridor and wildlife habitat.

Provisions of Use

- See the General Guidelines for Use of Lafayette Open Space.

Citizen Suggestions

Citizen suggestions for the Madrigal Open Space property have not been solicited as yet.

Site Preparation for Pubic Use

Long-Term Recommendations

- Construct connecting segment of Coal Creek/Rock Creek Trail.

Immediate Maintenance and Management Issues

Manage noxious weeds.

WAREMBOURG PROPERTY

Property Description and History

The Warembourg property is a 110-acre parcel located in Boulder County, south of Coal Creek and east of Public Road. The Warembourg property was purchased as open space by Lafayette and Boulder County in July of 2003 for a total of \$1,988,000. It provides riparian wildlife habitat by Coal Creek and serves as a buffer between developments within Lafayette city limits.

Use

The Lafayette Open Space Advisory Committee recommends that the Warembourg property continue to be used for agricultural purposes consistent with agreements with the joint tenants. The committee recommends the following limited improvements be made to this property:

- Monitor the property for noxious weeds.
- Incorporate the property into long range planning.
- Consider placing signage on the property.
- Public use is not encouraged until the property is no longer agricultural.
- Trails in corridors identified in the purchase agreement.

Provisions of Use

- The General Guidelines for the Use of Lafayette Open Space should apply to the Warembourg Property, although some adjustments may be appropriate given an identified need and subsequent coordination with Boulder County.

Citizen Suggestions

Citizen suggestions for the Warembourg Property have not been solicited yet.

Site Preparation for Public Use

- Signage

Immediate Maintenance and Management Issues

- Monitor noxious weeds

Ongoing Maintenance and Management Issues

- Coordination with Boulder County

Appendix 3: 1998 Coal Creek Trail Maintenance Agreement

COAL CREEK TRAIL Lafayette and Boulder County Maintenance Agreement

THIS AGREEMENT made and entered into by and between the City of Lafayette a Colorado municipality, hereinafter referred to as "City", and the County of Boulder, a body corporate and politic, hereinafter referred to as "County".

WHEREAS, the City and the County, along with the Urban Drainage and Flood Control District (UDFCD) have constructed a flood control maintenance road and trail along Coal Creek; and

WHEREAS, a portion of the flood control maintenance road and trail along Coal Creek is constructed on property within the City and that portion is hereinafter referred to as the "Trail"; and

WHEREAS, the Trail has been constructed primarily with a granular surface according to mutually agreed upon design guidelines; and

WHEREAS, the City and County desire to assign the responsibility for on-going maintenance of the Trail and the Trail Corridor as defined below

NOW, THEREFORE, the City and the County agree as follows;

1. Definition of Trail Corridor: A portion of the flood control maintenance road and trail along Coal Creek is constructed on property within the City and that portion is hereinafter referred to as the "Trail"; and the Trail Corridor is defined as the area including the creek, the Trail/maintenance road and an additional buffer area at least four feet in width or greater as determined by on the outermost side of both the creek and the Trail, but only to the extent that the buffer area consists of land owned by the County and/or the City.
2. It shall be the responsibility of the City and the County to protect the natural and cultural resources within the Trail Corridor. The City and the County shall be responsible, at their sole expense, for management and maintenance of the entire trail within the City's jurisdiction as defined in attached Exhibit "A"
 - a. The City may request that UDFCD perform maintenance along Coal Creek and of all flood control related facilities within or in proximity to the creek.
 - b. An annual inspection will be performed by representatives of all participating parties in order to prepare a CC/RC Trails Capital Construction Budget for the following year. This inspection should be completed by May 15th and the CC/RC Trail Coordinator will develop and submit an appropriate budget request.
3. When new Trail segments have reached a stage of significant completion all parties

will meet with the contractor and develop a punch list. Once this punch list has been successfully completed and the project accepted, management and maintenance responsibilities will be assumed by City.

4. Neither the City nor the County shall make any alteration, addition or improvement to the Trail or Trail Corridor without prior written consent of the other party.
5. Trail improvements which are not part of the original construction project and are added later shall be subject to the provisions of this Agreement, in its present form or as it may be amended in the future.
6. The Trail shall remain accessible at all times by maintenance and emergency vehicles from Lafayette, Boulder County and Urban Drainage and Flood Control District.
7. Apportionment of capital expenditures for replacement of major structures between or among the City of Lafayette, Boulder County and Urban Drainage and Flood Control District will be determined on a case by case basis as the need arises.

Third Party Beneficiary.

The enforcement of the terms and conditions of this Maintenance Agreement and all rights of action relating to such enforcement shall be strictly reserved to the County and the City, and nothing contained in this Maintenance Agreement shall give or allow any claim or right of action whatsoever by any other or third person. It is the express intent of the parties to this Maintenance Agreement that any person receiving services or benefits under this Maintenance Agreement shall be deemed an incidental beneficiary only.

Governmental Immunity Act.

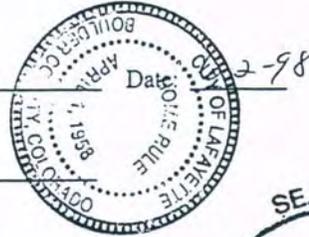
Each party retains the benefits which are provided to the City and The County under the Governmental Immunity Act, §24-10-101, et seq., C.R.S.

IN WITNESS WHEREOF, the City and the County have executed this Agreement on the date set forth opposite their respective signatures.

CITY OF LAFAYETTE

By: *Charles J. McGee*
Mayor

ATTEST: *Laurie Andersen*
City Clerk



COUNTY OF BOULDER

By: *Ronald K. Stewart*
Ronald K. Stewart, Chair of Board of County Commissioners

ATTEST: *Paul Messerschmidt* 6-18-98
Clerk to the Board

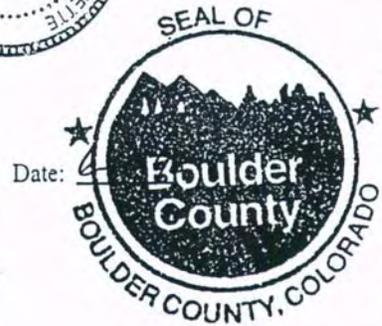


Exhibit "A"

COAL CREEK TRAIL

Lafayette and Boulder County Maintenance Agreement

Tasks	Lafayette	County	Other
Enforcement	Enforce all trail rules and regulations	Limited with P&OS Law enforcement Specialist and S.O. Officers	
Encroachments	Remove any encroachment that interferes with City operations or creates a liability issue	None	
Mowing	Mow to a standard height on a regular basis, as defined by Lafayette	None	
Trash	Provide trash containers. Removal of trash in those containers as well as debris along the trail corridor.	Removal of significant debris associated with annual trail inspections	
Weed Control	Mechanical and manual means; occasional spot chemical applications	Broad chemical applications across the entire trail corridor where appropriate	
Drainage	Routine maintenance and upkeep of existing drainage improvements including culverts, grates and concrete pans	Annual inspections	Capital recommendation for additional improvements or replacements: UDFCD, County & Lafayette

Exhibit "A"

COAL CREEK TRAIL

Lafayette and Boulder County Maintenance Agreement

Tasks	Lafayette	County	Other
Enforcement	Enforce all trail rules and regulations	Limited with P&OS Law enforcement Specialist and S.O. Officers	
Encroachments	Remove any encroachment that interferes with City operations or creates a liability issue	None	
Mowing	Mow to a standard height on a regular basis, as defined by Lafayette	None	
Trash	Provide trash containers. Removal of trash in those containers as well as debris along the trail corridor.	Removal of significant debris associated with annual trail inspections	
Weed Control	Mechanical and manual means; occasional spot chemical applications	Broad chemical applications across the entire trail corridor where appropriate	
Drainage	Routine maintenance and upkeep of existing drainage improvements including culverts, grates and concrete pans	Annual inspections	Capital recommendation for additional improvements or replacements: UDFCD, County & Lafayette

Tasks	Lafayette	County	Other
Re-surfacing	Routine maintenance and upkeep of existing surface: including minor surface replacement, eliminating low spots, and grooming trail.	Annual inspection and drag trail one time per year	Capital recommendation for additional improvements or replacements: UDFCD, County & Lafayette
Fencing and Gates	Routine maintenance and repair of all adjacent fencing, gates and bollards.	Annual inspection	Capital recommendation for additional improvements or replacements: UDFCD, County & Lafayette
Bridges & Decking	Routine maintenance and repair associated with preventive maint, including removal of graffiti, replace decking as needed, painting of bridges, replacement of reflectors	Annual inspection	Capital recommendation for additional improvements or replacements: UDFCD, County & Lafayette
Benches	All maintenance associated with replacement and repairs.	None	
Limbs and Trees	Removal of hazardous tree adjacent to trail and any down trees or limbs that affect the trail	Annual Clearance Trimming	UDFCD will remove trees and limbs that impact or alter the flow of the stream

Tasks	Lafayette	County	Other
Signage -Sign plan to be agreed upon by all parties	Maintain existing signs and posts. Inform County of need for replacement. Responsible for all posts and installation	Will provide original and replacement signs	
Inspections	Routine documented inspections, as determined by the city of Lafayette	Annual documented inspections	**

** An annual inspection will be performed by representatives of all participating parties in order to prepare a CC/RC Trails Capital Construction Budget for the following year. This inspection should be completed by May 15th and the CC/RC Trail Coordinator will develop and submit an appropriate budget request.

Appendix 4: Summary of *Grassland Management Plan, Prairie Dog Habitat Element*

The plan establishes prairie dog habitat designations and provides management direction. The plan was first adopted by the Board of County Commissioners in May 1999 and a recent update was adopted in May 2002. The plan attempts to strike a balance between the sometimes-conflicting goals contained in the County Comprehensive Plan. In particular, the plan seeks to balance wildlife habitat protection goals and goals for preserving agriculture in Boulder County. The Prairie Dog Habitat Element reflects the values and vision of a broad cross-section of county residents, describes the main strategies for achieving the vision, and serves as a decision making guide for property-specific management plans.

Prairie Dog Management Categories:

The plan delineates the areas within the county open space system that contain the most suitable prairie dog habitat and areas that are not suitable habitat by virtue of their ecological characteristics or land uses. It lays out the parameters for maintaining appropriate habitat and guidelines for removing prairie dogs from unsuitable areas.

Three management categories are utilized: Habitat Conservation Area (HCA), Multiple Objective Area (MOA), and No Prairie Dog (NPD). HCAs will ideally allow prairie dogs to function with minimal human intervention without causing or experiencing significant negative impacts to or from adjacent land uses. HCAs will be managed so that prairie dogs may undergo natural processes of expansion and decline and thus fulfill their ecological function. Natural shifts in vegetation dominance and animal use will occur. These areas ideally will have appropriate soils, vegetation, slope, natural or man-made barriers and sufficient acreage to support healthy prairie dog colonies and associated species over time.

Multiple-objective areas will allow prairie dogs to coexist with other uses but they may not be the highest management priority. MOAs are important in the overall prairie dog management strategy as a complement to HCAs. Some MOAs will function as important links between HCAs throughout the county to maintain a viable metapopulation of prairie dogs. This is an important ecological consideration that will allow for reestablishment of colonies should they be decimated by plague. MOAs will support associated wildlife species outside of HCAs. MOAs will have a combination of management goals and require a more intensive management regime. Examples of MOAs are properties with noxious weed or soil erosion problems, or properties that contain suitable habitat but are simply too small to allow the kind of hands off management afforded by an HCA.

NPD areas are not appropriate for prairie dog habitation because of unsuitable ecological conditions or existing agricultural uses. The goal is to remove prairie dogs from these properties.

Prairie Dog Management Activities:

The County will prioritize areas for removal of prairie dogs. Proposed prairie dog removal priorities and strategies will be presented to the County's Parks and Open Space Advisory Committee and the Board of County Commissioners in an annual update, along with a progress report of the previous year's activities.

The County will evaluate the feasibility of relocation as the preferred removal option, and extermination will be used as a last resort. When extermination is necessary, the County will use the most humane method available, applicable permits and clearances will be obtained, and appropriate procedures will be followed to minimize damage to non-target species.

Predator Recovery Program Contributions- If it is not feasible to relocate any more prairie dogs from NPD or MOA sites to HCA locations, contribution of prairie dogs to several local predator recovery programs will be initiated. This will be done if populations on HCA sites are at or exceed the 25% occupancy level of available habitat.

Removal and Control in HCAs- Removal or control of prairie dogs in HCAs would be considered only if necessary to protect the underlying habitat. This might be necessary in HCAs if prairie dog populations exceed guidelines for healthy burrow densities or extent of occupation. Given current conditions with 5,043 acres of HCAs containing over 1,100 acres of prairie dogs, and considering the pattern of plague epizootics every five to 10 years, it is unlikely that prairie dog removal will be necessary on HCAs. However, the county prefers to keep this management strategy as an option in the event that these situations do occur. Buffer zones will be established around HCAs to minimize conflicts with adjacent landowners. Buffer zone boundaries will be established according to each property's unique circumstances, but will be a minimum of 50 meters and no more than 10% of the total HCA area. In all cases, the feasibility of relocation will be investigated as the preferred removal option.

Removal and Control in MOAs- Circumstances that might call for prairie dog removal include: prairie dog population encroachment into reclamation/revegetation areas, recreation areas or portions of the property that are under agricultural uses; colony density or extent of occupation exceeding optimal levels; conflicts with other management priorities such as prevention of soil erosion or eradication of noxious weeds; and conflicts with adjacent landowners. Each of these properties has its own unique circumstances and will be managed accordingly. In all cases, the feasibility of relocation will be investigated as the preferred removal option.

Removal and Control in NPDs- The goal is to remove prairie dogs from all No Prairie Dog areas. Once prairie dogs are removed from these areas, prevention strategies will be used to prevent their return.

Relocation- Potential prairie dog relocation sites will be evaluated for ecological suitability and potential land use conflicts. The County will notify adjacent landowners and take reasonable measures to mitigate land use conflicts in advance of relocations. The County will consider accepting prairie dogs from other public agencies and private property owners on a case-by-case basis after the management plan has been implemented on County owned lands.

Appendix 5: Relevant Provisions from IGAs

Broomfield IGA

County shall permit access to open space properties to City and County of Broomfield residents upon the same terms and conditions as Boulder County residents.

Boulder County shall convey to Broomfield a conservation easement on all open space properties located south of 40.00 degrees North Latitude (Baseline Road) and east of Highway 93. With respect to those open space properties that are jointly owned, Boulder County shall endeavor to convey a joint conservation easement with the other owner. If this is unachievable, Boulder County shall nevertheless convey a conservation easement effective as to its interest in the property.

Regarding policies and plans proposed for the above-mentioned open space properties, Boulder County shall notify in writing and confer with Broomfield prior to adopting such policies and plans. If and when future trails or recreation facilities are planned for these properties, the two entities shall consider joint funding.

Appendix 6: Vegetation Community Definitions

(for Figures 8 and 9)

Irrigated Cropland (IC): Irrigated grassland defined by introduced species such as smooth brome, fescue, wheatgrass, or alfalfa that is used for the production of hay and/or pasturing of livestock.

Riparian (R): A zone that contains water-dependant plant species, including rushes and sedges, and typically parallels streams, ditches or other water features.

Riparian Forest (RF): An overstory plant community defined by tree species such as plains cottonwood, peachleaf willow, crack willow, black locust, and Russian-olive that is associated with stream systems or well-defined ditches. Shrubs include snowberry and golden currant.

Reclaimed Upland Grassland (RUG): A zone that was reclaimed with native plant species such as side oats grama, blue grama, buffalograss, western wheatgrass, fringed sage, and rabbitbrush.

Upland Grassland (UG): A grass community defined by native and introduced species including crested wheatgrass, blue grama, smooth brome, Russian wild rye, cheatgrass, or fescue. Some areas host scattered stands of needle-and-thread grass, yucca, and prickly pear. Wetter areas within this zone may contain shrubs such as snowberry and wild rose.

Wetland (W): An area at least occasionally inundated by water that supports water-dependant species such as cattails, arctic rush, reed canarygrass, spikerush, and hoary cress.

Weedy Upland Grassland (WUG): An upland grass community that contains significant concentrations of weed species such as cheatgrass, musk thistle, diffuse knapweed, or kochia.

Appendix 7: Avian Observations

Two Creeks Open Space (formerly Flagg Park Complex)- May 20, 2004
(D. Morris- Boulder County Parks and Open Space)

<u>Species</u>	<u>Number</u>	<u>Location/Habitat Type</u>
Orchard Oriole	1	Riparian
Barn Swallow	1	Riparian
Chipping Sparrow	1	Riparian
Downy Woodpecker	1	Riparian
Turkey Vulture	1	Riparian/Upland
Swainson's Hawk	1	Roosting on 50' stream cut on Haselwood property; Basic 1 plumage, 1-yr. old
Warbling Vireo	1	Upland
Red-tailed Hawk	2	Nest located on Serrano property
Great-blue Heron	2	Riparian
Bullock's Oriole	2	Riparian
Yellow Warbler	2	Riparian
Northern Flicker	2	Riparian
Horned Lark	2	Upland
Black-billed Magpie	3	Riparian/Upland
House Wren	3	Upland
House Sparrow	4	Riparian/Upland
Mallard	4	Riparian
Red-winged Blackbird	5	Riparian
American Robin	5	Riparian/Upland
Vesper Sparrow	5	Upland
Western Kingbird	7	Upland
Song Sparrow	8	Riparian
Northern Rough-winged Swallow	10	Riparian/Upland
Brown-headed Cowbird	12	Riparian
Common Grackle	15	Riparian
Western Meadowlark	23	Upland
Mourning Dove	34	Riparian/Upland
European Starling	70	Riparian/Upland

Appendix 8: Plan Advisory Team

Boulder County Parks & Open Space

Summer Adamietz, Natural Resource Planner and Project Manager (2007 Plan Amendment)

Patrick Malone, Natural Resource Planner and Lead Author (2004 Plan)

Ron Stewart, Director

Ben Pearlman, Special Projects Manager (2004 Plan)

Rich Koopmann, Manager, Resource Planning Division

Kristi VanDenBosch, GIS/GPS Technician

Luke Stromquist, Manager, Agricultural Resources Division

Rob Alexander, Agricultural Resource Specialist

Therese Glowacki, Manager, Resource Management Division

Tim D'Amato, Former Weed Management Coordinator

David Bell, Lead Ranger

Mark Brennan, Wildlife Specialist

Dave Hoerath, Wildlife Specialist

Claire DeLeo, Senior Plant Ecologist

Brent Wheeler, Manager, Park Operations Division

Kevin Lyles, Landscape Architect

Al Hardy, Trails Supervisor

Boulder County Transportation

Tim Swope, Alternative Modes Coordinator (2007 Plan Amendment)

Scott Robson, Former Coal Creek-Rock Creek Trail Coordinator (2004 Plan)

City of Lafayette

Monte Stevenson, Director of Parks, Recreation & Golf

Rod Tarullo, Former Director of Parks, Recreation & Golf (2004 Plan)

Judy Wolfe, Former Senior Administrative Assistant (2004 Plan and 2007 Plan Amendment)

Appendix 9: Feasibility Study

The Feasibility Study can be obtained as a separate document.

Appendix 10: Public Comment

Appendix 10 can be obtained as a separate document.



Parks and Open Space

5201 St. Vrain Road • Longmont, Colorado 80503

To: Mountain View Egg Farm Planning Team

From: Jesse Rounds – Boulder County Parks and Open Space

Claire DeLeo, Plant Ecology Supervisor

Dave Hoerath, Wildlife Specialist

Date: June 14, 2010

RE: Highway 7 Crossing Feasibility Study

The Boulder County Parks and Open Space Department is a partner in the Great Outdoors Colorado-funded project to complete the Eastern Link of the Coal Creek Regional Trail. In order to complete the trail it must cross Colorado Highway 7. With the agreement of our partners, the Department decided to undertake a feasibility study of two options for crossing the highway.

Our contractor, SEH, is developing a study of the engineering and cost implications of the two crossing options. The Department chose to explore the impacts of the options on plant ecology and wildlife. When combined, these two efforts will allow decision-makers to make an educated decision as to the preferred alternative for crossing Highway 7.

History of Project

In October 2009 Boulder County, the City of Lafayette, and the Town of Erie began negotiations to submit a grant to Great Outdoors Colorado to complete the eastern link of the Coal Creek Trail. In September of the same year, Boulder County and the City of Lafayette jointly purchased the Mountain View Egg Farm, which significantly expanded the options for completing the trail by reducing possible environmental impacts to Coal Creek.

In March of 2010 Boulder County Parks and Open Space began planning a trail on the Mountain View Egg Farm that would connect the existing proposed trail south of Highway 7 with the proposed trail north of Highway 7 without impacting the highly erosive and steep banks of Coal Creek south of Highway 7.

The staff initially focused on designing a trail alignment that would minimize impacts to natural resources and agricultural opportunities on the property. However, public input began to stress concerns about the impacts of the trail crossing at a bridge over Coal Creek east of the Mountain View Egg Farm. In response the Department agreed to complete a feasibility study of two options for crossing Highway 7.

In 2007 the Department updated the Two Creeks Open Space Management Plan along with the City of Lafayette. At the time the Department obtained an access easement for possible trail construction on the eastern edge of the Egg Farm property in order to address concerns about trail impacts and construction costs of the proposed Coal Creek Trail if it were designed to remain near Coal Creek on publicly owned open space.

At the time of the 2007 revision of the Two Creeks plan the staff stated that due to access issues, the preferred method of crossing Highway 7 was to cross under the Highway 7 bridge at Coal Creek as opposed to an existing box culvert on the eastern edge of the Mountain View Egg Farm property.

With the purchase of the Egg Farm property, increase in nearby population, and changes in flow regimes at both the box culvert and Coal Creek, BCPOS staff agreed that the public's request to complete a feasibility study was wise.

History of Trail

The Coal Creek Regional Trail is part of Boulder County's regional trail network. The trail corridor has been part of the trail system since the first County Trails Map was included in the County Comprehensive Plan.

Description of Options

The planning team first discussed crossing Highway 7 by building a pedestrian overpass bridge, but due to cost this is not an option. The team rejected an at-grade crossing due to safety issues. This feasibility study explores two further options for crossing Highway 7. The two remaining options use existing underpasses to cross Highway 7.

Box Culvert

The box culvert option is a 10-foot by 10-foot concrete tunnel located on the northeastern edge of the Mountain View Egg Farm property. The box culvert serves as a drainage way for run-off from the Anthem housing development. Based on the topography of the area, it is likely that this drainage way at one time served to drain the fields south and east of the Mountain View Egg Farm. In recent years, increased development has led to increased run-off in the drainage way.

Coal Creek

Coal Creek is a naturally occurring creek that begins south of the City of Boulder and eventually links into the South Platte River. South of the Highway 7 crossing, it is joined by Rock Creek. Highway 7 crosses Coal Creek on a 2-lane bridge. The bridge is approximately 18 feet above the bed of the creek at its highest point. The creek bed is about 20 feet wide at the crossing. Development up stream of the crossing has increased seasonal flows and changed flood regimes in the creek.

Parameters of Feasibility Study

In order to complete a feasibility study on the short timeline dictated by the grant agreement with GOCO, the partners agreed that the contractor would complete the engineering and cost reviews for both options while Boulder County Parks and Open Space staff would assess the environmental impacts of the alternatives.

The short timeframe for this study limits some of the options available to the team for review of the options. However, based on similar sites across the county and the skills of our staff, we feel that an honest assessment can be made of the environmental impacts of the two options. To that end, wildlife and plant ecology staff will address the pros and cons of the options and select a preferred alternative.

The preferred alternative based on environmental factors will then be compared to the preferred alternative based on cost and engineering factors. If all parties choose the same option that will be the preferred alternative. Where there are differences, staff will meet to discuss and choose a preferred alternative based on that group discussion.

Wildlife Findings

Putting a trail through either of the highway crossing options will have impacts to wildlife and wildlife habitat. State Highway 7 and the bridge are the biggest impacts to wildlife habitat in the area and to the connectivity of habitats. Each of the crossing options still provides some level of habitat connectivity and likely reduce vehicle collisions with wildlife by offering travel options under the road. While value is a relative term, the stream habitat at the bridge site is more valuable and important than the box culvert habitat; it connects larger habitat patches and is a more important travel corridor. The box culvert is less attractive to mammal crossing use when it is running water in the spring or after periods of rain. The floodway space at the bridge above the channel devoid of vegetation provides a dry space that is well-used by mammals. A trail through either would affect or preclude most diurnal use by mammals, especially at the box culvert site. However, most use is likely nocturnal and would probably be little impacted at either site.

Each of the structures (bridge and box culvert) provide nesting substrate for swallows and birds that use old swallow nests. The small box culvert is an ideal nesting site for cliff swallows and is currently occupied by many nests. Any trail through it would likely preclude or heavily retard any nesting due to the low ceiling, confined space, and proximity to neighborhoods. The highway bridge construction has created multiple angles underneath supporting dozens of swallow nests. A trail along the east bank under the bridge would probably disrupt some nesting close to the trail, but the added height, space, and tolerance of swallows would retain most nesting. There would likely be no change in nesting attractiveness on the west side of the bridge support pier. It is unknown whether or not the bridge supports roosting bats, but they should react similarly to the swallows, being even more removed from potential visitors/users in both time and space.

Each of the crossing options has riparian character that would be compromised by placing a trail at that location. The box culvert crossing would be completely engrossed by the trail, with or without the water going through it, with additional impact of the trail alignment in entry and exit from the culvert. The bridge crossing trail footprint would be only a small portion of the substantial under-bridge area/cross-section, with impacts diminishing farther away from the trail surface and on the western side of the bridge support pier. The trail footprint would not dominate the site, and would not be in the central riparian core of the habitat or at the stream level (as it would be in the box culvert). It too would have entry and exit impacts to the riparian area. The entry/exit impacts would appear to be relatively more substantial at the box culvert site due to terrain restrictions, and the openness and overall size of the habitat patch. The terrain would force the trail to be in the drainage for a greater length, especially in comparison to the north side entry/exit of the highway bridge site. The dense riparian vegetation, bridge span, and bridge pier create a relatively closed canopy forest situation that offers substantial cover and insulation from potential trail users in contrast to the box culvert site.

Plant Ecology Findings

The box culvert option will have greater wetland impacts than the Coal Creek bridge option. These wetland impacts can be mitigated in a different location, but wetland avoidance is preferred before mitigation since wetlands are difficult and costly to create. Wetland impacts are regulated federally by the US Army Corps of Engineers, and a wetland delineation will need to be conducted to determine the exact jurisdictional wetland impacts. The Coal Creek bridge option may have some impact on jurisdictional wetlands, but they will certainly be less than the box culvert option because the trail will be elevated above most of the existing wetland vegetation at Coal Creek. Trail construction under the bridge at Coal Creek may

have some temporary wetland impacts. Permanent impacts are unknown at this time since final trail alignments have not been designed at either location. Both options will require grading of the banks to enter and exit the crossing under the highway, so they will have equal disturbances in this way that will need to be reseeded. The banks themselves do not have any sensitive vegetation.

Preferred Alternative

Due to the smaller proportion of the habitat impacted at the bridge crossing site, this crossing site is preferred as the choice for passing under State Highway 7. The Coal Creek site is better able to handle the impacts of a trail through it and would not be continue to function as a wildlife corridor. Additionally, the trail platform will likely aid nocturnal highway crossings by larger mammals by providing a clean, clear, quick path under the highway. Furthermore, due to the raised nature of the trail under the bridge, impacts to vegetation would be temporary and can be mitigated.

FINAL DRAFT - Underpass Feasibility Analysis

Coal Creek Trail at State Highway 7

Boulder County, Colorado

SEH No. 110780

June 10, 2010



**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

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Appendix D	Summary of Underpass Alternative Advantages and Disadvantages
Appendix E	Photos, Existing Plans and Inspection Reports



MEMORANDUM

TO: Kristine Nelson, PE – Boulder County Transportation Department
Jesse Rounds – Boulder County Parks and Open Space

FROM: Phil Weisbach, PE – SEH Project Manager
Matt MacLachlan, PE – SEH Project Engineer

DATE: June 10, 2010

RE: Coal Creek Trail – Underpass Feasibility Analysis

The purpose of this memorandum is to provide a summary of the study and evaluation results of two potential crossing locations for the future Coal Creek Trail in Boulder County, Colorado. Both locations are underpass crossings of State Highway 7 (SH 7), also known as Baseline Road. The trail would utilize areas below or within existing structures currently carrying SH 7 vehicular traffic over drainage courses.

Project Scope, Goals and Criteria

In a cooperative effort to complete the last eastern segment of the Rock Creek trail and the easternmost segment of the Coal Creek trail, the City of Lafayette and the Town of Erie have teamed with Boulder County to extend the existing Coal Creek/Rock Creek Trail System. The Rock Creek Trail will be extended from its current terminus in the Vista Business Park of Lafayette, Colorado to the confluence of the Coal and Rock creeks. The Coal Creek Trail will then continue from the confluence to Erie's existing trail terminus at Vista Parkway. On property jointly owned by Lafayette and Boulder County, a design for the extension of the trail from the Vista Business Park to SH 7 is underway. The Town of Erie is also in the design phase of extending the trail north from SH 7. The alignments of these portions of the trail are in the development stage and will be impacted by the preferred underpass crossing location. This work is being designed and constructed using local funds and grant funding.

Two existing structures, the "SH 7 Bridge over Coal Creek" and "SH 7 Box Culvert" are both in the general vicinity of the desired trail alignments. This analysis is intended to identify issues, opportunities, and constraints associated with the two alternative underpass crossing locations.

This study attempts to answer three primary questions:

1. Are either, or both, underpass possibilities feasible from an engineering standpoint? i.e.: Are they physically possible to construct within accepted underpass and trail design criteria (maximum grades, drainage considerations, headroom clearance required, etc)
2. If both are feasible, what are the comparative merits or issues between the two?
3. How compatible is each alternative with the potential trail alignment possibilities both north and south of SH 7?

This is a comparative and qualitative, as well as quantitative, analysis.

In order to begin the comparison, the design and evaluation criteria need to be established. The design criteria include geometric parameters required for both crossing locations. The evaluation criteria include those features and products that result from using the crossing. These evaluation criteria will ultimately be used to compare the two options.

Design Criteria include:

- 1) Trail Users – The trail will be designed for use by pedestrians and bicyclists. The trail north of SH 7 is not intended for equestrian use, although equestrians are allowed south of SH 7.
- 2) Trail Cross Section – Consistent with the design of the Vista Business Park to SH 7 portion of the trail, the trail cross section approaching the underpass will ideally be 8 feet wide, with 1 foot shoulders and consist of 6” of compacted crusher fine material on a compacted subgrade. This also matches other recent trail projects completed by the County. Per the County’s direction, the trail surface at the underpass will be concrete, ideally 10 feet wide with 2 foot colored concrete shoulders.
- 3) Trail Grades Approaching Crossing – In accordance with previous County review comments, the Americans with Disabilities Act (ADA) Standards for Accessible Design do not apply to trails or trail structures. As a matter of best practice, the County prefers to keep multi-use trail grades no steeper than 5%. When considering trail gradients over 5%, the County attempts to comply with the design standards outlined in the *Forest Service Trail Accessibility Guidelines* and the *Architectural Barriers Act Proposed Accessibility Guidelines for Outdoor Developed Areas*. The technical provisions of these documents stipulate the following:
 - 5% grade – acceptable for any distance
 - 8.33% grade – acceptable for up to 200 feet before a rest interval of 5 feet
 - 10% grade – acceptable for up to 30 feet before a rest interval
 - 12.5% grade – acceptable for up to 10 feet before a rest intervalGrades steeper than 5% may require alternative trail materials to maintain stability.
- 4) Trail Horizontal Alignment Radii – A minimum horizontal radius of 55 feet is assumed. This is in accordance with the AASHTO Guide for the Development of Bicycle Facilities.
- 5) Vertical Clearance – A vertical clearance of 8 feet (minimum) shall be provided for the users of the underpass at SH 7. For the purposes of this evaluation, a minimum 8 foot vertical clearance is assumed to accommodate pedestrians and bicyclists.

Evaluation Criteria include:

- 1) Trail Alignment – Design Criteria Impact – The underpass crossing location must meet the identified design criteria. Modifications to the preliminary conceptual alignments may need to be made in order to meet these criteria.
- 2) Trail Alignment – Overall Trail Alignment Compatibility – The selected underpass crossing location may be influenced by, or have an influence on, the preferred trail alignment north and south of SH 7. This criterion attempts to quantify how the different feasible underpass locations might affect the preferred trail locations north and south of SH 7, since the trail alignment will need to meet the underpass crossing. SEH has been provided with these approximate conceptual alignments, as indicated in the attached alignment graphic, Figure 1 of Appendix A.
- 3) Trail User Experience – The trail is intended to be primarily a recreational trail used by hikers, bicyclists, and equestrians south of SH 7, and hikers and bicyclists only north of SH 7. As such, the underpass connection between the trails north and south should – to the extent possible – contribute to the trail user experience rather than detract from it.

- 4) Drainage Considerations – Both underpass crossing locations will be impacted by existing drainage patterns. Modifications to the existing structures must not negatively impact water surface elevations upstream or downstream of the crossing. The design flows at each structure must be maintained.
- 5) Flooded Trail Considerations – Both underpass locations are in active drainage channels. During higher water events, the trail may be flooded, requiring users to cross SH 7 at-grade. This criterion evaluates the safety associated with crossing SH 7 at-grade when the underpass location is flooded.
- 6) Private Property Impacts – While the majority of these proposed trails are on publicly owned land, the potential exists for impact to private property. Using information obtained from the Boulder County and Weld County Assessor’s websites, the attached property ownership exhibit (Appendix A, Figure 2) delineates the private and public property limits. Acquisition of private property in order to meet trail alignment criteria may not be possible and may determine the viability of the crossing location.
- 7) Potential Impacts to Adjacent Neighbors – Despite public ownership of the actual crossing and approaching/departing property, the concerns and impact of the trail to the adjacent community shall be considered in this evaluation.
- 8) Environmental Impacts – The stakeholders of this project are sensitive to environmental impacts of this type of improvement. Each crossing option will consider the qualitative impact to the natural resources that may be affected. SEH’s analysis of environmental impacts of the alternatives is limited to a cursory review of the potential construction impacts within the project area. Boulder County Parks and Open Space is preparing a more detailed environmental assessment of the “Two Creeks” open space, and will provide additional environmental resource analysis of the underpass locations beyond just the construction impacts in that document.
- 9) Potential Colorado Department of Transportation (CDOT) Issues – The existing structures are owned by CDOT. The trail will cross CDOT Right of Way, and some options require the trail to parallel SH 7 within CDOT Right of Way. Any impact to these structures, as well as portions of the trail along the CDOT owned SH 7 Right of Way will require CDOT review, approval, and permits. CDOT may have criteria regarding the distance of the trail from the highway, as well as significant drainage design criteria for any new structures or modifications to existing structures.
- 10) Additional Coordination Requirements – Additional public agencies or impacted businesses in the area may require coordination depending on the selected underpass crossing.
- 11) Maintenance Considerations – The agencies involved need to agree on maintenance responsibilities once the underpass is constructed. Agreements between the responsible agency and CDOT will need to be in place detailing the limits of responsibility for the maintenance of this underpass. As such, the effort and cost of this maintenance shall be a consideration for the evaluation.
- 12) Cost – Using the established alignments and current CDOT unit prices, an Engineer’s Opinion of Probable Cost was developed for each option. For estimating purposes, a Right of Way cost of \$8 per square foot was used for the permanent easements needed near the Box Culvert alternatives. This property is zoned commercial.

The following considerations were not included in this analysis:

- 1) Complete Topographic Survey – Existing topographic maps were used to establish existing ground elevations and approximate the required trail grading and alignments. Portions of available survey data and existing construction plans were also used to estimate existing ground topography.

- 2) Extensive Drainage Evaluation – Drainage considerations include a qualitative analysis without a complete hydrologic or hydraulic evaluation. It is understood that both crossings will need to continue to convey water under SH 7, however the quantity of water has not been established.
- 3) Structural Design – The analysis includes approximations of structural requirements or modifications without a complete, detailed structural design.
- 4) Evaluation of Impacted Utilities – During the design and survey process, existing utilities in the area are typically identified and the owners are contacted to determine the project’s impact on their lines. This effort will be completed at a later date.
- 5) Geotechnical Analysis – Soil samples and trail and/or foundation recommendations may be required as part of the future underpass design.
- 6) Environmental Analysis – Environmental considerations include a qualitative analysis without a complete environmental analysis. Further review of environmental impacts and permit applications may be required for the final design.
- 7) Coordination with CDOT – Both underpass locations are within existing CDOT Right of Way. CDOT will need to be involved in the underpass design to ensure their compliance.

Coal Creek Bridge Under SH 7 Underpass Alternative

This underpass alternative consists of benching a trail into the existing rip-rap slope on the east side of Coal Creek under the bridge. This method usually results in a greater drainage opening under the bridge, thus hydraulic capacity of the structure is not affected, and in many cases, slightly improved. This location presents an ideal opportunity for the trail underpass, because:

- There is plenty of headroom available for the trail (approximately 14.5 feet from the bottom of the girders to the flat sandy area adjacent to the creek)
- There is a wide bridge opening, giving the underpass an open, unconstrained feel
- Once the trail is in place, there is a large freeboard between the normal water elevation of Coal Creek and the top of the trail, thus keeping the trail high and dry except potentially in the relatively short periods of high water flows or flood events.

General Crossing Description

The existing, two span 146’ long, 46’-6” wide structure carrying SH 7 over Coal Creek was built in 1990. The structure number for this bridge is D-16-DM, and it is located approximately 1.7 miles east of US 287. Since the bridge is on a state highway, CDOT is responsible for the construction and maintenance of this structure.

The superstructure carries two lanes of traffic with wide shoulders and consists of four spread precast, prestressed concrete box girders supporting a concrete deck. The superstructure is supported by concrete piers and abutments on deep foundations. The structure is inspected every two years as part of the Federal Highway Administration (FHWA) inspection requirement of “major” structures for geometric and structural adequacy. The results of these inspections yield a sufficiency rating, defined by the FHWA as “a method of evaluating highway bridge data by calculating four separate factors to obtain a numeric value which is indicative of bridge sufficiency to remain in service. The result of this method is a percentage in which 100 percent would represent an entirely sufficient bridge and zero percent would represent an entirely insufficient or deficient bridge”. This structure has a calculated sufficiency rating of 91.5, which indicates it is of sound construction and a long, useful service life is expected.

As part of the original design plans completed in 1988, the ordinary (normal) high water elevation is estimated at 17.4 feet (minimum) below the existing beams. The design high water (1 anticipated occurrence in 50 year event) is estimated at 2.2 feet (minimum) below the existing beams (see below for further definition of drainage terminology). The ground slopes at 2:1 from the abutments to the slope bottom and is lined with riprap. The ordinary high water is approximately 1.5 feet above the slope bottom. The slope bottom is approximately 35.4 feet from the face of the abutment. It should be noted, however, that the slope bottom appears to have silted in approximately 5 feet deep, creating a natural sandy “bench” several feet above the ordinary flow of the channel. The benched area has moderate vegetation growth.

See the attached pictures of the structure geometry and condition (Appendix E).

Possible Modifications to Utilize the Existing Crossing Location

The attached graphic (Appendix A, Figure 3) depicts possible modifications to the existing underpass to establish a trail. The existing vertical clearance from the naturally formed “bench” on the east side of Coal Creek is approximately 14 feet. The proposed modifications include building a trail into the existing riprap slope, providing 8 feet of vertical clearance. This will raise the trail as high as possible above Coal Creek. A retaining wall will be necessary to support the riprap/ground slope from the east abutment.

Determination of Trail Elevation at Underpass

The existing bridge plans and a recent survey of SH 7 (as part of another project) indicate that the roadway elevation is approximately 5107 at the east end of the bridge. The bridge plans indicate that the approximate low chord elevation is 5101.5. Subtracting the 8 foot vertical clearance, the trail elevation at the underpass would be approximately 5093.5, or 10 feet above the ordinary high water elevation as indicated in the bridge plans.

Evaluation Criteria

- 1) Trail Alignment – Design Criteria Impact – One generic trail alignment is being considered with the use of this underpass, labeled in Figure 1 as Alignment A. South of SH 7 (Alignment A – South), beginning from the high point / ridge east of Coal Creek, the trail would run down to the SH 7 ground elevation. Using available topographic maps, the trail grade would exceed 5% if taken directly along this approximately 750 feet from the ridge to SH 7. Approximately 150 feet of additional trail length would be required to meet the 5% grade. This additional length is typically accomplished through switchbacks. Once at the SH 7 Right of Way, the trail can travel the approximately 250 feet with a grade of 3% until reaching the underpass.

Once the trail reaches the underpass, the trail will travel along the east edge of Coal Creek approximately 100 feet north of the bridge along Alignment A - North. The trail will then turn east and head towards the adjacent solar field. The 5% grade would be exceeded over these 250 feet; therefore an additional 125 feet of trail will be necessary to meet the desired grade of 5%. It appears this can be accomplished entirely on public land.

- 2) Trail Alignment – Overall Trail Alignment Compatibility – Should the preferred trail alignments for the trail links north and south of SH 7 be more to the west side of the public properties involved, the location of this underpass on the east side of the creek (and towards the west side of those properties), would make this location naturally more compatible with more western trail alignments. However, this underpass location would also work acceptably if the preferred trail

links north and south of SH 7 were more to the easterly side of the public properties involved. The trail to/from the Coal Creek Bridge underpass would simply be longer and more meandering.

- 3) Trail User Experience – The suggested modifications to the underpass provide a relatively open undercrossing for the trail. The user benefits from being near the creek, and the experience of traveling near a flowing creek. The SH 7 bridge, although not architecturally significant, is currently in very good condition and does not present the user with any concern about integrity or safety.
- 4) Drainage Considerations – Based on the data used during the original design of the bridge, the structure has adequate capacity to handle anticipated hydraulic events. The ordinary high water elevation is approximately 10' below the anticipated trail grade. The ordinary high water elevation is usually considered the visual marking of the typical water surface elevation. Flooding or design water surface elevations are predictions of water levels resulting from a storm or flood that is anticipated to occur at a specified frequency. On these relatively rare occasions, the trail will not be usable during these events which are generally short term in duration at large events. This will likely result in trail closures for brief periods of time during flood events in the high runoff season. An updated hydrologic and hydraulic analysis during final design would further refine the anticipated water elevations at different flood events, and determine the frequency in which the trail can be expected to be at or below the water elevation. However, it does not appear that the installation of a trail at this location will raise the water elevations up or downstream of the crossing. An alternative cross section material approaching the underpass and/or additional armoring of the area with riprap may be necessary to reduce erosion, scour or siltation.
- 5) Flooded Trail Considerations – In the event the underpass is flooded during high water events, trail users wanting to cross SH 7 must cross at-grade. Trail users could cross SH 7 at grade without signal protection somewhere east of the bridge. Alternatively, they could use the SH 7 paved shoulders to get to the traffic signal with pedestrian push buttons at SH 7 and County Line Road, approximately 500 feet to the west. With this alternative, trail users have a relatively (compared to the box culvert crossing) short out-of-direction path to be able to cross SH 7 with traffic signal protection. This option may be important to trail users who are hiking or biking with young children.
- 6) Private Property Impacts – All trail alignments leading to/from the underpass can occur on public right-of-way or publicly owned property.
- 7) Potential Impacts to Adjacent Neighbors – Some of the adjacent neighbors along Flagg Drive and east of Coal Creek have expressed their preference that the trail be further away from the Creek. Concerns from the neighbors include safety, environmental impact, and privacy. A driveway to a private residence that is within the CDOT Right of Way would need to be crossed as part of the intended trail alignment.
- 8) Environmental Impacts – There would be some impact to the riparian area in the areas where the trail transitions from the underpass back up to the creek bank. However, the disturbed areas should be able to be mitigated and restored without undue complication. The trail would be located within the 100 year floodplain of Coal Creek and would require appropriate local, state, and/or federal permits. A “topographic bench” that has formed on the east side of Coal Creek would be utilized that would lessen direct water quality impacts to the creek. A wetland delineation would be required to determine if any wetlands are present that would be affected by

this alignment. Coordination with the United States Army Corps of Engineers (USACE) would be necessary to determine the need for a permit and possible wetland mitigation requirements.

- 9) Potential CDOT Issues – Modifications to the area below a CDOT owned structure will require CDOT approval. The stakeholders need to ensure that the trail will not compromise the integrity of the structure. An agreement must be in place to determine maintenance responsibilities for this area. In addition, the trail alignment requires the use of additional CDOT Right of Way along SH 7 approaching and departing the underpass. CDOT will need to approve the plans for the improvements in their Right-of-Way, and a Special Use permit from CDOT will be required. However, no unusual or insurmountable problems are anticipated in securing the needed CDOT approvals.
- 10) Additional Coordination Requirements – There are no additional entities identified at this time.
- 11) Maintenance Considerations – A maintenance agreement will need to be in place with CDOT regarding the responsibilities for the trail and bridge within the CDOT Right of Way. The area is generally accessible to maintenance crews via CDOT Right of Way. The anticipated higher water events may require additional maintenance by the County to re-grade and clear the trail of debris.
- 12) Cost – See the attached spreadsheet with an Engineer’s Opinion of Probable Cost (Appendix B). Due to the limited data available to establish the alignment at this time, a 30% contingency has been included. The total estimated construction cost for the portion of the underpass and its trail approaches is approximately \$87,000.

Box Culvert Underpass Options Under SH 7

The Box Culvert underpass options attempt to use the existing box culvert structure as much as possible. In each of these options, however, it is necessary to separate the trail underpass from the drainage flows as much as practical. One box culvert underpass option (#1) attempts to use the existing underpass to accommodate both the trail and low water flows. The second box culvert underpass option (#2) attempts to separate the trail from the low water flows by dedicating the existing box culvert opening for the trail, and provide for the low water flows via a separate new pipe under SH 7. Because the existing box culvert is silted up with 2 to 3 feet of sediment, these first two options require that the existing channel upstream and downstream be re-graded to allow the original flowline of the drainage course to be re-established.

However, CDOT may not allow the existing hydraulic capacity of the box culvert to be compromised. In this instance, a new box culvert underpass for trail users would have to be constructed (#3), and the new facility designed to channel all current flows into the existing box culvert.

General Crossing Description

The existing single cell four sided concrete box culvert carries SH 7 over an unnamed drainage ditch, referred to only as “Gulch” in the original design plans. The 10’ by 10’ box was built in 1938. The structure does not have a designated CDOT number, but is located approximately 1.9 miles east of US 287 and 1600 feet east of Flagg Drive. Since the culvert is on a state highway, CDOT is responsible for the construction and maintenance of this structure.

Since the structure is less than 20 feet long, it is considered a “minor” structure and the Federal Highway Administration does not require biennial inspections. CDOT has elected to have these structures inspected every 5 years as a matter of good practice. The last inspection, completed in 2009 by SEH, provided a structural evaluation appraisal of 7 (out of 9). Minor deterioration was observed, but no significant problems were noted.

The structure was designed to carry the drainage flows in the “gulch” under SH 7. The original drainage design is not available; however, it is assumed the culvert was designed to carry the design year flows as stated by their then-current drainage design criteria. Since the culvert was installed, it is obvious that the channel downstream and upstream has silted in. As a result, there is standing water in the structure nearly year round. The bottom of this structure is silted in approximately 2 to 3 feet. Considerable vegetation has grown around the inlet and outlet of the structure. Based on the inspection observations and the original plans, the structure has approximately 15 to 20 feet of cover over the concrete top slab.

See the attached pictures of the structure geometry and condition (Appendix E).

Possible Modifications to Utilize Existing Crossing Location

As indicated above and shown in the attached graphics (Appendix A, Figures 4 – 6), there are three primary alternatives for the modifications to this underpass to accommodate a trail.

- **Box Culvert Alternative 1** would partition the existing 10’ wide culvert in such a manner as to provide a 6’6” trail and a 3’ low flow channel, separated by a 6” cutoff wall as shown in Figure 4. This would reduce the trail width at the underpass, but minimize costs and roughly match the hydraulic capacity of the 36” diameter culvert further downstream (under the old East County Line Road roadbed) of this location.
- **Box Culvert Alternative 2** uses the entire existing box culvert for the trail and bores a 36” diameter culvert under SH 7 adjacent to the box culvert on the east side to accommodate the low water flows as shown in Figure 5. The 36” low flow pipe needs to be on the east side in order to prevent the need for another trail crossing across the low flow channel. This provides for a 10’ wide trail in the box culvert under SH 7, and is superior to Box Culvert Alternative 1 for that reason. However, this would be an unusual design and would require CDOT hydraulic review. The potential exists that CDOT would not permit this design.
- **Box Culvert Alternative 3** would be required if CDOT objected to the proposed impacts to the hydraulic capacity of the existing box culvert. This alternative keeps the entire existing box culvert intact, to be used as a drainage culvert only. A second box culvert will be constructed to be used for the trail. This culvert will be sized to meet the preferred geometric requirements of the trail, providing an 8’ x 12’ opening as shown in Figure 6. This culvert invert will be considerably higher than the existing culvert invert, reducing the extent of necessary grading to meet the approaching trail. A large retaining wall will be necessary between the two culverts to ensure support and stability of the structures, and to funnel the drainage flows into the existing box culvert.

In all cases, cut-off walls would be needed upstream and downstream of the box to separate the trail from the low flow water channel. In all alternatives except possibly Alternative #3, when the volume of water crossing under SH 7 exceeds the low flow channel capacity, water will spill over and flood out the trail.

Determination of Trail Elevation at Underpass

The recent survey as part of another project does not extend far enough east to this location. However, the original construction plans for this structure show roadway vertical alignment information within the recent survey limits and the near the Coal Creek bridge location. The roadway elevations in these locations match, providing a confidence in the structural elevations shown in the original culvert construction drawings.

Box Culvert Alternatives 1 and 2: Using the original construction plans, the inlet elevation of the structure is approximately 5099, while the outlet elevation is approximately 5097.

Box Culvert Alternative 3: Based on the top of roadway elevation shown in the original plans and subtracting a nominal depth of fill (3 feet) over the culvert as well as the culvert geometry, an invert elevation of 5114 is estimated.

Evaluation Criteria

- 1) Trail Alignment – Design Criteria Impact – One generic trail alignment south of SH 7 is being considered with the use of this underpass, as indicated in Figure 1 and noted as both “Alignment B – South” and “Alignment C – South”. The trail will travel along the existing Egg Farm access driveway for a distance of approximately 700 feet.
 - **Box Culvert Alternatives 1 and 2:** Based on the existing culvert invert elevation, a grade of 5% cannot be attained in the first 400 feet heading south from the culvert. Instead, an additional 220 feet will be required in order to use a 5% trail. Based on the existing topographic maps, the last 300 feet heading south of this portion of the trail will essentially be flat.
 - **Box Culvert Alternative 3:** Based on the assumed culvert invert elevation, a grade of 4% can be attained in the first 400 feet heading south from the culvert. Based on the existing topographic maps, the last 300 feet heading south of this portion of the trail will essentially be flat.

Heading north from the box culvert, the stakeholders are considering two horizontal alignments. The first alignment (Alignment B – North) immediately departs the culvert and heads west along the SH 7 Right of Way for a distance of 300 feet, before turning north again for 300 feet.

- **Box Culvert Alternatives 1 and 2:** The first 50 feet of this east/west portion would be very steep. In order to use a 5% grade, an additional 210 feet of trail will be needed. Given the minimum radii design criteria, it will not be possible to achieve these switchbacks without encroaching upon the adjacent private property. Using the available property line information, the existing culvert plans, and based on the location of the existing fence lines north of the box culvert, it appears that there is only approximately 20 feet of available CDOT Right of Way beyond the end of the culvert. Even with an exception of the minimum radius immediately departing the culvert, the additional switchbacks cannot be accomplished entirely within CDOT Right of Way. The remaining portions of this alignment (east/west and north/south) can be graded at 5%.
- **Box Culvert Alternative 3:** Based on the assumed trail invert elevation, grades of less than 5% can be met along this alignment without encroaching upon private property if an exception of the minimum radius immediately departing the culvert is permitted.

The second potential alignment identified by the stakeholders (Alignment C - North) north of the box culvert runs diagonally along the west side of the “gulch” drainage course, along private property. Using the estimated invert elevation for the existing culvert, a trail grade of less than

1% can be attained. For Box Culvert Alternative 3, the trail grade over this approximately 450 feet would be 4%.

- 2) Trail Alignment – Overall Trail Alignment Compatibility – If the preferred alignments for the trail link north and south of SH 7 is more to the east side of the public properties involved, the location of this underpass is more compatible with these more eastern trail alignments. However, this underpass location would also work acceptably if the preferred trail links north and south of SH 7 were more to the westerly side of the public properties involved. The trail to/from the Box Culvert underpass would simply be longer and more meandering. However, as mentioned above, Alternatives 1 and 2 would require the trail to be on private property to meet trail grade criteria.
- 3) Trail User Experience –
 - **Box Culvert Alternative 1:** The narrow trail width, combined with the tunnel effect from using this box culvert may not provide a pleasing or safe experience to the user. It would feel narrow and cramped. There would not be enough room for bikes going in opposite directions to pass safely. It would have a “narrow tunnel” effect. The box culvert should have lights installed to improve comfort and safety. As a result, SEH believes Box Culvert Alternative 1 is NOT FEASIBLE.
 - **Box Culvert Alternative 2:** Although the trail width would be wider than the typical trail approaching the underpass, the enclosed box culvert would still present a “narrow tunnel” effect which could negatively impact the trail user experience. The box culvert should have lights installed to improve safety and comfort. This option is preferable to Alternative 1 in that the trail width is at least 10’ wide. However, when vertical walls are directly adjacent to a trail in an underpass, trail users shy away from the wall, resulting in a narrower effective width.
 - **Box Culvert Alternative 3:** Although the trail width would be wider than the typical trail approaching the underpass, the enclosed box culvert would still present a “narrow tunnel” effect which could negatively impact the trail user experience. The box culvert should have lights installed to improve safety and comfort. This option is preferable to Alternatives 1 and 2 in that the trail width is 12’ wide.
- 4) Drainage Considerations – This structure carries water from an unnamed drainage course, flowing from south to north into Coal Creek. The existing structure’s hydraulic capacity is unknown at this time. A 36” diameter culvert under the old East County Line roadbed is located further downstream, prior to outletting into Coal Creek and therefore was assumed to carry the “everyday” flow at this location. Modifying the hydraulic capacity of the SH 7 crossing may impact the water surface elevations upstream near the Anthem residential development. A thorough hydraulic analysis will be necessary prior to the installation of Box Culvert Alternative 2, as this 36” diameter culvert may not meet CDOT’s required hydraulic criteria. It may not be possible to meet additional headwater/diameter criteria with a pipe of this size. Box Culvert Alternative 3 does not alter the hydraulic capacity of this crossing. As discussed below, it could actually provide additional hydraulic capacity.
- 5) Flooded Trail Considerations – In the event the underpass is flooded during high water events, trail users wanting to cross SH 7 must cross at-grade. At this location (approximately 1750 feet – more than a quarter of a mile- east of the intersection of SH 7 and East County Line Road and approximately 1 mile west of the intersection of SH 7 with the entrance to the Anthem development) it is unlikely that trail users would travel that far west to the signal to cross with the

safety of a traffic signal. It is highly likely that trail users would cross at-grade. Although sight distance to the east and west is adequate, SH 7 is a busy, high-speed highway. Given the variety of trail users that may be using the trail (the very young and old, as well as the young and fit), there would be a much higher safety risk crossing SH 7 during underpass flooding events compared to the Coal Creek bridge underpass option. A detailed hydraulic evaluation was not part of this study's scope, thus it is unknown how often underpass flooding would be expected with the box culvert options. Alternatives 1 and 2 would have a much higher risk of trail flooding than Alternative 3.

- 6) Private Property Impacts – The property immediately north of the CDOT owned SH 7 Right of Way is privately owned. The trail alignment for Alternatives 1 and 2, as discussed above, will require encroachment into this property due to the existing ground elevations and required grading. It is uncertain whether property acquisition in this area is possible. Alternative 3 does not require the acquisition of private property to meet trail grade criteria for Alignment B, however Alignment C – North is entirely within private property.
- 7) Potential Impacts to Adjacent Neighbors – In adjacent Broomfield County, the Anthem development residents will not be provided a direct connection to this proposed trail. However, it should be anticipated, given the proximity, that some of these residents will use the trail system. As this underpass location is closer to them than the Coal Creek underpass location, Anthem residents may more readily use it. Based on feedback received to date, this location is preferred by some of the Flagg Drive neighborhood residents, as it would tend to move trail underpass users to the east away from the Flagg Drive properties.
- 8) Environmental Impacts –
 - **Box Culvert Alternative 1:** A wetland delineation would be required to determine the extent of the wetland impacts. Coordination with the United States Army Corps of Engineers (USACE) would be necessary to determine the need for a permit and possible wetland mitigation requirements. This alternative would directly impact this drainage by changing its course, which may require a permit through the USACE.
 - **Box Culvert Alternative 2:** A wetland delineation would be required to determine the extent of wetland impacts. Coordination with the USACE would be necessary to determine the need for a permit and possible wetland mitigation requirements. This alternative would directly impact this drainage by changing its course and placing it within a new culvert. This action would require a USACE permit.
 - **Box Culvert Alternative 3:** Compared to Alternatives 1 and 2, wetland impacts would be minimized with Alternative 3, since the majority of the construction would take place away from the watercourse and wetlands. However, the extent of this would not be known until final design. A wetland delineation would be required to determine the extent of wetland impacts, if any. Coordination with the USACE would be necessary to determine the need for a permit and possible wetland mitigation requirements. This alternative would not directly impact this drainage, since the course and capacity of this crossing are not changed. This work may still require a USACE permit.

With either Box Culvert Alternative 1 or 2, significant re-grading will be required both upstream and downstream to re-establish the channel flowline to the bottom of the box culvert. Significant impacts to the existing wetlands upstream and downstream will result. Some of the wetlands area may be re-established within the new channel and thus the ultimate impact may be largely offset.

However, depending on the details of the final designs of Box Culvert Alternatives 1 and 2 if selected, there may be permanent impacts that have to be mitigated.

- 9) Potential CDOT Issues – Modifications to the effective size and hydraulic capacity of a CDOT owned structure will require CDOT approval. The stakeholders need to ensure that trail use will not compromise the integrity of the structure. Alternatively, a new box culvert structure under an existing highway will require extensive review and coordination. An agreement must be in place to determine maintenance responsibilities for this crossing. In addition, the trail alignment requires the use of additional CDOT Right of Way along SH 7 approaching and departing the underpass. CDOT will likely require a complete hydrologic and hydraulic analysis prior to approving either Box Culvert Alternative 1 or 2. CDOT will need to approve the plans for the improvements in their Right-of-Way, particularly with Box Culvert Alternative 3, and a Special Use permit from CDOT will be required. However, no unusual or insurmountable problems are anticipated in securing the needed CDOT approvals.
- 10) Additional Coordination Requirements – In addition to the public agencies supporting this project, adjacent private property owners and CDOT, this project may require additional coordination. The area north of SH 7 is in Weld County, so Weld County will be contacted to determine their level of interest in the project details. In addition, the Erie Airport is directly north of the underpass. While the trail location will head west and remain clear of airport property, the proximity of the trail to existing buildings and doorways may require additional coordination and security provisions.
- 11) Maintenance Considerations – A maintenance agreement will need to be in place with CDOT regarding the responsibilities for the trail and the culvert. The area is generally accessible to maintenance crews via CDOT Right of Way. The anticipated higher water events may require additional maintenance to clear the trail of debris.
- 12) Cost – See the attached spreadsheet (Appendix B) with an Engineer’s Opinion of Probable Cost. Due to the limited data available to establish the alignment at this time, note that a 30% contingency has been included. The total estimated construction cost for the portion of the trail shown on the attached graphics is:
 - a. Box Culvert Alternative #1 – Ranges from \$97,000 to \$143,000
 - b. Box Culvert Alternative #2 – Ranges from \$167,000 to \$213,000
 - c. Box Culvert Alternative #3 – Ranges from \$414,000 to \$505,000

Note that the ranges are necessary due to the uncertainty of the desired trail alignment and actual costs of Right of Way acquisition.

Analysis

See Appendix C for a Summary Evaluation Matrix for the alternatives analyzed.

- SH 7 Bridge over Coal Creek Underpass

The Coal Creek bridge underpass alternative is feasible. It will provide the desired vertical clearance and width for the trail under the bridge. The trail approaches entering/exiting the underpass on both sides of SH 7 can be designed to meet the 5% max grade requirement, and can be accomplished entirely using public lands. While this location would naturally be more

compatible with more western trail alignments, this underpass location would also work acceptably if the preferred trail links north and south of SH 7 were more to the easterly side of the public properties involved. This alternative would provide a superior trail user experience, since it travels close by a running stream, and the bridge opening is wider and more open than any of the box culvert options. This option also provides a significant freeboard of about 10 feet over the normal high water surface elevation. This means that typically, the trail underpass will only be under water in flood events, but not normal high water events. In those rare cases when the trail is flooded, trail users not wanting to cross SH 7 unprotected at-grade can access the traffic signal at SH 7/East County Line Road and cross with pedestrian actuated traffic signal protection if they wish. The environmental impacts involved with the construction are minor, and no significant, non-mitigatable impacts are foreseen. The cost of this underpass is estimated at \$87,000, and that is less than all of the Box Culvert Underpass Options.

- **Box Culvert Underpass Options**

- **Alternative #1 is NOT feasible.** The trail width is only 6.5 feet wide, which is less than the desired minimum width of 8 feet. The wall on the west side would further reduce the effective trail width to 6 feet or less. This underpass alternative would not provide for a good trail user experience. Bikes coming from opposite directions attempting to pass within the underpass would be susceptible to collisions. The cost of this underpass is estimated to range between \$97,000 and \$143,000 (depending on the selected alignment) and that is more than the Coal Creek Underpass Option.
- **Alternative #2 is potentially feasible.** It will provide the desired vertical clearance and trail width under SH 7. The trail approaches entering/exiting the underpass on both sides of SH 7 can be designed to meet the 5% max grade requirement, but this cannot be accomplished entirely using public lands. Easements from a private property on the north side of SH 7 would be required to make the grades work. It is unknown if that private property owner would allow a trail on their property. In addition, easements from the private property owner would also be required to re-grade the channel downstream to re-establish the channel flowline for the 36" diameter pipe. While this location would naturally be more compatible with more eastern trail alignments, this underpass location would also work acceptably if the preferred trail links north and south of SH 7 were more to the westerly side of the public properties involved. This alternative would provide a lesser trail user experience compared to the Coal Creek Bridge underpass or Box Culvert Alternative #3, since trail users are in a narrow concrete tunnel, and without the stream amenity adjacent that the Coal Creek Bridge option has, or the additional trail width of Box Culvert Alternative #3. This alternative may not meet CDOT's hydraulic design criteria and may not be a viable option as depicted. This option will be flooded and thus unusable to trail users when low water flows exceed the capacity of the 36 inch diameter low water flow pipe. It is unknown how often this will be. In those cases when the trail is flooded, trail users not wanting to continue north or south most likely will cross at SH 7 unprotected at-grade. The traffic signal at SH 7/East County Line Road is so far to the west it is unlikely that trail users will be willing to travel that far out of direction. This would result in a potentially less safe situation than the Coal Creek Bridge underpass option when the Box Culvert is flooded or has standing water from storms. The environmental impacts involved with the construction are primarily related to the wetlands, but no significant, non-mitigatable impacts are foreseen. The cost of this underpass is estimated to range between \$167,000 and \$213,000 (depending on the selected alignment) and that is more than the Coal Creek Underpass Option.

- **Alternative 3 is feasible.** It will provide the desired vertical clearance and trail width under SH 7. The trail approaches entering/exiting the underpass on both sides of SH 7 can be designed to meet the 5% max grade requirement, and this can be accomplished entirely using public lands for Alignment B. Easements from a private property on the north side of SH 7 would be required using Alignment C - North. While this location would naturally be more compatible with more eastern trail alignments, this underpass location would also work acceptably if the preferred trail links north and south of SH 7 were more to the westerly side of the public properties involved. This alternative would provide a lesser trail user experience, since trail users are in a narrow concrete tunnel, without the stream amenity adjacent that the Coal Creek Bridge option has. This option will only be flooded and thus unusable to trail users when high water flows exceed the capacity of the existing 10' x 10' box culvert. It is unknown how often this will be, but that would likely be a rare event. In those rare cases when the trail is flooded, trail users not wanting to continue north or south must cross SH 7 unprotected at-grade. The traffic signal at SH 7/East County Line Road is so far to the west it is unlikely many trail users will be willing to travel that far out of direction. This would result in a potentially less safe situation than the Coal Creek Bridge underpass option when the Box Culvert is flooded or has standing water in it from storms. The environmental impacts involved with the construction are primarily related to the wetlands, but no significant, non-mitigatable impacts are foreseen. The cost of this underpass is estimated to range between \$414,000 and \$505,000 (depending on the selected alignment) and that is more than the Coal Creek Underpass Option.

Appendix D provides an “Advantages and Disadvantages” summary of the alternatives.

Conclusions

Both the Coal Creek Underpass and Box Culvert Underpass Alternatives #3 are feasible. Box Culvert Underpass Alternative #2 is potentially feasible depending on whether CDOT will accept the 36” diameter pipe as shown. The Coal Creek Underpass is preferable because of its superior trail user experience, being more open and being next to the creek. There is no “long concrete tunnel” effect as with the Box Culvert options. In addition, the Coal Creek underpass can be constructed without acquisition of easements from private property owners to make it work. This can potentially only be accomplished at the box culvert by using Box Culvert Alternative #3 and Alignment B – North. Thus, the Coal Creek underpass option is totally within the control of the public agencies involved, and is not contingent on private property owner cooperation to make it viable. In times of underpass flooding, a safer option to cross SH 7 with traffic signal protection is available for trail users with the Coal Creek underpass. The environmental impacts involved with the construction are minor, and no significant, non-mitigatable impacts are foreseen. This underpass will work acceptably whether the trails north and south of SH 7 are located towards the west, east, or middle of the public properties involved, although this underpass location naturally works best with trail alignments further to the west. The estimated cost of construction of the Coal Creek Underpass is significantly less than either of the viable Box Culvert Alternatives.

MM

**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

Appendix A

Supporting Graphics -- Figures 1 – 6

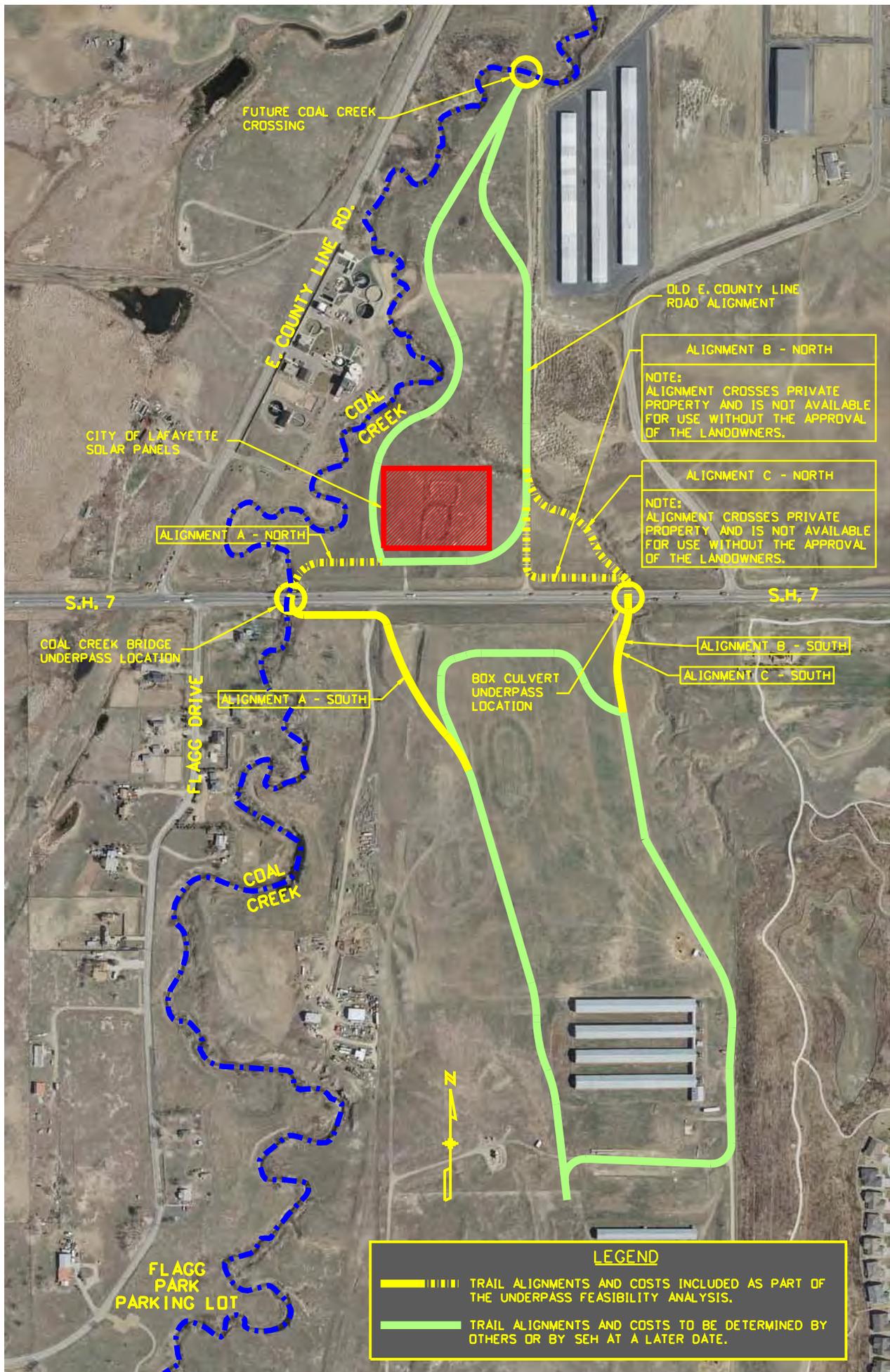


FIGURE 1 - CONCEPTUAL ALIGNMENTS

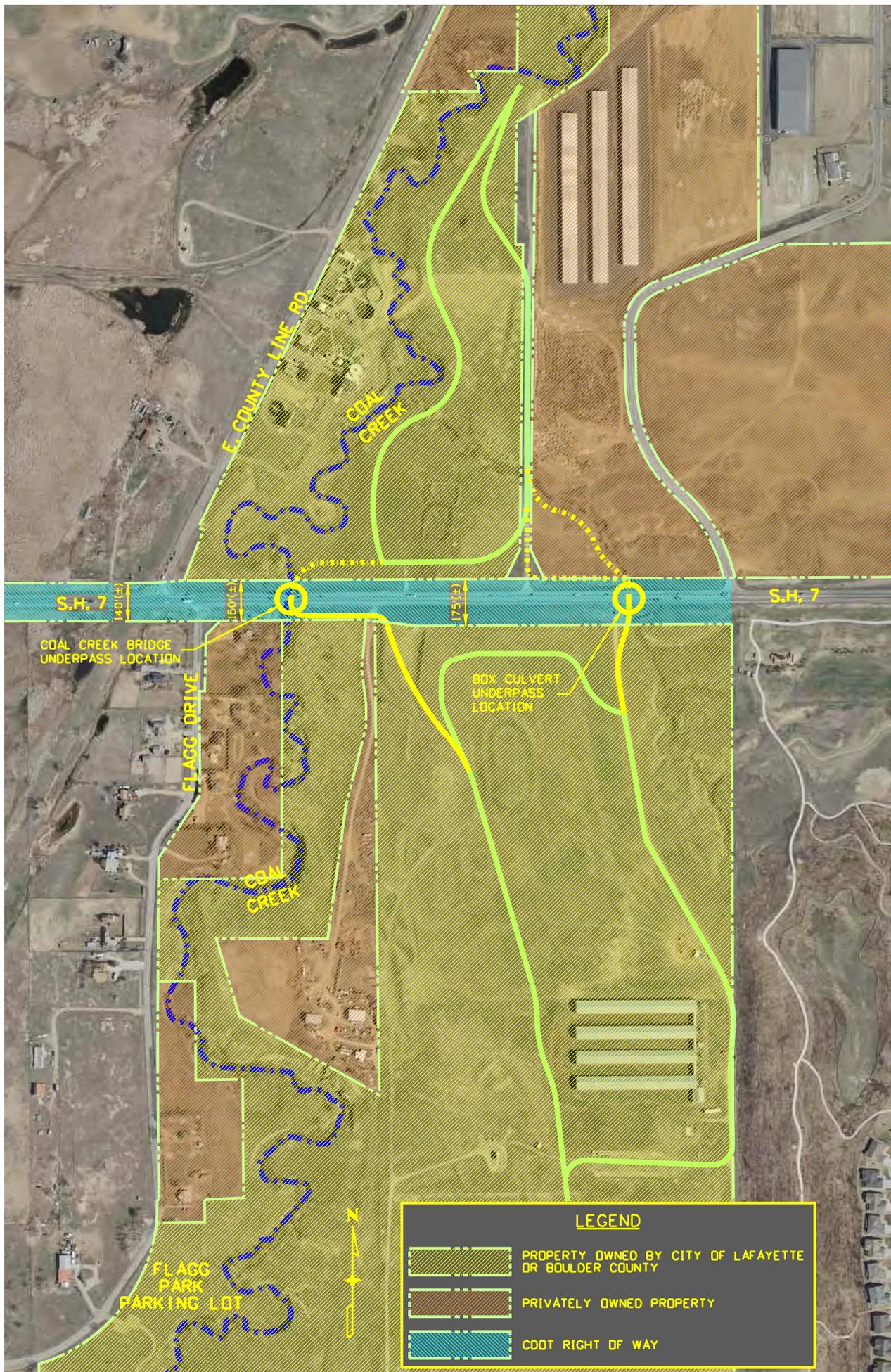


FIGURE 2 - PROPERTY OWNERSHIPS IN PROJECT AREA

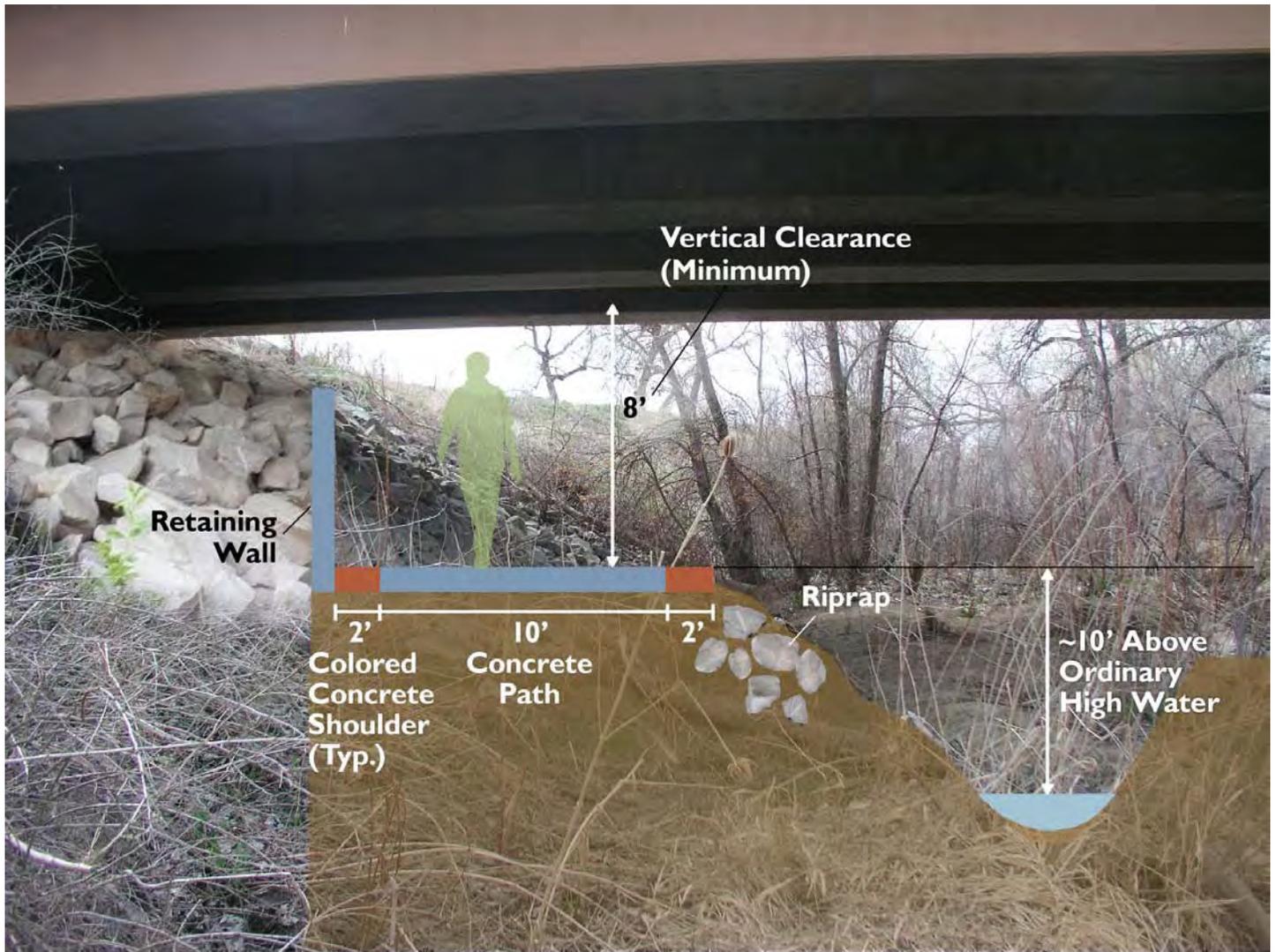


Figure 3–Coal Creek Bridge Underpass

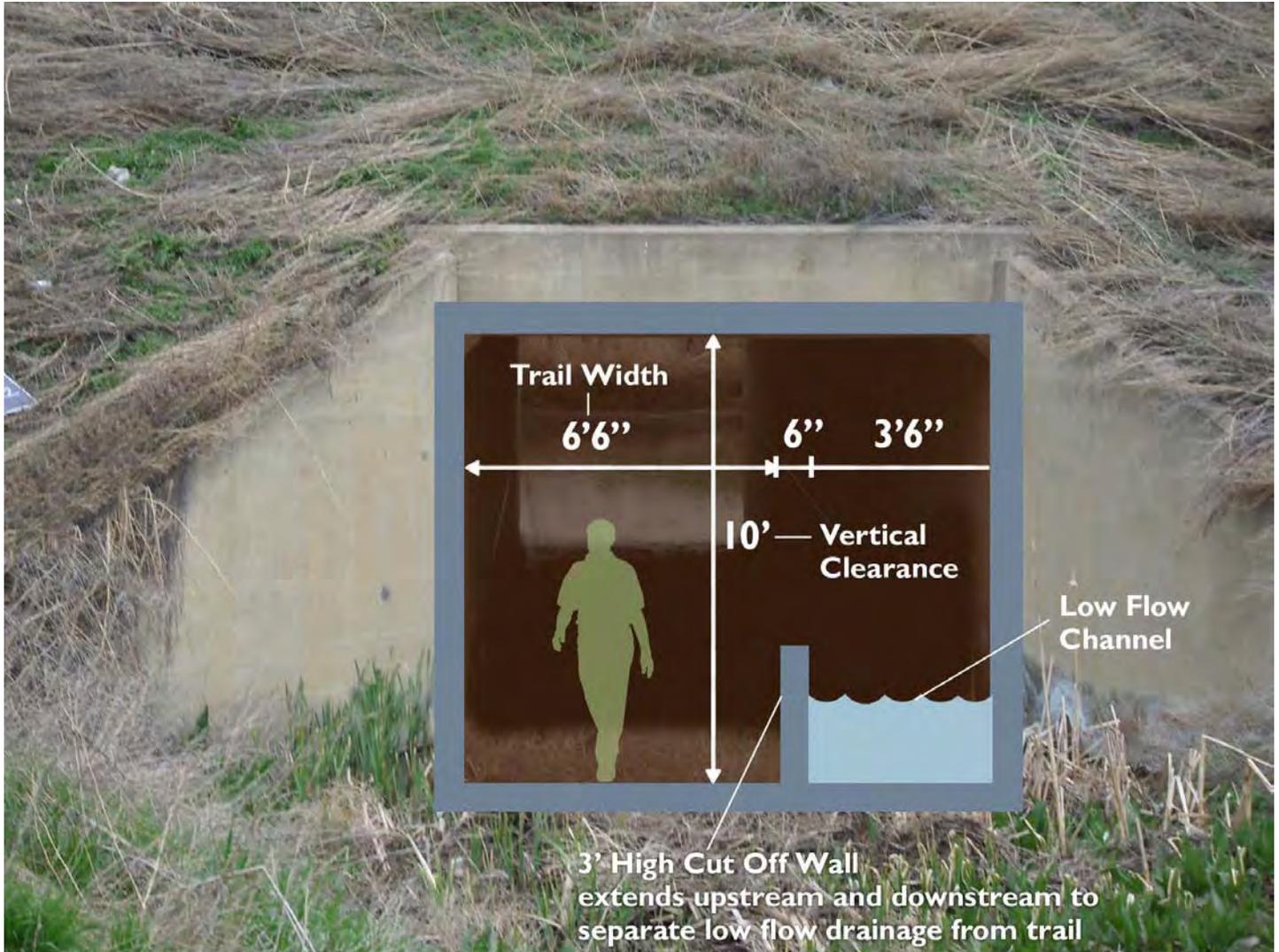


Figure 4–SH 7 Box Culvert–Alternative 1
(View Looking North)
Use existing culvert for trail and low flow channel

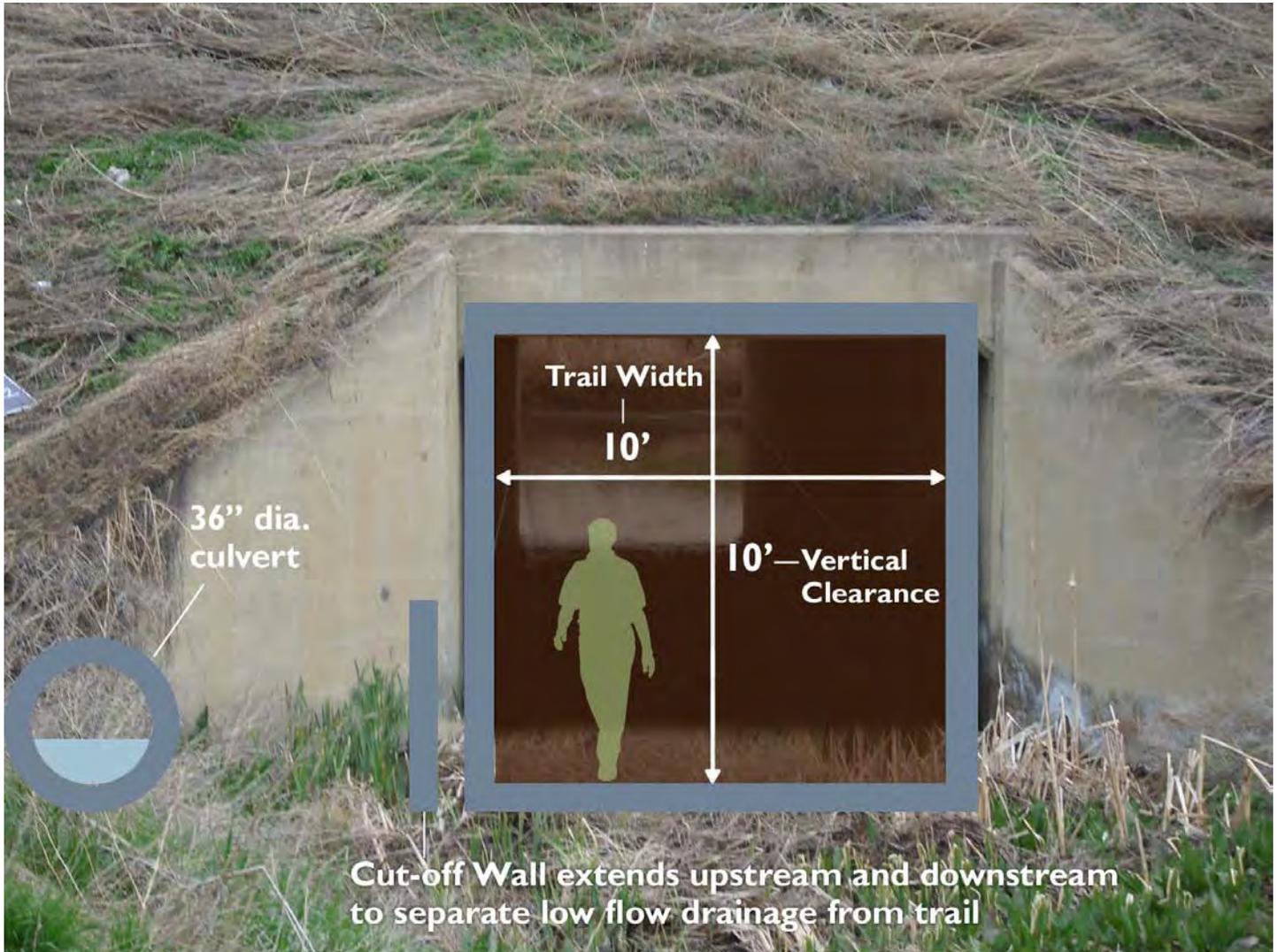


Figure 5–SH 7 Box Culvert–Alternative 2

(View looking South)

Use existing culvert for trail. Bore pipe under SH 7 for low flows.

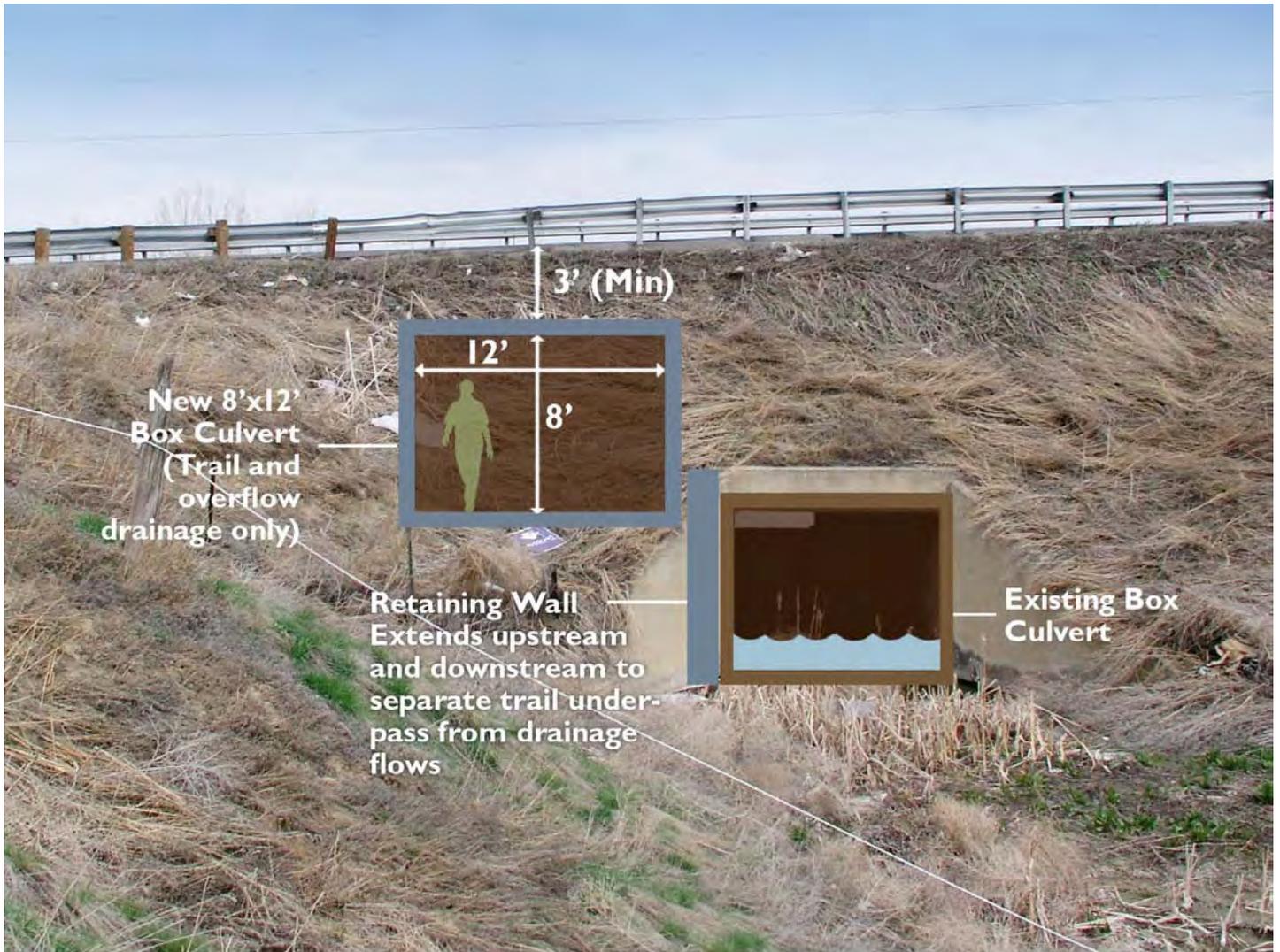


Figure 6–SH 7 Box Culvert–Alternative 3
(View looking North)

New culvert for trail. All drainage forced to existing culvert.

**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

Appendix B

Engineers Opinions of Probable Cost

COAL CREEK TRAIL UNDERPASS FEASIBILITY ANALYSIS SUMMARY OF OPINION OF PROBABLE COSTS

Underpass Alternative	Alignment Option	Estimated Construction + ROW Cost
Coal Creek Bridge Underpass	Alignment A	\$87,310
Box Culvert Alternative # 1	Alignment B	\$97,020
Box Culvert Alternative # 1	Alignment C	\$142,690
Box Culvert Alternative # 2	Alignment B	\$167,380
Box Culvert Alternative # 2	Alignment C	\$213,040
Box Culvert Alternative # 3	Alignment B	\$414,040
Box Culvert Alternative # 3	Alignment C	\$505,820

Structure No.

SEH Job No. 110780

Date: 6/3/2010

Underpass Feasibility Analysis - Coal Creek Bridge Underpass

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	0.5	0.5	1	3,000.00	3,000
203	EARTHWORK	LS	1		1	3,000.00	3,000
206	STRUCTURE EXCAVATION AND BACKFILL	LS		1	1	5,000.00	5,000
208	EROSION CONTROL DEVICES	LS		1	1	5,000.00	5,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	3,000.00	3,000
304	CRUSHER FINES PATH (6 INCH)	SY	1500		1,500	7.00	10,500
304	AGGREGATE BASE COURSE (CLASS 6)	CYD		20	20	75.00	1,500
420	GEOTEXTILE	SY	1500		1,500	1.50	2,250
506	RIPRAP	LS		1	1	2,500.00	2,500
	RETAINING WALL	LS		1	1	17,000.00	17,000
607	FENCE WORK	LS	1		1	500.00	500
608	CONCRETE BIKEWAY	SY		95	95	40.00	3,800
625	CONSTRUCTION SURVEYING	LS	0.5	0.5	1	4,000.00	4,000
626	MOBILIZATION	10%					6,110
	CONTINGENCY	30%					20,150
ESTIMATED CONSTRUCTION COSTS - COAL CREEK BRIDGE UNDERPASS				\$87,310			

ASSUMPTIONS: ESTIMATE IS BASED ON TRAIL CONSTRUCTION WITH LIMITS SHOWN ON ATTACHED EXHIBIT
ESTIMATE ASSUMES THAT UNDERPASS CONSTRUCTION WILL TAKE PLACE AS
PART OF A LARGER PROJECT. AS SUCH, SEVERAL ITEMS (EX. FIELD OFFICE,
SANITARY FACILITY) HAVE NOT BEEN INCLUDED
NO TREE REMOVAL IS DESIRED/NECESSARY
UTILITY RELOCATION NOT INCLUDED
ROW ESTIMATE IS BASED ON ESTIMATED LENGTH OF TRAIL IN PRIVATE PROPERTY
AND 20' IMPACT WIDTH. ESTIMATED UNIT PRICE OF \$8/SFT FOR PERM EASEMENT
CRUSHER FINE PAY ITEM INCLUDES EXCAVATION, EMBANKMENT, AND PROOFROLLING
GEOTEXTILE PAY ITEM INCLUDES CHICKEN WIRE

Structure No.

SEH Job No. 110780

Date: 6/3/2010

Underpass Feasibility Analysis - Box Culvert Alternative #1 - Using Alignment B

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	1,500.00	1,500
203	EARTHWORK	LS	1		1	3,000.00	3,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	3,000.00	3,000
304	CRUSHER FINES PATH (6 INCH)	SY	1600		1,600	7.00	11,200
420	GEOTEXTILE	SY	1600		1,600	1.50	2,400
506	RIPRAP	LS		1	1	1,500.00	1,500
	CUTOFF WALL	LS	0.5	0.5	1	8,500.00	8,500
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1.0		1	2,000.00	2,000
626	MOBILIZATION	10%					3,730
	RIGHT OF WAY	SFT	4200		4,200	8.00	33,600
	CONTINGENCY	30%					22,390
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #1 - Using Alignment B					\$97,020		

Underpass Feasibility Analysis - Box Culvert Alternative #1 - Using Alignment C

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	1,500.00	1,500
203	EARTHWORK	LS	1		1	3,000.00	3,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	3,000.00	3,000
304	CRUSHER FINES PATH (6 INCH)	SY	1250		1,250	7.00	8,750
420	GEOTEXTILE	SY	1250		1,250	1.50	1,880
506	RIPRAP	LS		1	1	1,500.00	1,500
	CUTOFF WALL	LS	0.5	0.5	1	8,500.00	8,500
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1.0		1	2,000.00	2,000
626	MOBILIZATION	10%					3,430
	RIGHT OF WAY	SFT	9000		9,000	8.00	72,000
	CONTINGENCY	30%					32,930
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #1 - Using Alignment C					\$142,690		

ASSUMPTIONS: ESTIMATE IS BASED ON TRAIL CONSTRUCTION WITH LIMITS SHOWN ON ATTACHED EXHIBIT
ESTIMATE ASSUMES THAT UNDERPASS CONSTRUCTION WILL TAKE PLACE AS
PART OF A LARGER PROJECT. AS SUCH, SEVERAL ITEMS (EX. FIELD OFFICE,
SANITARY FACILITY) HAVE NOT BEEN INCLUDED
NO TREE REMOVAL IS DESIRED/NECESSARY
UTILITY RELOCATION NOT INCLUDED
ROW ESTIMATE IS BASED ON ESTIMATED LENGTH OF TRAIL IN PRIVATE PROPERTY
AND 20' IMPACT WIDTH. ESTIMATED UNIT PRICE OF \$8/SFT FOR PERM EASEMENT
CRUSHER FINE PAY ITEM INCLUDES EXCAVATION, EMBANKMENT, AND PROOFROLLING
GEOTEXTILE PAY ITEM INCLUDES CHICKEN WIRE

Structure No.

SEH Job No. 110780

Date: 6/3/2010

Underpass Feasibility Analysis - Box Culvert Alternative #2 - Using Alignment B

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	1,500.00	1,500
203	EARTHWORK	LS	1		1	3,000.00	3,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	3,000.00	3,000
304	CRUSHER FINES PATH (6 INCH)	SY	1600		1,600	7.00	11,200
420	GEOTEXTILE	SY	1600		1,600	1.50	2,400
506	RIPRAP	LS		1	1	1,500.00	1,500
	CUTOFF WALL	LS	0.5	0.5	1	4,500.00	4,500
603	36 INCH REINFORCED CONCRETE PIPE - BORED	LF	30	100	130	400.00	52,000
603	36 INCH REINFORCED CONCRETE END SECTION	EA	2		2	600.00	1,200
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1		1	2,000.00	2,000
626	MOBILIZATION	10%					8,650
	RIGHT OF WAY	SFT	4200		4,200	8.00	33,600
	CONTINGENCY	30%					38,630
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #2 - Using Alignment B					\$167,380		

Underpass Feasibility Analysis - Box Culvert Alternative #2 - Using Alignment C

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	1,500.00	1,500
203	EARTHWORK	LS	1		1	3,000.00	3,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	3,000.00	3,000
304	CRUSHER FINES PATH (6 INCH)	SY	1250		1,250	7.00	8,750
420	GEOTEXTILE	SY	1250		1,250	1.50	1,880
506	RIPRAP	LS		1	1	1,500.00	1,500
	CUTOFF WALL	LS	0.5	0.5	1	4,500.00	4,500
603	36 INCH REINFORCED CONCRETE PIPE - BORED	LF	30	100	130	400.00	52,000
603	36 INCH REINFORCED CONCRETE END SECTION	EA	2		2	600.00	1,200
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1		1	2,000.00	2,000
626	MOBILIZATION	10%					8,350
	RIGHT OF WAY	SFT	9000		9,000	8.00	72,000
	CONTINGENCY	30%					49,160
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #2 - Using Alignment C					\$213,040		

ASSUMPTIONS: ESTIMATE IS BASED ON TRAIL CONSTRUCTION WITH LIMITS SHOWN ON ATTACHED EXHIBIT
ESTIMATE ASSUMES THAT UNDERPASS CONSTRUCTION WILL TAKE PLACE AS
PART OF A LARGER PROJECT. AS SUCH, SEVERAL ITEMS (EX. FIELD OFFICE,
SANITARY FACILITY) HAVE NOT BEEN INCLUDED
NO TREE REMOVAL IS DESIRED/NECESSARY
UTILITY RELOCATION NOT INCLUDED
ROW ESTIMATE IS BASED ON ESTIMATED LENGTH OF TRAIL IN PRIVATE PROPERTY
AND 20' IMPACT WIDTH. ESTIMATED UNIT PRICE OF \$8/SFT FOR PERM EASEMENT
CRUSHER FINE PAY ITEM INCLUDES EXCAVATION, EMBANKMENT, AND PROOFROLLING
GEOTEXTILE PAY ITEM INCLUDES CHICKEN WIRE

Structure No.

SEH Job No. 110780

Date: 6/3/2010

Underpass Feasibility Analysis - Box Culvert Alternative #3 - Using Alignment B

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	2,000.00	2,000
203	EARTHWORK	LS	1		1	10,000.00	10,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	5,000.00	5,000
304	CRUSHER FINES PATH (6 INCH)	SY	1200		1,200	7.00	8,400
420	GEOTEXTILE	SY	1200		1,200	1.50	1,800
506	RIPRAP	LS		1	1	1,500.00	1,500
	RETAINING WALL	LS	1		1	100,000.00	100,000
601	CONCRETE CLASS D (BOX CULVERT)	CY		160	160	550.00	88,000
602	REINFORCING STEEL (EPOXY COATED)	LB.		40,000	40,000	1.25	50,000
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1		1	5,000.00	5,000
	TRAFFIC CONTROL	LS		1	1	15,000.00	15,000
626	MOBILIZATION	10%					27,590
	CONTINGENCY	30%					95,550
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #3 - Using Alignment B					\$414,040		

Underpass Feasibility Analysis - Box Culvert Alternative #3 - Using Alignment C

ITEM NO.	CONTRACTOR ANTICIPATED CONTRACT ITEM	UNIT	TRAIL	UNDERPASS	TOTAL	UNIT PRICE '\$'	COST '\$'
201	CLEARING AND GRUBBING	LS	1		1	2,000.00	2,000
203	EARTHWORK	LS	1		1	10,000.00	10,000
208	EROSION CONTROL DEVICES	LS	1		1	3,000.00	3,000
212	GROUND RESTORATION (TOPSOIL, SEEDING, ETC)	LS	1		1	5,000.00	5,000
304	CRUSHER FINES PATH (6 INCH)	SY	1050		1,050	7.00	7,350
420	GEOTEXTILE	SY	1050		1,050	1.50	1,580
506	RIPRAP	LS		1	1	1,500.00	1,500
	RETAINING WALL	LS	1		1	100,000.00	100,000
601	CONCRETE CLASS D (BOX CULVERT)	CY		160	160	550.00	88,000
602	REINFORCING STEEL (EPOXY COATED)	LB.		40,000	40,000	1.25	50,000
607	FENCE WORK	LS	1		1	1,200.00	1,200
625	CONSTRUCTION SURVEYING	LS	1.0		1	5,000.00	5,000
	TRAFFIC CONTROL	LS		1	1	15,000.00	15,000
626	MOBILIZATION	10%					27,460
	RIGHT OF WAY	SFT	9000		9,000	8.00	72,000
	CONTINGENCY	30%					116,730
ESTIMATED CONSTRUCTION COSTS - BOX CULVERT ALTERNATIVE #3 - Using Alignment C					\$505,820		

ASSUMPTIONS: ESTIMATE IS BASED ON TRAIL CONSTRUCTION WITH LIMITS SHOWN ON ATTACHED EXHIBIT
ESTIMATE ASSUMES THAT UNDERPASS CONSTRUCTION WILL TAKE PLACE AS
PART OF A LARGER PROJECT. AS SUCH, SEVERAL ITEMS (EX. FIELD OFFICE,
SANITARY FACILITY) HAVE NOT BEEN INCLUDED
NO TREE REMOVAL IS DESIRED/NECESSARY
UTILITY RELOCATION NOT INCLUDED
ROW ESTIMATE IS BASED ON ESTIMATED LENGTH OF TRAIL IN PRIVATE PROPERTY
AND 20' IMPACT WIDTH. ESTIMATED UNIT PRICE OF \$8/SFT FOR PERM EASEMENT
CRUSHER FINE PAY ITEM INCLUDES EXCAVATION, EMBANKMENT, AND PROOFROLLING
GEOTEXTILE PAY ITEM INCLUDES CHICKEN WIRE

**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

Appendix C

Summary Evaluation Matrix

SH 7 / Coal Creek Trail Underpass Alternatives - Summary Evaluation Matrix

Option:	Coal Creek Bridge	Box Culvert Alt. #1	Box Culvert Alt. #2	Box Culvert Alt. #3	Notes:		
Is Option Feasible?	Yes	No	Potentially	Yes	Box Culvert Alt. #1 doesn't provide adequate trail width. Box Culvert Alt. #2 may not meet CDOT hydraulic design criteria		
Evaluation Criteria	Advantage To ↓						
1 Trail Alignment - Design Criteria Impact	Meets all design criteria	Trail width sub-standard	Meets design criteria, but requires ROW from private property owner for vertical alignment	Meets all design criteria	Coal Creek Bridge	Box Culvert Alternative #3	Box Culvert Alternative #1 determined NOT FEASIBLE due to insufficient trail width.
2 Trail Alignment - Overall Trail Alignment Compatibility	Works acceptably with all potential trail alignments. Most naturally fits best with westerly or central trail alignments.	Works acceptably with all potential trail alignments. Most naturally fits best with central or eastern trail alignments.	Works acceptably with all potential trail alignments. Most naturally fits best with central or eastern trail alignments.	Works acceptably with all potential trail alignments. Most naturally fits best with central or eastern trail alignments.	Depends on overall trail alignment decisions		
3 Trail User Experience	Best - open underpass with flowing stream nearby	Poor - Long, very narrow concrete tunnel effect	Not great - Long, narrow, concrete tunnel effect	Not great - Long, narrow, concrete tunnel effect	Coal Creek Bridge		Coal Creek Bridge Underpass Alternative provides for best trail user experience.
4 Drainage Considerations	Good - Trail can remain high and dry except for flood flows	Fair - Trail can be protected from low flows via a cut-off wall. Will be flooded in higher storm events.	Fair - Trail can be protected from low flows via a separate pipe and cut-off wall. Will be flooded in higher storm events. May not meet CDOT hydraulic design criteria	Good - Separates trail from drainage flows; underpass remains dry except for flood overflows	Coal Creek Bridge	Box Culvert Alternative #3	Box Culvert Alternative #3 may not be feasible, depending on the CDOT hydraulic design criteria. This would be an unusual design and would require CDOT review.
5 Flooded Trail Considerations	Users have option of crossing at-grade with traffic signal protection at SH7/E County Line Rd traffic signal	Users have to cross at-grade.	Users have to cross at-grade.	Users have to cross at-grade.	Coal Creek Bridge		Optimizing safety crossing SH 7 when the trail is flooded should be a high priority.
6 Private Property Impacts	None. All underpass work can take place on public property	Requires an easement or right-of-way from private property owner	Requires an easement or right-of-way from private property owner	Can be accomplished within public right-of-way unless "Alignment C - North" is selected	Coal Creek Bridge	Box Culvert Alternative #3	Only Coal Creek Bridge Underpass Alternative eliminates all possibility of having to acquire public property with any and all possible trail alignments north and south of SH 7
7 Potential Impact to Adjacent Neighbors	Some in Flagg Park neighborhood prefer underpass and trail further to the east	May be more readily used by Anthem neighborhood residents, as is closer to them.	May be more readily used by Anthem neighborhood residents, as is closer to them	May be more readily used by Anthem neighborhood residents, as is closer to them	Box Culvert Alternative #2	Box Culvert Alternative #3	Moves underpass away from Flagg Drive neighborhood.
8 Environmental Impacts	Some impact to riparian and possible wetland area	Some impact to wetlands up- and downstream	Some impact to wetlands up- and downstream	Due to trail culvert location, minimal wetlands impact	Coal Creek Bridge	Box Culvert Alternative #3	
9 Potential CDOT Issues	Will require cooperation, design approvals, and permits from CDOT	Will require cooperation, design approvals, and permits from CDOT	Will require cooperation, design approvals, and permits from CDOT	Will require cooperation, design approvals, and permits from CDOT	Neutral		Given the unknown hydraulic design criteria for the existing culvert, Box Culvert Alternatives #1 and #2 may require extensive hydraulic design/review. Box Culvert Alternative #3 and Coal Creek Bridge Alternative will require structural design/review.
10 Additional Coordination Requirements	None	May require coordination w/ Weld County	May require coordination w/ Weld County	May require coordination w/ Weld County	Coal Creek Bridge		Probably not a big issue.
11 Maintenance Considerations	Maintenance agreement required; add'l maint. may be needed after high water events	Maintenance agreement required; add'l maint. may be needed after high water events	Maintenance agreement required; add'l maint. may be needed after high water events	Maintenance agreement required; add'l maint. may be needed after high water events	Neutral		
12 Cost	\$87,000	Ranges from \$97,000 to \$143,000	Ranges from \$167,000 to \$213,000	Ranges from \$414,000 to \$505,000	Coal Creek Bridge		The Coal Creek Bridge underpass costs are significantly lower than all Box Culvert Options.

**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

Appendix D

Summary of Underpass Alternative Advantages and Disadvantages

SH 7 / Coal Creek Trail Underpass Analysis Summary of Advantages and Disadvantages

SH 7 Bridge over Coal Creek

Advantages to Underpass:

- 5% maximum grade can be met within publicly owned property
- Works best with westerly or central trail alignments
- Pleasing trail user experience – provides open undercrossing along flowing creek
- Crossing remains dry except at flood levels
- At-grade crossing with traffic signalization within 500 feet (important during flood events when trail is flooded)
- No private property impacts
- Lowest relative cost of approximately \$87,000

Disadvantages to Underpass:

- Requires 275 feet of additional trail length to meet grades
- Requires additional trail length for easterly trail alignments
- Crossing becomes flooded at high water events
- Some adjacent neighbors along Flagg Drive may prefer a more easterly location

SH 7 Box Culvert Alternative 1

Advantages to Underpass:

- Works best with easterly or central trail alignments
- May be preferred by some neighbors along Flagg Drive
- Cost ranges from \$97,000 to \$143,000 (depending on alignment and actual cost of Right of Way acquisition)

Disadvantages to Underpass:

- Does not meet trail width design criteria
- Requires 430 feet of additional trail length to meet grades for Alignment Option B; requires 220 feet of additional trail length to meet grades for Alignment Option C
- Alignment Options B and C both require the use of private property – may not be possible
- Requires additional trail length for westerly trail alignments
- Poor trail user experience – long narrow tunnel effect
- Reduces hydraulic capacity of underpass; hydraulic requirements unknown; CDOT will need to review hydraulic design
- Poor at-grade crossing options for user during flood event

SH 7 Box Culvert Alternative 2

Advantages to Underpass:

- Meets trail design criteria
- Works best with easterly or central trail alignments
- May be preferred by some neighbors along Flagg Drive
- Cost ranges from \$167,000 to \$213,000 (depending on alignment and actual cost of Right of Way acquisition)

Disadvantages to Underpass:

- Requires 430 feet of additional trail length to meet grades for Alignment Option B; requires 220 feet of additional trail length to meet grades for Alignment Option C
- Alignment Options B and C both require the use of private property – may not be possible
- Requires additional trail length for westerly trail alignments
- Poor trail user experience – long narrow tunnel effect
- CDOT will need to review the hydraulic design. The hydraulic design criteria are unknown at this time. The potential exists that CDOT will not permit this approach or require significant modifications.
- Poor at-grade crossing options for user during flood event

SH 7 Box Culvert Alternative 3

Advantages to Underpass:

- Maintains hydraulic capacity of existing structure
- Meets trail design criteria without additional trail length requirements
- Works best with easterly or central trail alignments
- May be preferred by some neighbors along Flagg Drive

Disadvantages to Underpass:

- Alignment Options B and C both require the use of private property – may not be possible
- Requires additional trail length for westerly trail alignments
- Poor trail user experience – long narrow tunnel effect
- CDOT will need to review structural design
- Requires large retaining wall between the two structures
- Poor at-grade crossing options for user during flood event
- Highest relative cost range between \$414,000 and \$505,000 (depending on alignment and actual cost of Right of Way acquisition)

**Coal Creek Trail Feasibility Analysis
Boulder County, Colorado**

Appendix E

Photos, Existing Plans and Inspection Reports



SH 7 Bridge over Coal Creek



SH 7 Bridge over Coal Creek



SH 7 Bridge over Coal Creek



SH 7 Bridge over Coal Creek



SH 7 Box Culvert



SH 7 Box Culvert



SH 7 Box Culvert



SH 7 Box Culvert

AS CONSTRUCTED			
NO REVISIONS	REVISED 2.2.90	VOID	
FEDERAL ROAD REGION NO.	DIVISION	PROJECT NUMBER	SHEET NUMBER
XXX	COLORADO	BRF007-3(3)	9

GENERAL NOTES

ALL WORK SHALL BE DONE ACCORDING TO THE STANDARD SPECIFICATIONS OF THE DIVISION OF HIGHWAYS, STATE OF COLORADO, APPLICABLE TO THE PROJECT.

STRUCTURE EXCAVATION AND BACKFILL SHALL BE IN ACCORDANCE WITH STANDARD M-206-2 OR AS SHOWN ON THE PLANS.

EXPANSION JOINT MATERIAL SHALL MEET AASHTO SPECIFICATION M-213. IT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE WORK.

THE FOLLOWING STRUCTURAL STEEL SHALL BE AASHTO M-183 (ASTM A-36): PILING AND PIER NOSE ANGLE.

GRADE 60 REINFORCING STEEL IS REQUIRED.

ALL REINFORCING STEEL SHALL BE EPOXY COATED UNLESS OTHERWISE NOTED.

THE FOLLOWING TABLE GIVES THE MINIMUM LAP SPLICE LENGTH FOR REINFORCING BARS:

BAR SIZE #4 #5 #6 #7 #8 #9 #10 #11

SPLICE LENGTH FOR CLASS B CONCRETE 1'-3" 1'-7" 2'-0" 2'-9" 3'-9" 4'-7" 5'-9" 7'-1"

SPLICE LENGTH FOR CLASS D CONCRETE 1'-3" 1'-7" 1'-10" 2'-3" 2'-11" 3'-7" 4'-9" 5'-10"

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING CONSTRUCTION.

E.F. = EACH FACE
 F.F. = FAR FACE
 N.F. = NEAR FACE

(N) = NON-EPOXY COATED REINFORCING STEEL

PERMANENT STEEL BRIDGE DECK FORMS MAY BE USED AT THE CONTRACTORS OPTION, AT NO ADDITIONAL COST TO STATE OF COLORADO. NO STEEL DECK FORMS WILL BE ALLOWED IN THE CANTILEVERED PORTIONS OF THE DECK. ANY ADDITIONAL CONCRETE REQUIRED DUE TO THE USE OF PERMANENT FORMS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE WORK.

THE INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.

FOR STRUCTURE NUMBER INSTALLATION, SEE STANDARD S-614-12.

ADDITIONAL COPIES OF BRIDGE GEOMETRY WILL BE PROVIDED, UPON REQUEST, FROM THE DISTRICT.

ALL EXPOSED CONCRETE SURFACES, DOWN TO 1'-0" BELOW FINISHED GRADE, SHALL BE FINISHED WITH A CLASS 1 FINISH FOLLOWED BY AN APPLICATION OF COLORED COATING, MEETING THE REQUIREMENTS IN SPECIAL PROVISION REVISION OF SECTION 601. STRUCTURAL CONCRETE COATING, AND AS SHOWN ON THE PLANS.

DESIGN DATA

CURRENT AASHTO SPECIFICATIONS.

LIVE LOAD: AASHTO HS-20-44 AND INTERSTATE ALTERNATE

DEAD LOAD: ASSUMES 48 LBS. PER SQ. FT. FOR BITUMINOUS PAVEMENT AND 12 PSF. FOR STAY-IN-PLACE FORMS.

DESIGN METHOD: WORKING STRESS DESIGN, EXCEPT LOAD FACTOR FOR PIER COLUMNS.

REINFORCED CONCRETE:

★ CLASS B CONCRETE: $f_c = 1200$ psi, $n = 9$, $f_s = 3000$ psi
 CLASS D CONCRETE: $f_c = 1800$ psi, $n = 8$, $f_s = 4500$ psi

REINFORCING STEEL: $f_s = 24,000$ psi, $f_y = 60,000$ psi

STRUCTURAL STEEL: AASHTO M-183 (ASTM A-36) $f_s = 20,000$ psi

PRESTRESSED CONCRETE. $f_s = 270,000$ psi
 $f_c =$ (SEE DETAILS)

★ SUBSTITUTE CLASS A CONCRETE FOR CLASS B CONCRETE.

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	SUPER-STRUCTURE	ABUT. 1	PIER 2	ABUT. 3	TOTAL
202	REMOVAL OF BRIDGE	EACH					1 /
206	STRUCTURE EXCAVATION	CU YD		333	71	350	754 /
206	STRUCTURE BACKFILL (CLASS 1)	CU YD		13		13	26 /
206	STRUCTURE BACKFILL (CLASS 2)	CU YD			58		58 /
403	HOT BITUMINOUS PAVEMENT (GRADING E) (HAUL & ASPHALT)	TON	79				79 /
502	STEEL PILING (HP 14X89)	LIN FT		+64 136.40		+20 128.31	224.205 /
503	DRILLED CAISSON (36")	LIN FT			69		69 /
509	STRUCTURAL STEEL (GALVANIZED)	LB			473		473 /
515	WATERPROOFING (MEMBRANE)	SQ YD	714				714 /
601	CONCRETE CLASS B (BRIDGE)	CU YD		22	62	23	107 /
601	CONCRETE CLASS D (BRIDGE)	CU YD	+82	-20	+3	-21	246.225 /
601	STRUCTURAL CONCRETE COATING	SQ FT	2,790	354	1,128	354	4,626 /
602	REINFORCING STEEL	LB			1,005		1,005 /
602	REINFORCING STEEL (EPOXY COATED)	LB	61,407	3,772	9,689	3,772	78,640 /
606	BRIDGE RAIL TYPE 10	LIN FT	348				348 /
613	4 INCH ELECTRICAL CONDUIT	LIN FT	356				356 /
618	PRESTRESSED CONCRETE UNIT (BOX SECTION) (70 TO 75 FEET)	EACH	8				8 /

INDEX OF DRAWINGS

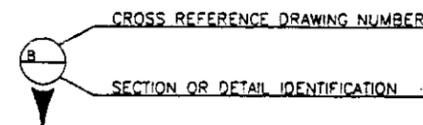
DWG. NO. B1	GENERAL INFORMATION - SUMMARY OF QUANTITIES
DWG. NO. B2	GENERAL LAYOUT
DWG. NO. B3	ENGINEERING GEOLOGY
DWG. NO. B4	BRIDGE HYDRAULIC INFORMATION
DWG. NO. B5	CONSTRUCTION LAYOUT - PILING AND CAISSON LAYOUT
DWG. NO. B6	ABUTMENT DETAILS
DWG. NO. B7	WINGWALL DETAILS
DWG. NO. B8	PIER 2 DETAILS
DWG. NO. B9	SUPERSTRUCTURE DETAILS
DWG. NO. B10	PRECAST BOX GIRDER DETAILS
DWG. NO. B11	BRIDGE RAIL TYPE 10
DWG. NO. B12	BRIDGE DECK ELEVATIONS
DWG. NO. B13	ROADWAY APPROACHES

BRIDGE DESCRIPTION

2 SPAN (70'-9", 70'-9") BRIDGE
 CONCRETE SLAB AND PRESTRESSED BOX GIRDER
 CONTINUOUS, SPREAD.

S.H. 7 OVER COAL CREEK
 44'-0" ROADWAY CURB TO CURB. 90° 00' SKEW.
 1'-3" CURBS. BRIDGE RAIL TYPE 10.

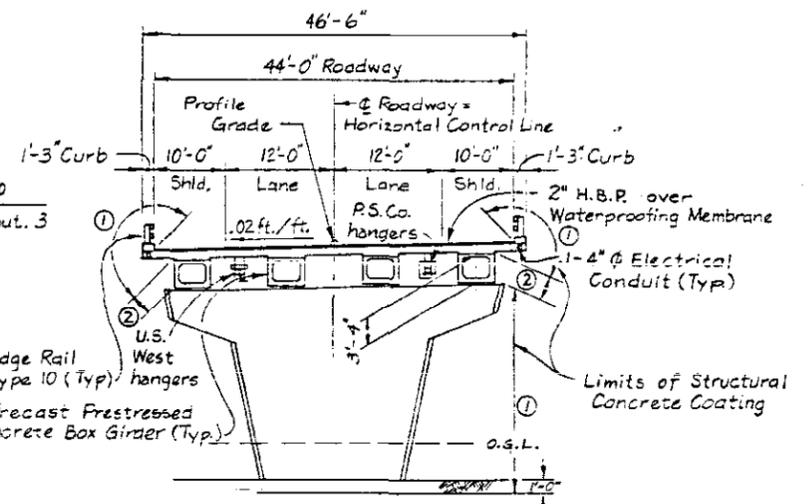
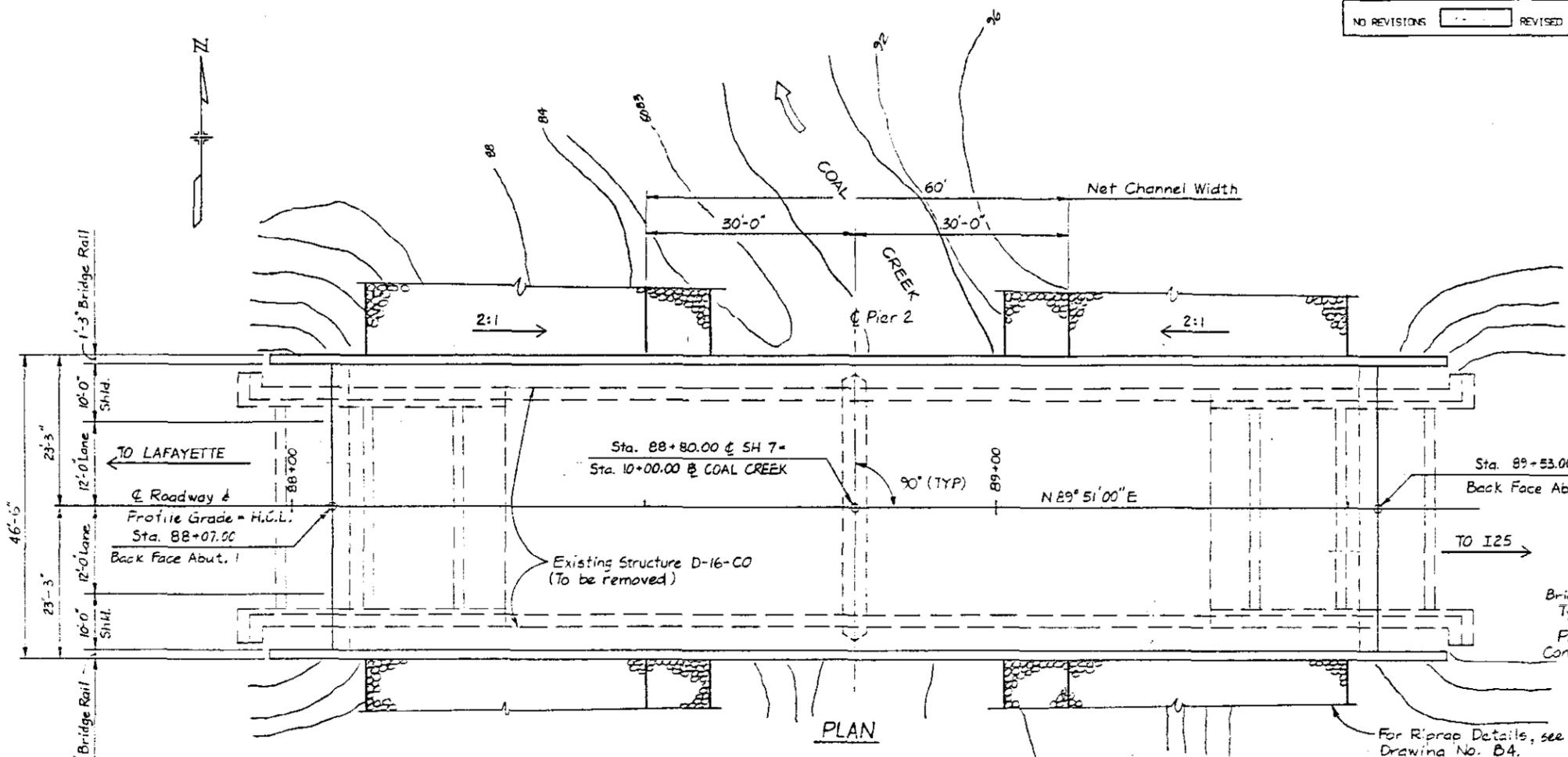
DIVISION OF HIGHWAYS		
GENERAL INFORMATION		
SUMMARY OF QUANTITIES		
Station 88+07.00 to Station 89+53.00		
Near LAFAYETTE S. 36/6 T. 1N/1S R. 69W		
Designer L. SANCHEZ	Structure	D-16-DM
Detailer J. VIRDI	Numbers	
Drawing Number B1		of 13 Drawings



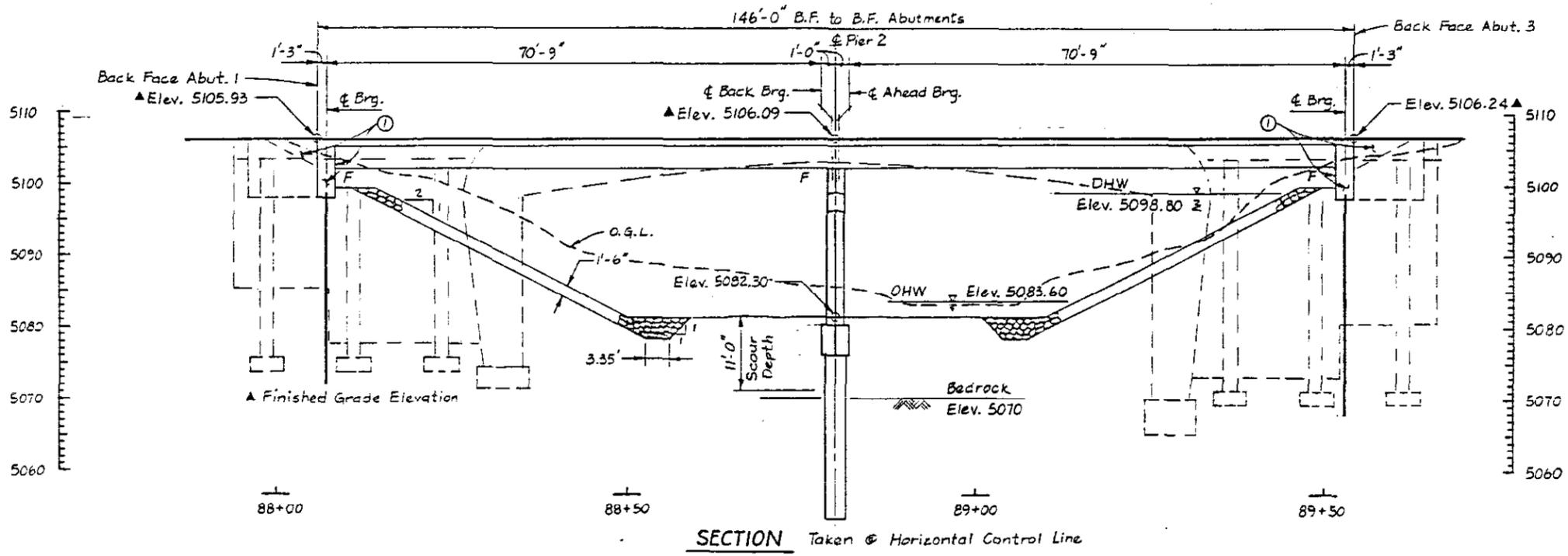
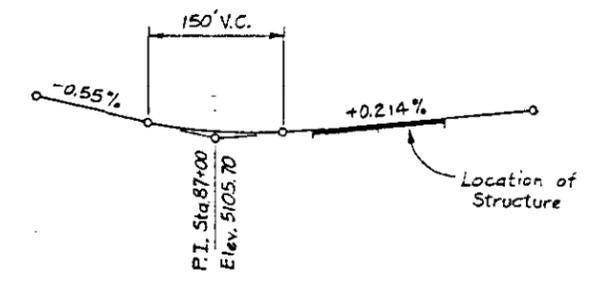
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NO REVISIONS	REVISED	VOID

FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
VIII	COLO.	BRF 007-3(3)	10	25

REVISIONS	



- ① Color Coating Finish (Typ) (No. 31667) See Project Spec. Provision 601.
- ② Color Coating Finish (Typ) (No. 30219) See Project Spec. Provision 601.

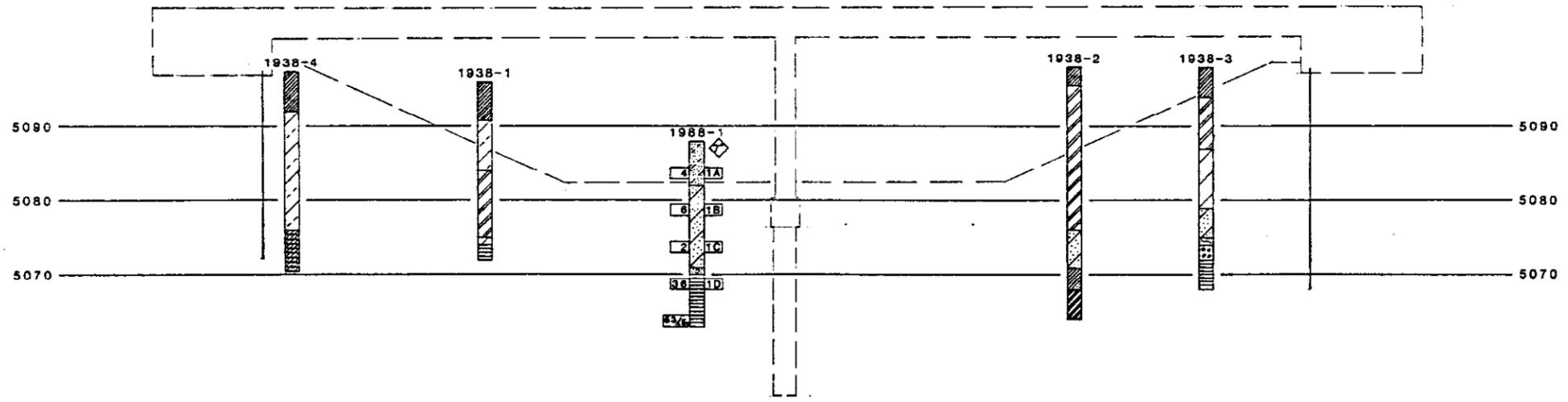
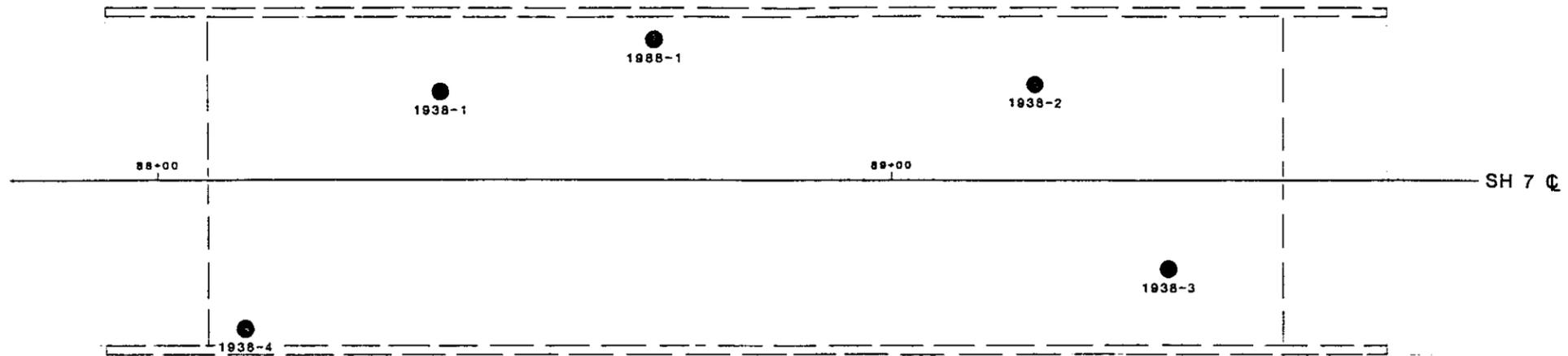


PROFILE GRADE Live Loading = HS20-44 and Interstate Alternate

DIVISION OF HIGHWAYS	
GENERAL LAYOUT	
DESIGNER L. Sanchez	STRUCTURE D-16-DM
DETAILER L. Sanchez	NUMBERS
DRAWING NUMBER B 2	OF 13 DRAWINGS

DESIGNED BY	DATE	CHECKED BY	DATE

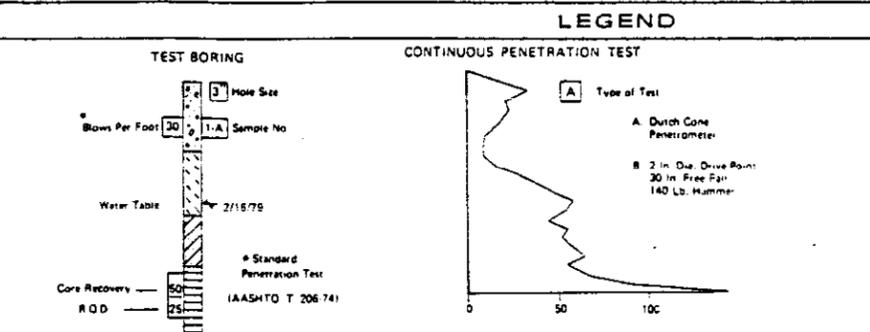
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NO REVISIONS	REVISED	VOID	VIII	COLORADO	BRF 007-3(3)	//	1



TEST HOLES PREFIXED BY '1938' WERE COPIED FROM 1938 PLAN SHEET

Sample No.	Depth	Classification		Grading Analysis				Atterberg Limits			Moist. Cont. %	Wet Unit Weight P.C.F.	Unconfined Strength Q_u T.S.F.	Tensile Shear Strength			Dia. of Sample (inches)
		Coras at Eng. in. Visual	AASHTO	Percent				Liquid Limit %	Plastic Limit %	Plastic Index %				C	B	A	
				Gravel	Coarse Sand	Fine Sand	Silt and Clay										
1C	3.5-5.0	SILTY SAND	A-2-4(0)	14	21	30	35	22	17	5	13.5						
1B	5.5-10.0	SANDY CLAY	A-8(5)	0	7	37	56	29	14	15	23.7						
1C	13.5-15.0	SANDY CLAY	A-8(5)	0	0	30	70	30	18	12	27.5						
1D	18.5-20.0	SHALE	A-7-5(33)	0	0	2	98	58	30	28	20.0						

TYPE OF MATERIAL	
	SOFT CLAY
	FIRM CLAY
	HARD CLAY
	VERY HARD CLAY
	SOFT SILTY CLAY
	SOFT SANDY CLAY
	SILTY SAND
	GRAVELLY SAND
	GRAVEL
	SOFT SANDSTONE
	HARD CLAY OR SHALE WITH GRAVEL
	HARD SHALE



DIVISION OF HIGHWAYS

ENGINEERING GEOLOGY

COAL CREEK,

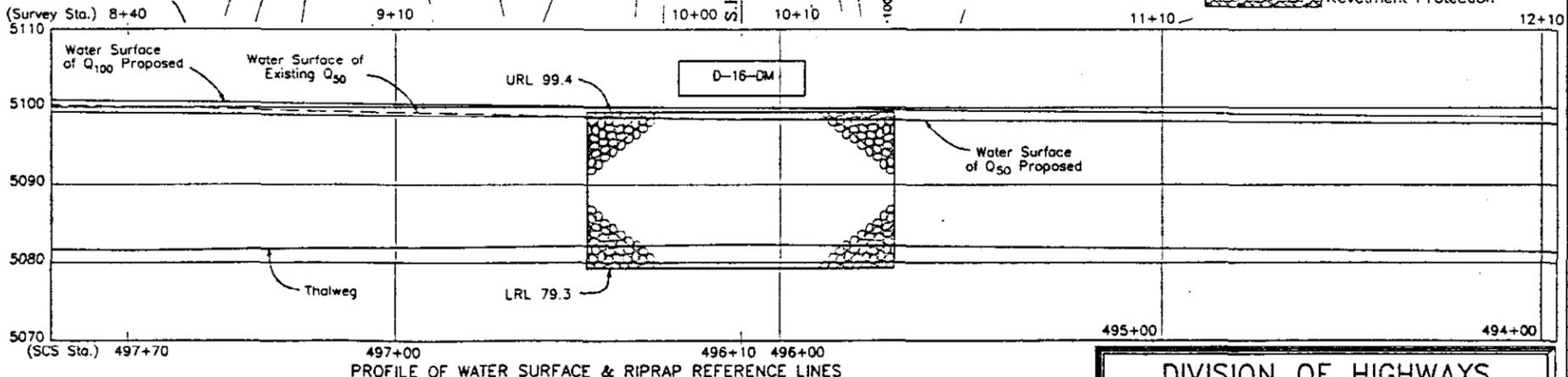
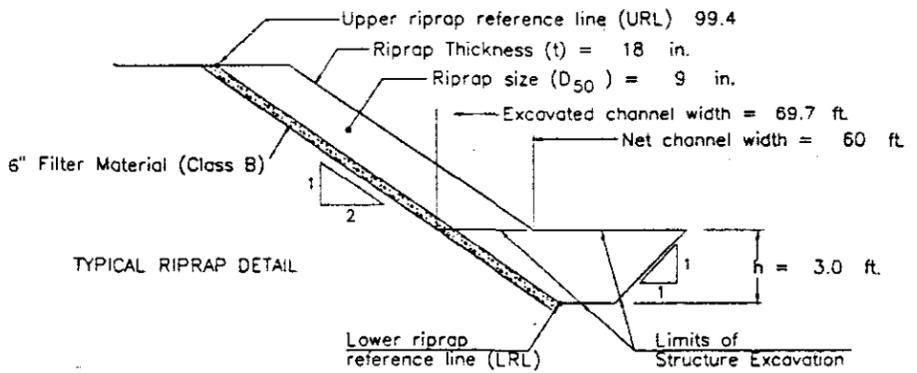
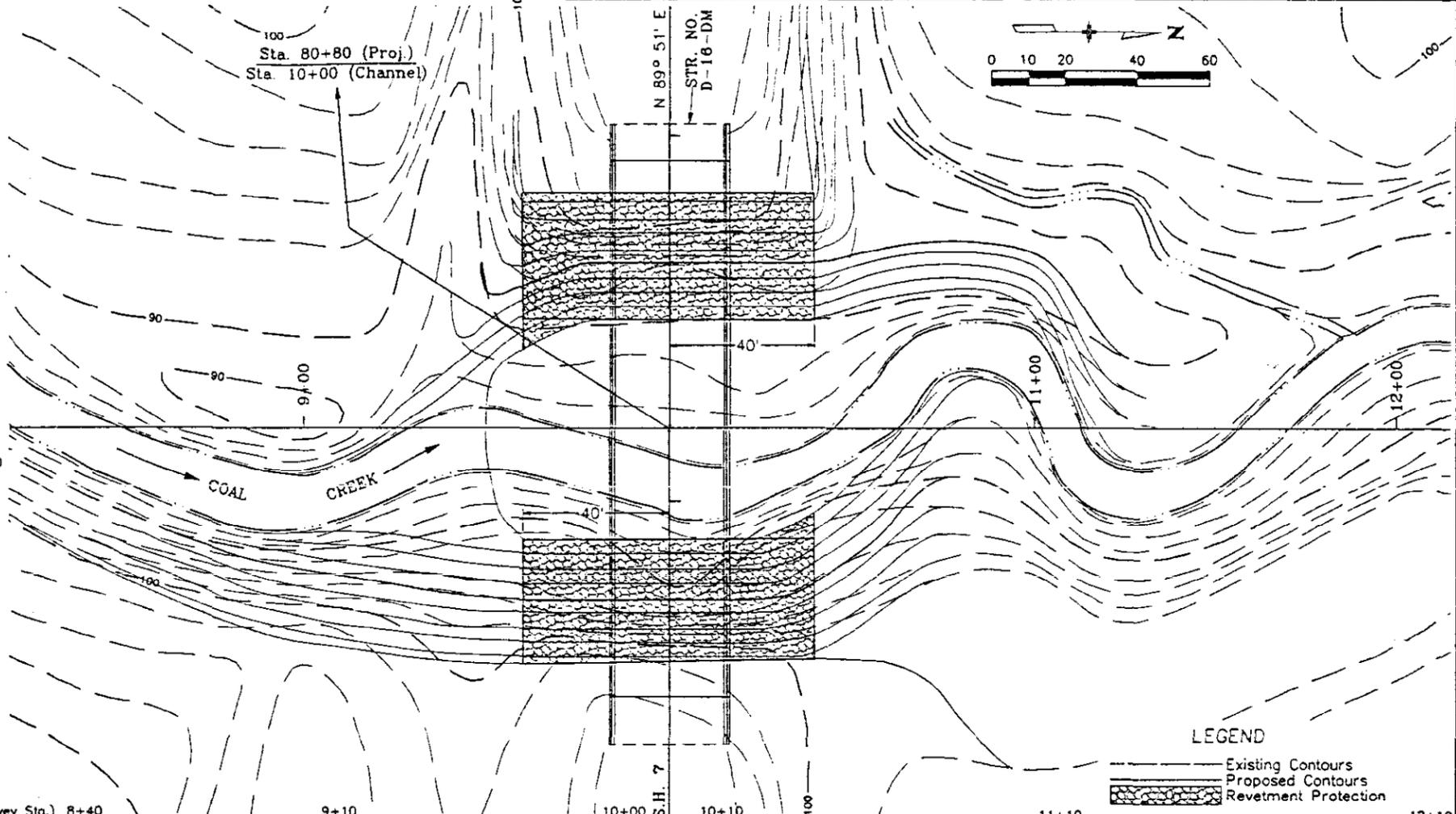
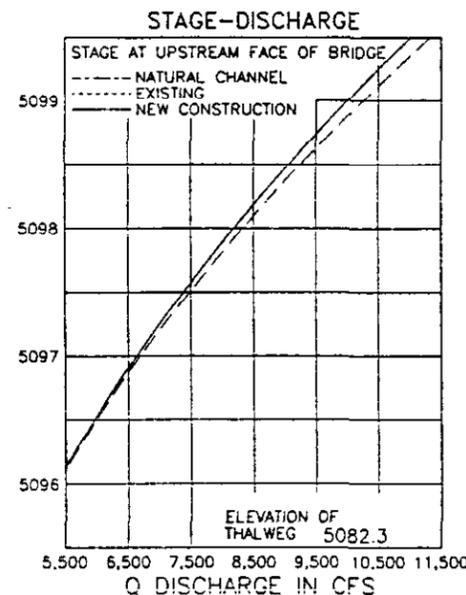
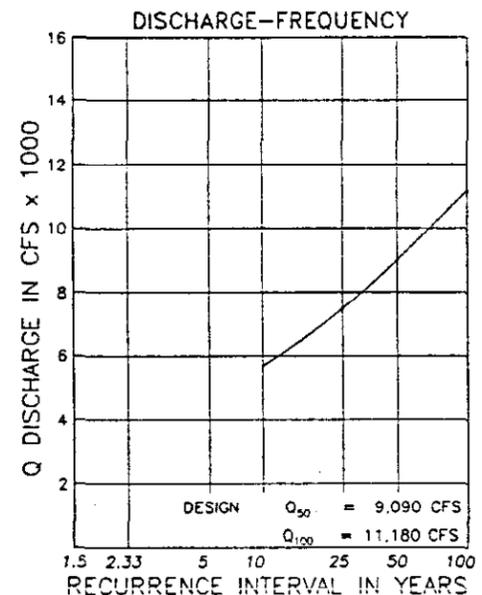
1.7 MI E OF JCT 287

Geologist: PM	Date: 1-6-89
Drawn by: TM	
Checked by: PM	

STRUCTURE NO. D-16-DM

BRIDGE HYDRAULIC INFORMATION

AS CONSTRUCTED		FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.
NO REVISIONS	REVISED	VIII	COLO.	BRF 007-3(3)	12



Drainage Area 64 Sq. Mi.

CHANNEL DESCRIPTION

Bottom Material - Cohesive Non Cohesive
 Bottom Material Size - Clay Silt Sand Gravel
 Cobbles Other
 Stream Form - Straight Meandering Braided
 Mannings "n" for Design - Channel .08 Overbank 0.1
 Debris - Brush Trees/Logs Ice Other

COMPARISON OF HYDRAULICS *

	Velocity	Freeboard	Max. Backwater
Natural Channel	fps	ft	ft
Existing	8.39	1.0	0.1
Proposed	6.08	2.0	0.1

*AT PROPOSED BRIDGE LOCATION DURING DESIGN DISCHARGE

DIVISION OF HIGHWAYS

BRIDGE HYDRAULIC INFORMATION

Across COAL CREEK Approved By Date
 Designer G.C. MOGS STRUCTURE D-16-DM
 Detailer NUMBERS OF 13 DRAWINGS
 DRAWING NUMBER B 4

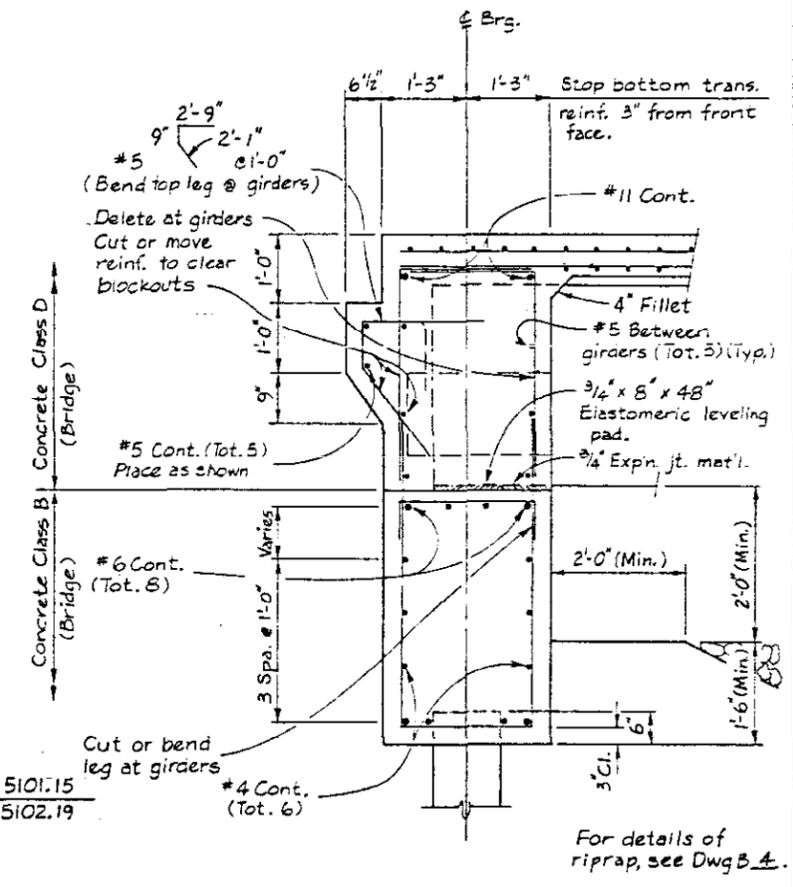
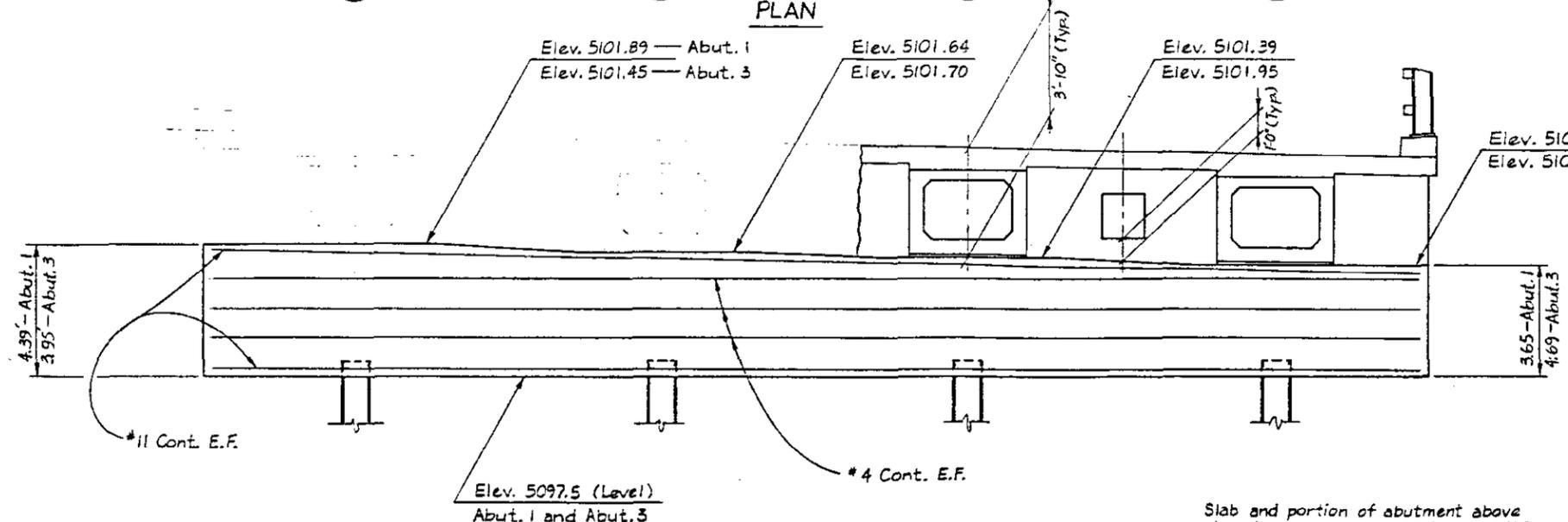
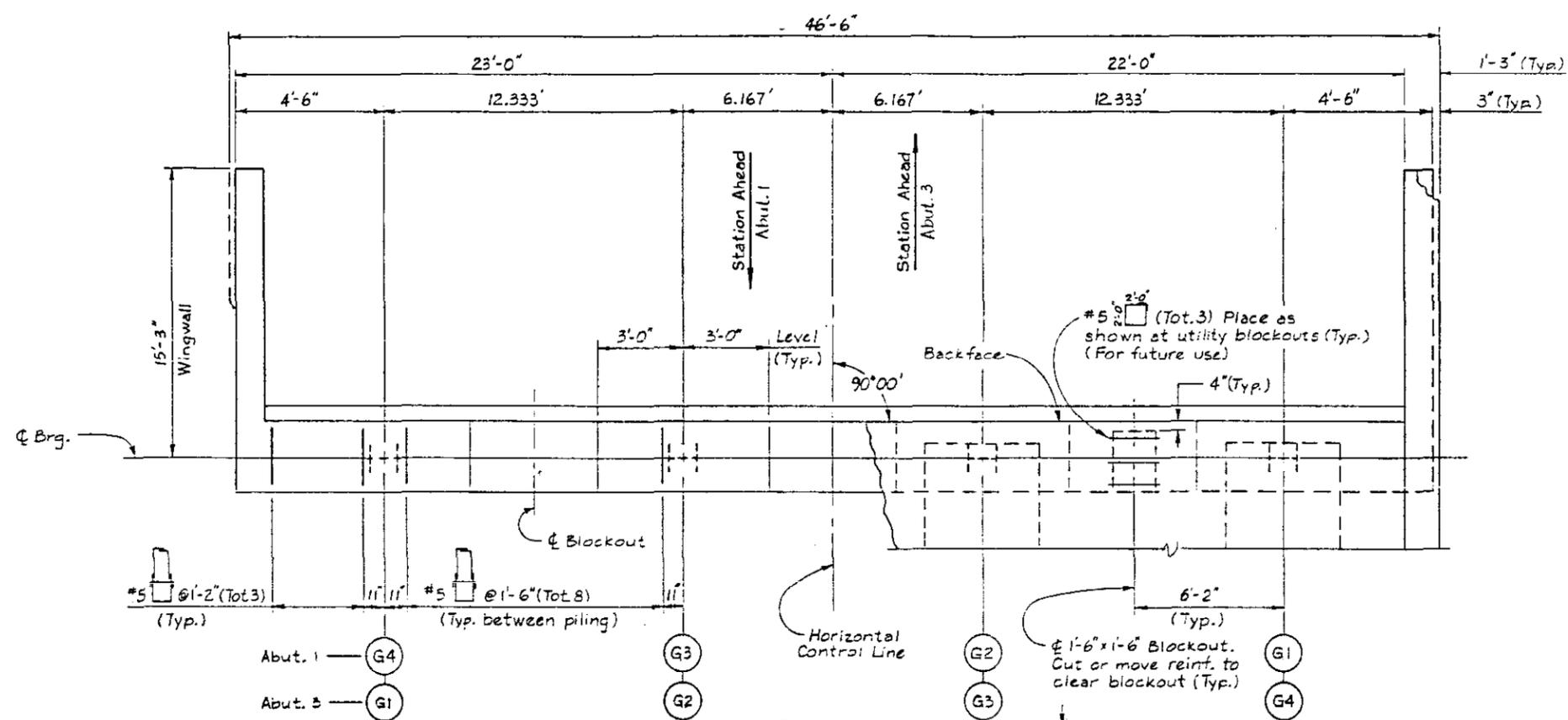
DESIGNED BY	DATE
CHECKED BY	DATE
QUANTITIES BY	DATE
CHECKED BY	DATE

09/12/83 14:54:57 INTERIOR... (UPPER/09/10/83/DRAFT) CORL_CK_B 88084 M000

AS CONSTRUCTED		
NO REVISIONS	REVISED	VOID
7		

FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
VIII	COLO.	BRF 007-3(3)	14	27

REVISIONS	



DESIGNED BY	
CHECKED BY	
DATE	
INITIALS	
DESIGNED BY	
CHECKED BY	
DATE	
INITIALS	

Slab and portion of abutment above bearing seat to be poured monolithically.
 For wingwall details, see Dwg. B 7.
 For excavation details, see Dwg. B 7.

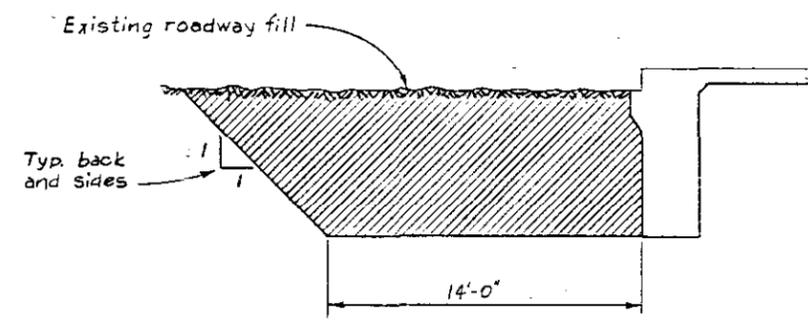
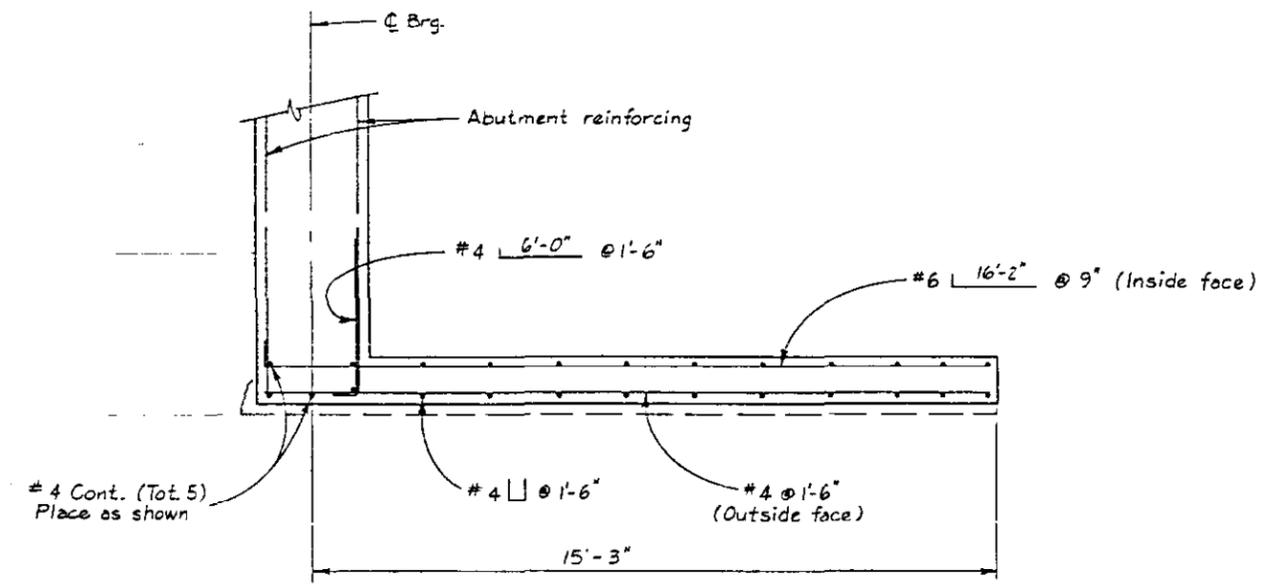
DIVISION OF HIGHWAYS		
ABUTMENT DETAILS		
DESIGNER L. Sanchez	STRUCTURE NUMBERS	D-16-DM
DETAILER L. Sanchez	DRAWING NUMBER B 6 OF 13 DRAWINGS	

REVISION DATES	(PRELIMINARY STAGE ONLY)

AS CONSTRUCTED		
NO REVISIONS	REVISED	VOID

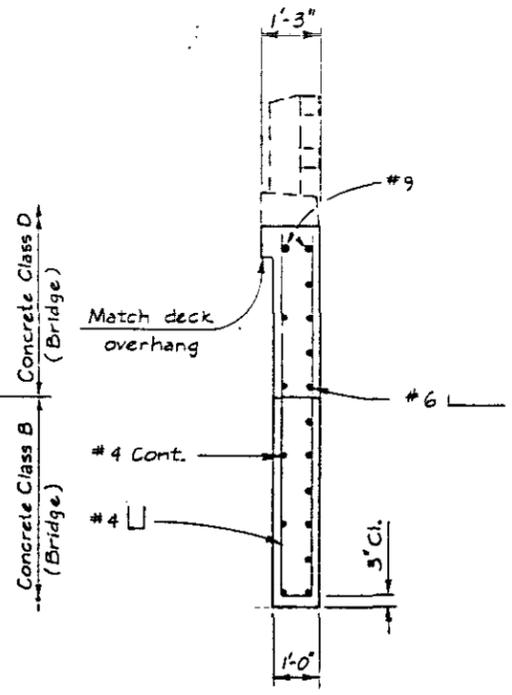
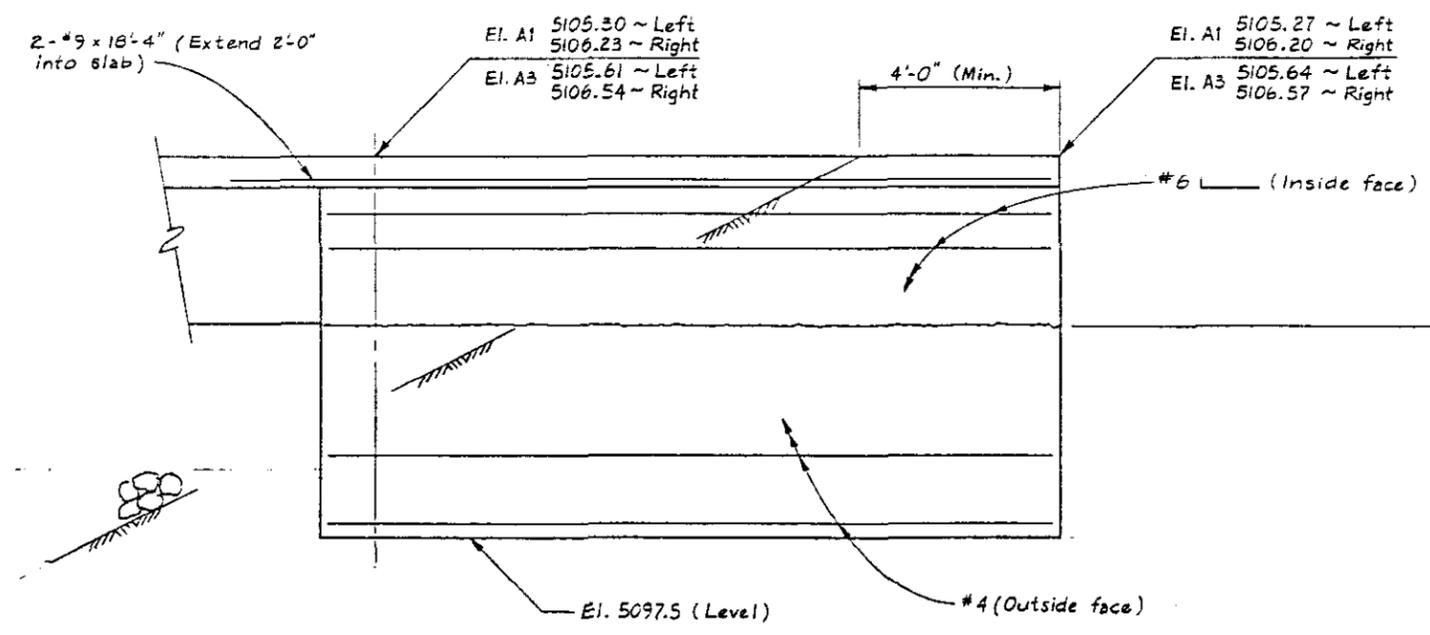
FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
VIII	COLO.	BRF 007-3(3)	15	25

REVISIONS	



Structure Excavation behind abutments shall be as indicated in this detail. Excavation for front of abutments shall be as shown in Standard M-206-2.

ABUTMENT EXCAVATION DETAIL



For details and reinforcing of Bridge Rail Type 10, see Dwg. No. B 11.

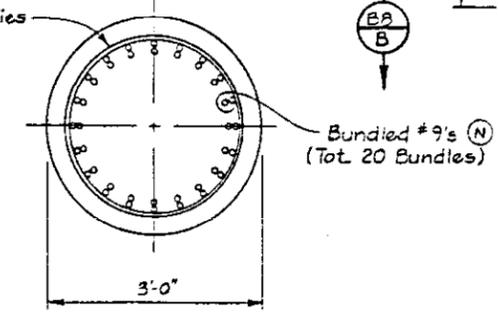
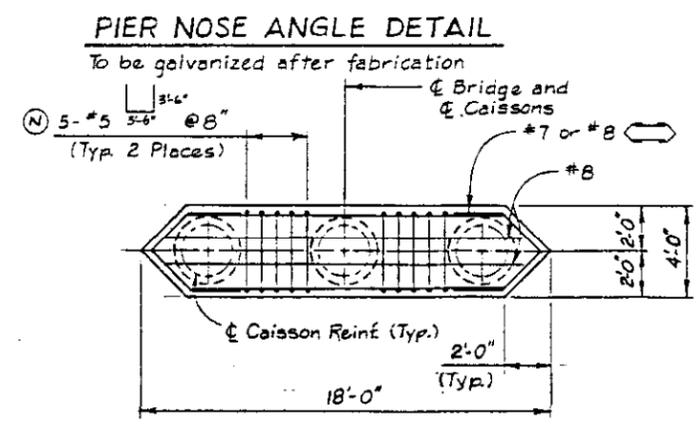
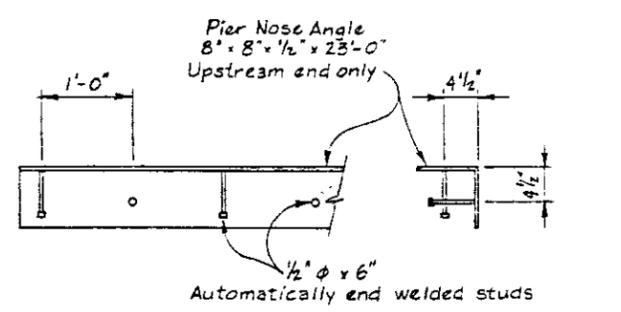
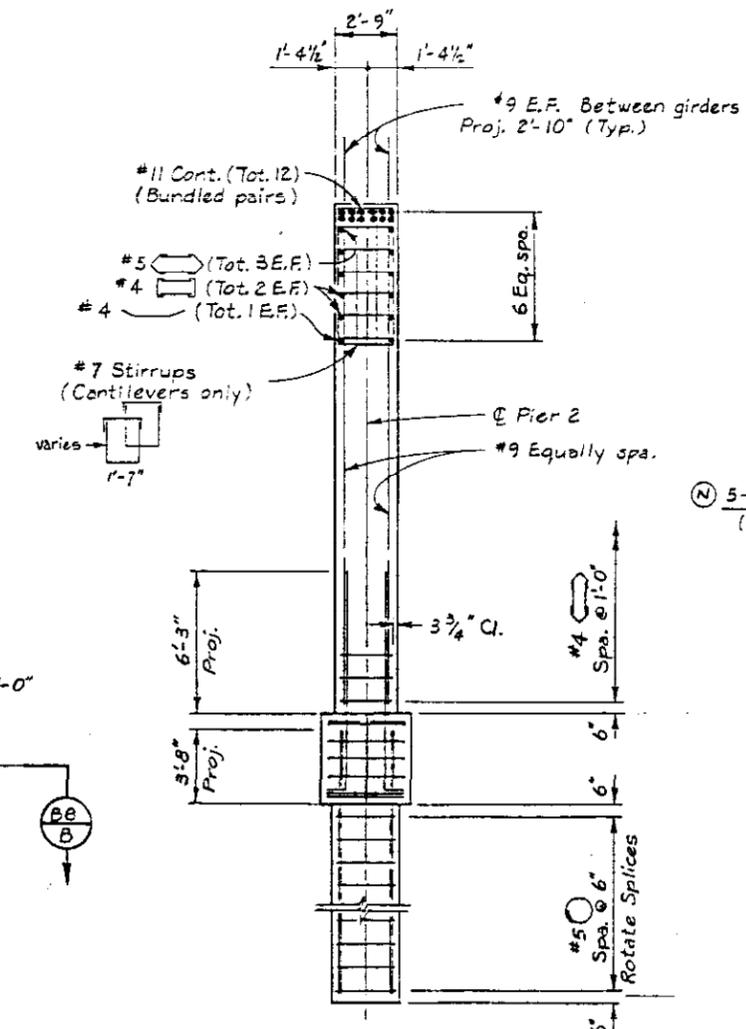
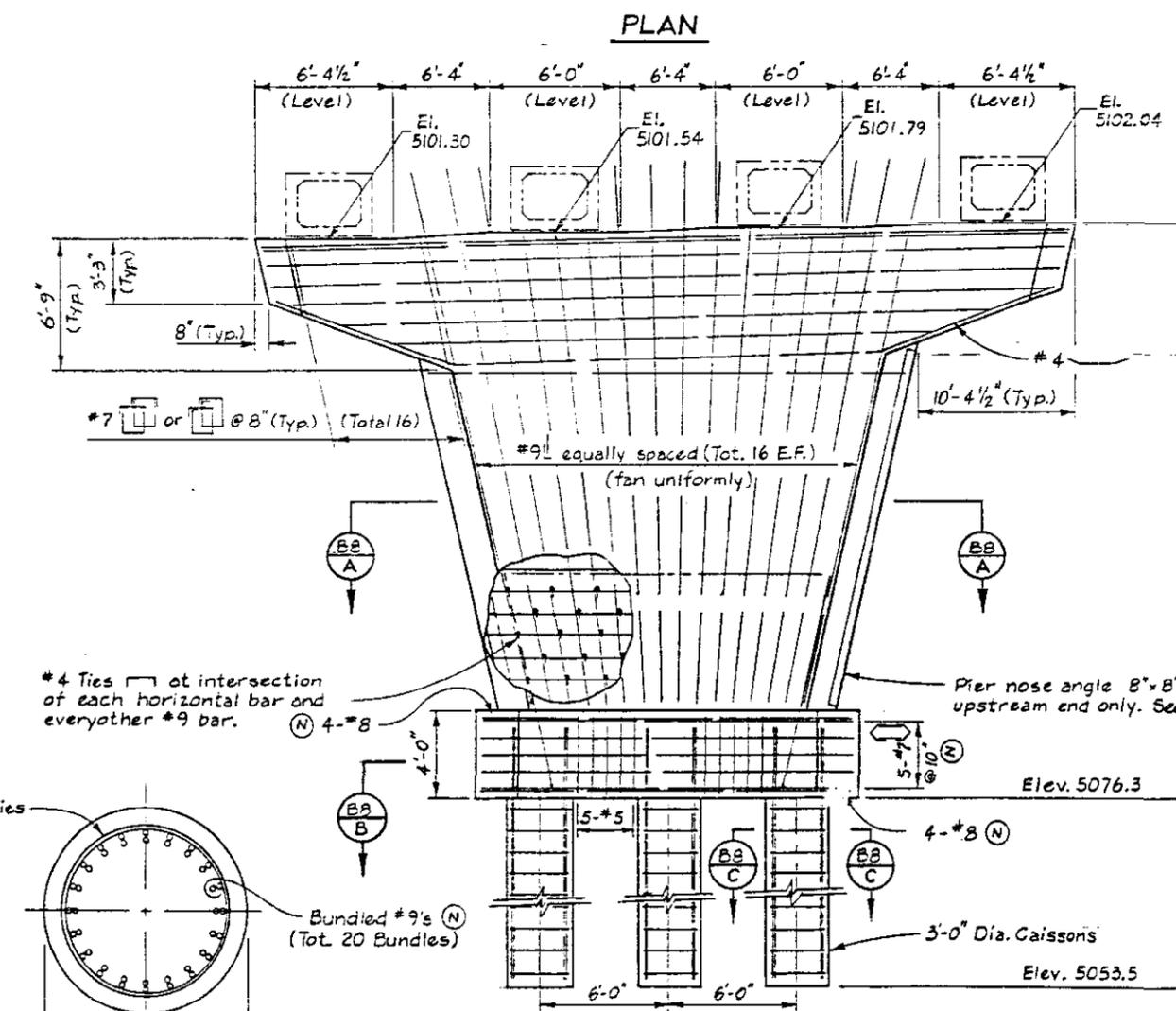
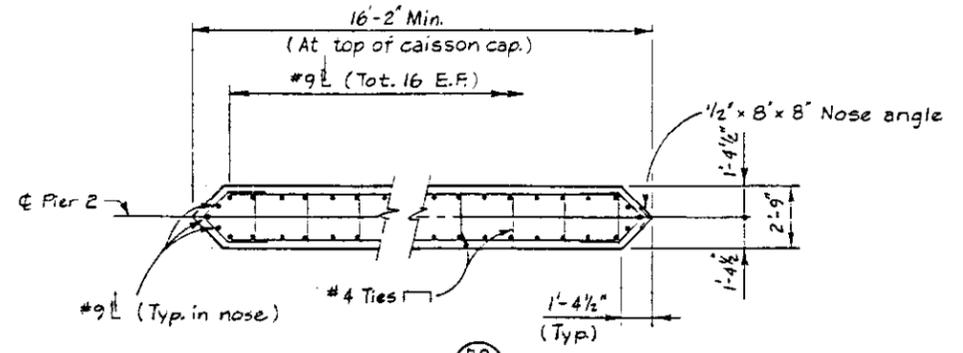
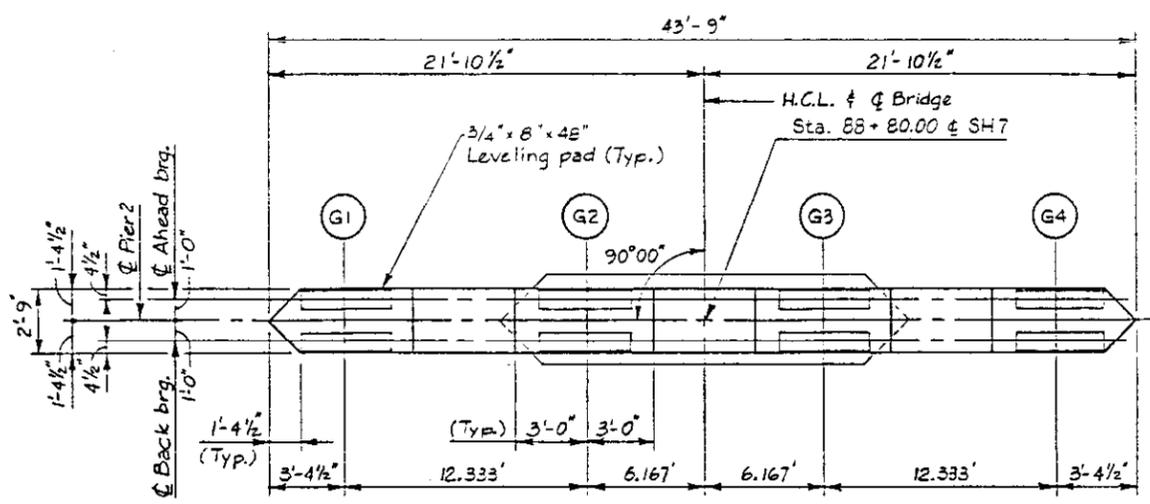
DESIGNED BY	CHECKED BY
DATE	DATE
INITIAL	INITIAL
BY	BY
DATE	DATE
INITIAL	INITIAL
BY	BY
DATE	DATE

DIVISION OF HIGHWAYS		
WINGWALL DETAILS		
DESIGNER L. Sanchez	STRUCTURE NUMBERS	D-16-DM
DETAILER L. Sanchez	DRAWING NUMBER B 7 OF 13 DRAWINGS	

REVISION DATES	(PRELIMINARY STAGE ONLY)
----------------	--------------------------

AS CONSTRUCTED			FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
NO REVISIONS	REVISED	VOID	VIII	COLO.	BRF 007-3(3)	16	37

REVISIONS	



* Included in Item 503 Drilled Caisson (36").

DIVISION OF HIGHWAYS		
PIER 2 DETAILS		
DESIGNER L. Sanchez	STRUCTURE D-16-DM	
DETAILER L. Sanchez	NUMBERS	
DRAWING NUMBER B 8	OF 13	DRAWINGS

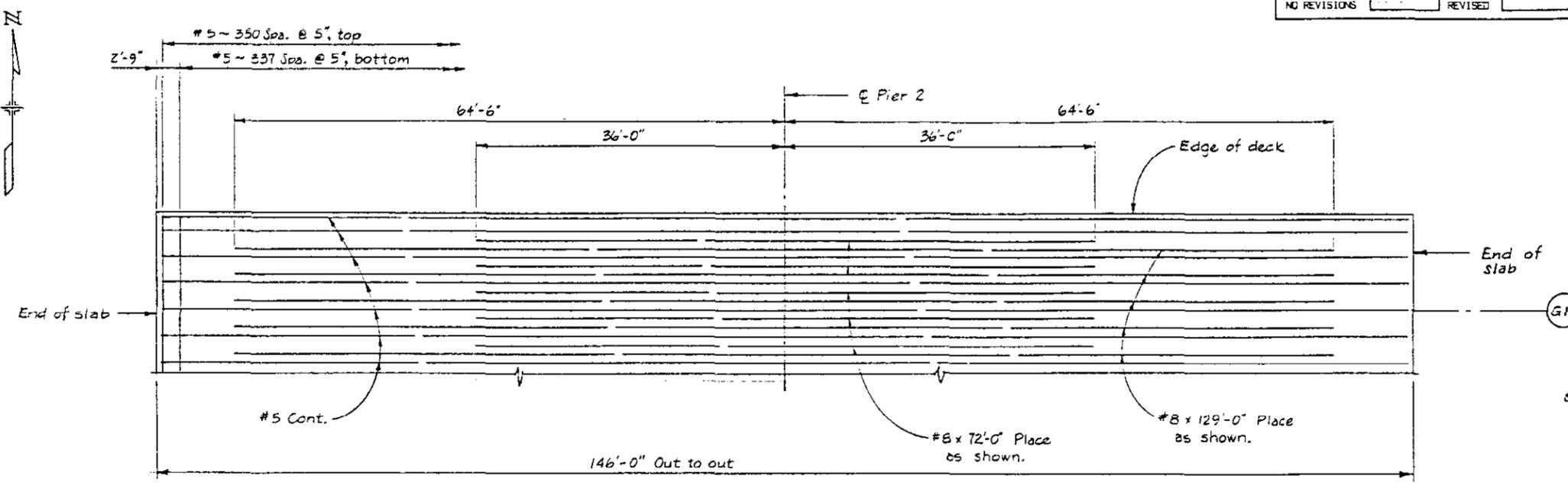
REVISION DATES (PRELIMINARY STAGE ONLY)

DESIGNED BY	CHECKED BY	DATE

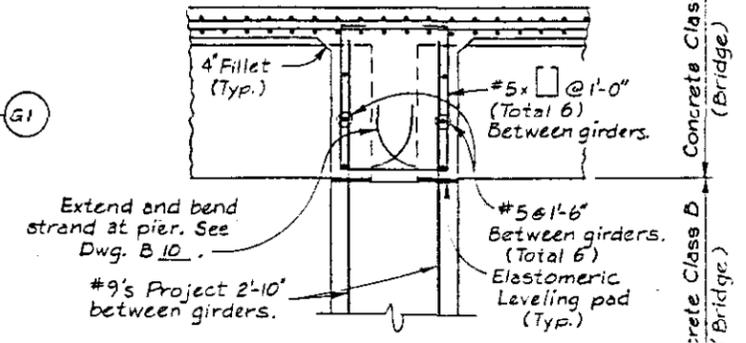
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NO REVISIONS	REVISED	VOID

FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
XIII	COLO.	BRF 007-3(3)	17	17

REVISIONS	

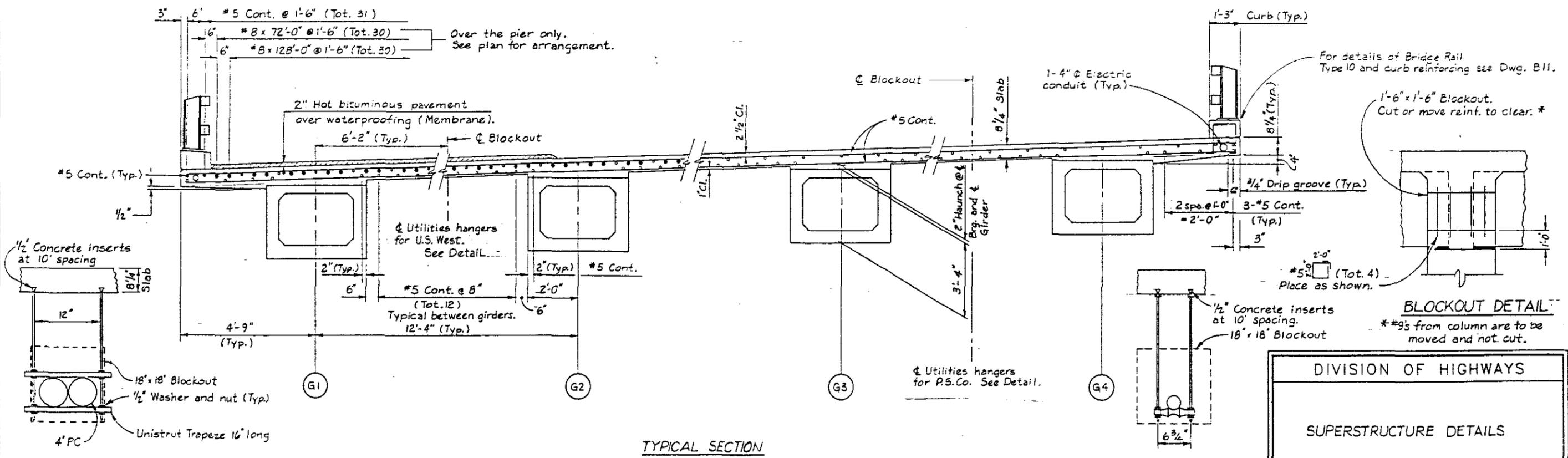


PART SLAB PLAN
 Bottom longitudinal reinf. not shown.



SECTION THRU DIAPHRAGM
 Slab and diaphragm to be poured monolithically.

DESIGNED BY	DATE	CHECKED BY	DATE



TYPICAL SECTION

HANGER DETAIL
 Hanger and hardware to be furnished by U.S. West.

HANGER DETAIL
 Hanger and hardware to be furnished by P.S. Co.

DIVISION OF HIGHWAYS		
SUPERSTRUCTURE DETAILS		
DESIGNER L. Sanchez	STRUCTURE NUMBERS	D-16-DM
DETAILER L. Sanchez		
DRAWING NUMBER B 9	OF 13	DRAWINGS

REVISION DATES (PRELIMINARY STAGE ONLY)

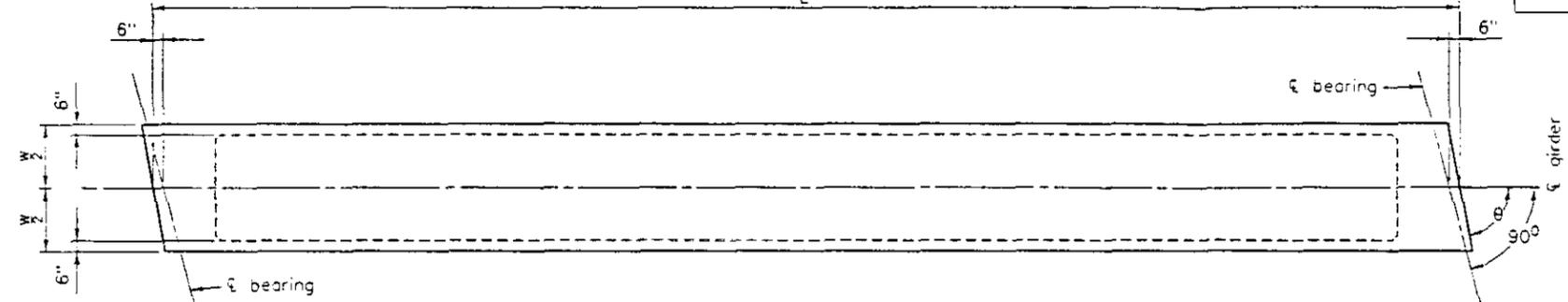
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NO REVISIONS	REVISED	VOID	III	COLORADO	BRF 007-3(3)	18

B-618-BX

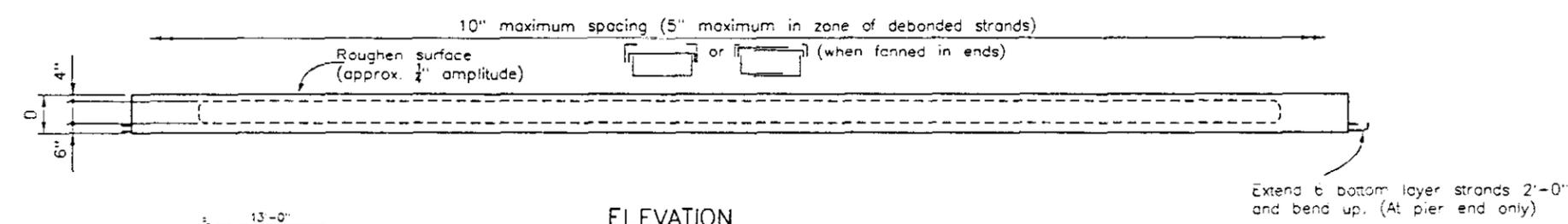
REVISIONS	

NOTES

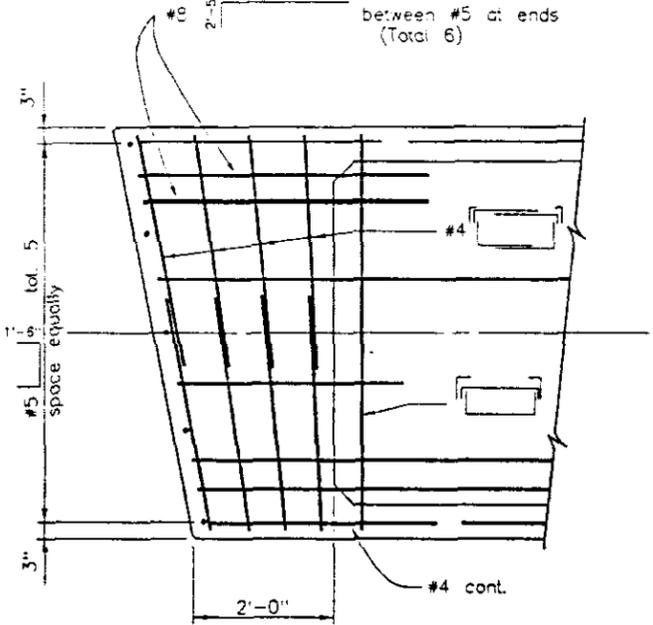
- All work necessary to fabricate and install the integral parts of the girder (including the elastomeric leveling and bearing pads) as shown on the plans shall be included in the bid price for Item No. 618, Prestressed Concrete Unit.
- When approved by the Engineer a minimum of tack welding will be permitted on ASTM A706 uncoated reinforcing steel.
- Reinforcing projecting from the top of the girder, reinforcing in the top slab of the box, and reinforcing within eight feet of an expansion device in the bridge deck shall be epoxy coated. Damaged coating on girder reinforcing need not be repaired.
- At girder ends not embedded in concrete diaphragms, cut strands off 1" below the surface of the concrete and finish with an approved epoxy grout. At girder ends embedded in concrete diaphragms, cut strands to proj. 3" unless otherwise noted. Do not make cosmetic repairs (damage less than 1/2" deep) to the parts of the girders embedded in concrete.
- Use 3/2" $f_p=270$ KSI low relaxation strands meeting the requirements of ASTM A416 grade 270. Alternate strands are stress relieved strands meeting the requirements of ASTM A416 grade 270. If stress relieved strands are used, the jacking force and initial concrete strengths shall be adjusted as appropriate to provide the final force (F_f) shown. The fabricator shall include design calculations with the shop plans.
- The minimum distance between groups or individual strands shall be 1 1/2" (measured between centers of adjacent strands). The minimum cover for prestressing steel is 1 1/2".
- A_p is the minimum area of the prestressing steel.
- f_p is the ultimate strength of the prestressing steel.
- F_j is the jacking force per girder.
- F_f is the final force per girder after all losses.
- f_{ci} is the required concrete strength at release of prestress force.
- f_c is the required concrete strength at 28 days of age.
- E_g is the distance from the bottom of girder to the C.G. of strands.
- Δ is the deflection at centerline of span due to cast-in-place slab, diaphragms, asphalt, curbs, rails, and walks.
- Concrete shall be Class S.
- Entrained air is not required for girder concrete.
- End blocks shall be used on all girders unless otherwise noted.
- Use 1/2" chamfer on all corners, except as noted.
- Expected camber is projected to 90 days girder age. If girders, deck forms, and reinforcing will not be placed before that girder age, weights, temporary tensioning, or other means approved by the Engineer shall be used to limit camber growth during storage. Prior to deck pour, measure and report to CDOT Bridge Design cambers exceeding the expected camber by more than 1/2". Cambers this large will normally interfere with placement of reinforcing in the deck.
- Debond 10 strands for 13'-0" at each end.



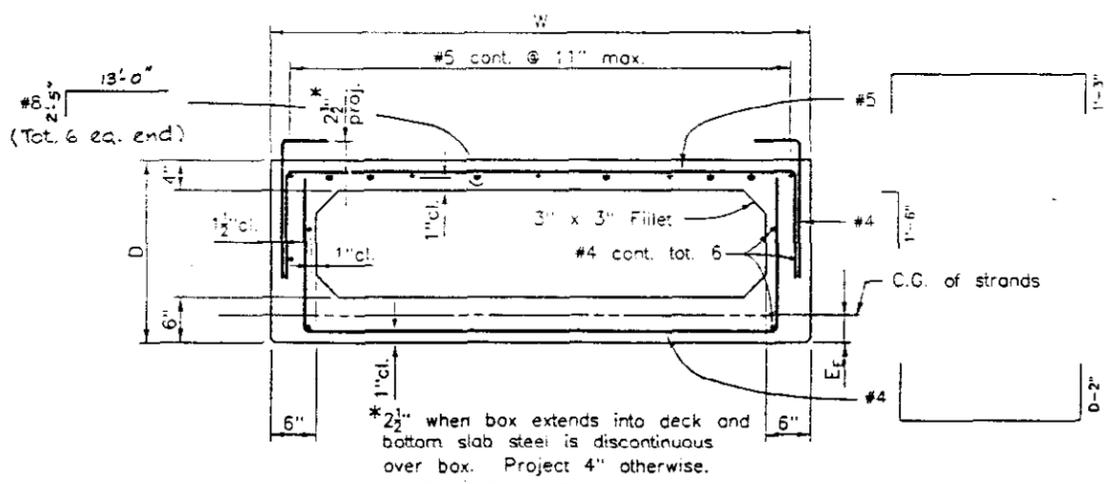
PLAN



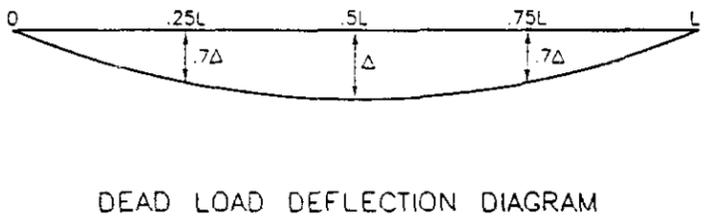
ELEVATION



END DETAIL (PLAN)



TYPICAL SECTION



DEAD LOAD DEFLECTION DIAGRAM

Span No.	Girder No.	L (Feet)	W (Inch)	D (Inch)	θ (Deg.)	A_p (Square inch)	E_g (Inch)	F_j (KIPS)	F_f (KIPS)	Concrete Strength		Δ (Inch)	Expected Camber (Inch)
										f_{ci} (PSI)	f_c (PSI)		
1	1-4	71.75	48	40	90	6.73	3.13	1304	985	5398	5647	1.209	1.947
2	1-4	71.75	48	40	90	6.73	3.13	1304	985	5398	5647	1.209	1.947

DIVISION OF HIGHWAYS

PRECAST BOX GIRDER DETAILS

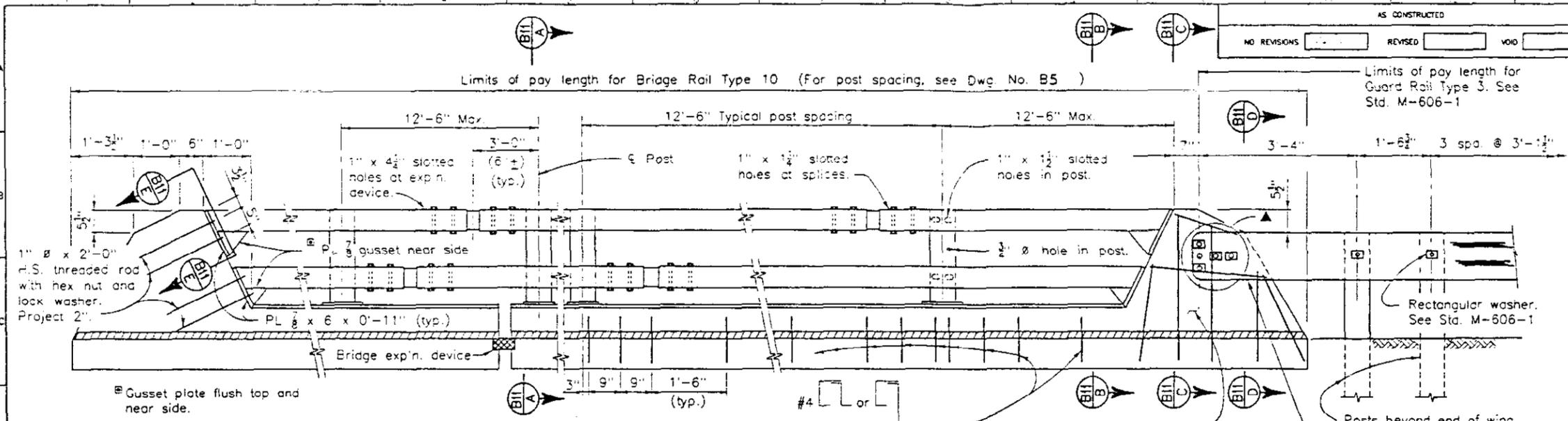
Designer L. Sanchez	Structure	2-18-DM
Detailer B. Lera	Numbers	
Drawing Number B 10 of 13		Drawings

INITIAL	DATE	CHECKED BY	QUANTITIES BY
I.S.	9-86	J.S.	J.S.
H.L.P.	7-89	J.S.V.	J.S.V.
H.L.P.	7-89	J.S.V.	J.S.V.

07/20/89 quantity (user/01700/bv_admin2/bruce/rnc)B618BXDM

FEDERAL ROAD REGION NO.	DIVISION	PROJECT NUMBER	SHEET NUMBER
1	COLORADO	BRF 007-3(3)	19

REVISIONS	

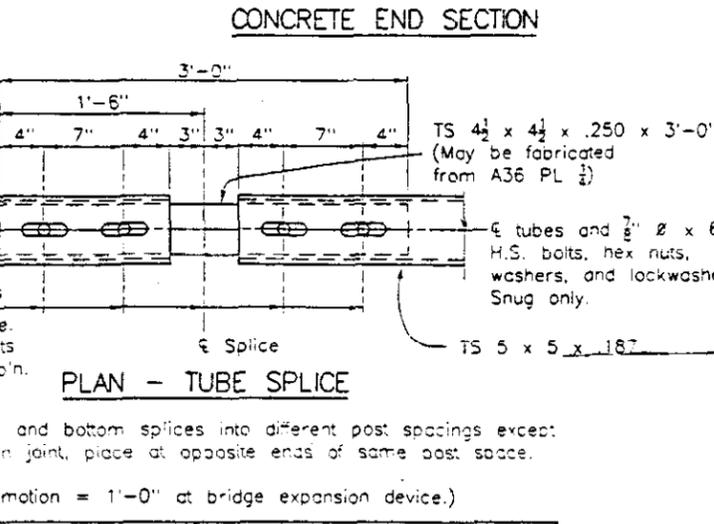
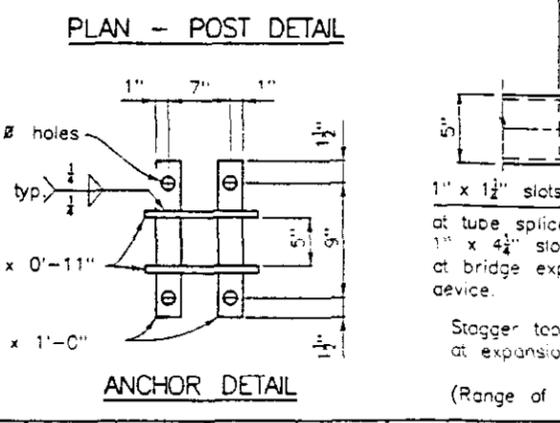
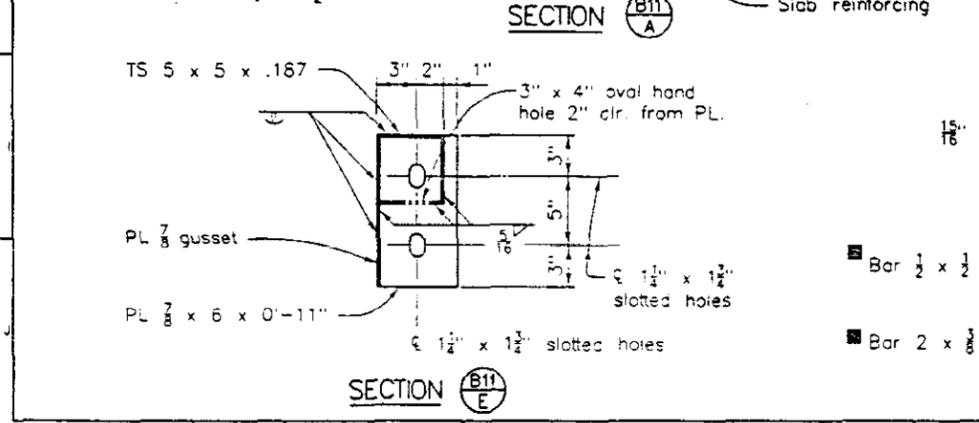
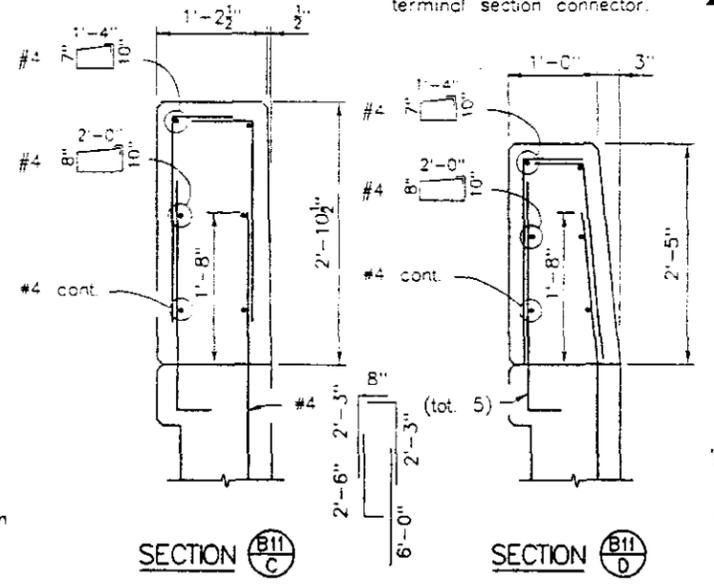
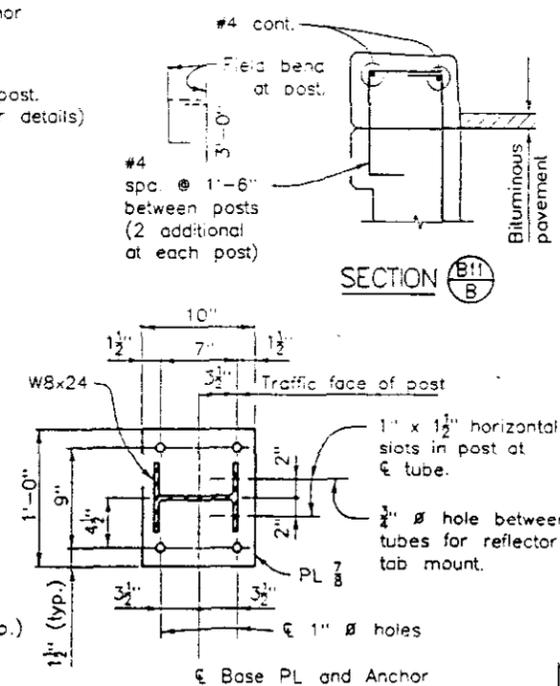
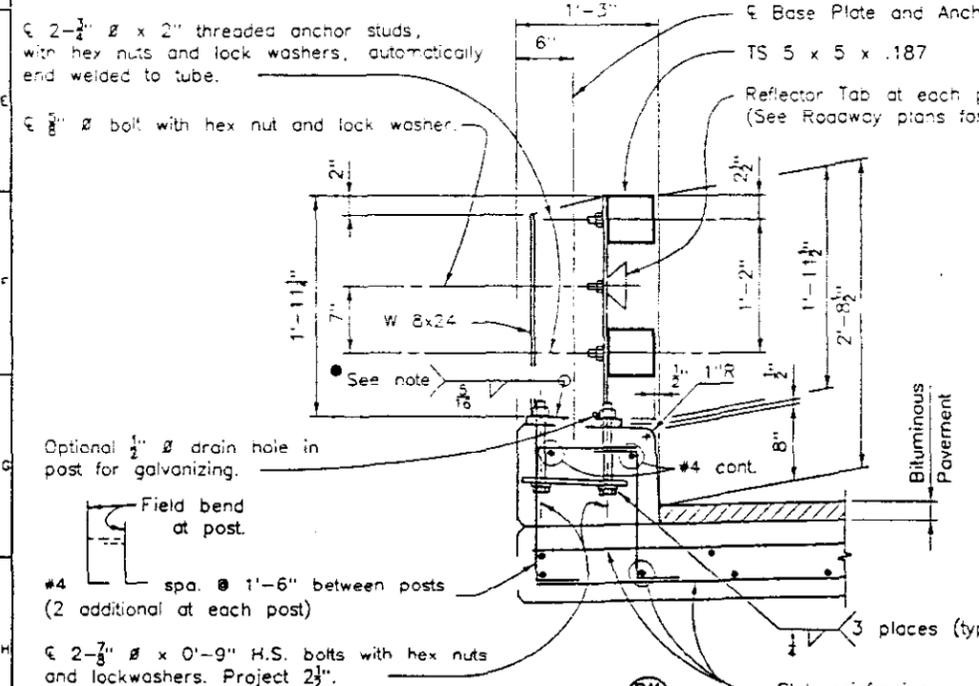


ELEVATION - BRIDGE RAIL

RAIL PANEL AT TERMINAL SECTION (See Roadway plans for ends requiring terminal section.)

RAIL PANEL AT EXPANSION DEVICE

RAIL PANEL ON WING



Notes: B-606-10

All tubes shall be fabricated from ASTM A-500 Grade B. All posts shall be fabricated from ASTM A-36 steel.

The above material and all anchor bolts and miscellaneous bolts, nuts, and washers shall be galvanized after fabrication in accordance with the specifications.

Post anchor, encased in concrete, shall be ASTM A-36 or AASHTO M-169 steel, as noted, and need not be galvanized.

The tubes shall be shop bent or fabricated to fit horizontal curve when radius is less than 1,500 feet.

Tubes shall be continuous over not less than two posts.

The centerline of the posts at the bridge expansion device shall be a minimum of 3'-0" from the centerline of the bridge expansion device, measured along the centerline of posts.

Posts shall be perpendicular to the grade of the deck.

One or more 12'-6" post spacings may be reduced (8'-4" min.) in order to maintain dimensions from the end of the wings and expansion joints.

7/8" x 1'-5" H.S. bolts, encased in 1" pipe sleeves, with hex nuts and lockwashers.

Optional drain hole for galvanizing may be drilled, punched, or clipped leaving smooth surfaces and transitions. No flame cutting or air carbon arc gouging is allowed.

Posts, post anchors, base plates, anchor bolts, miscellaneous bolts, nuts, washers, tubes, tube expansion devices, tube splices, end plates, curb and end section concrete (Class D), curb and end section reinforcing steel, and reflector tabs shall be included in Item 606 - Bridge Rail Type 10.

Prior to fabrication of this item, two sets of shop drawings which comply with the requirements of Section 105, shall be submitted to the division for information only. One set shall be sent to the Colorado Division of Highways, Staff Materials Branch, Materials Inspection Unit, 4340 E. Louisiana Avenue, Denver, Colorado 80222. One set shall be sent to the Engineer. The shop drawings will not be approved or returned.

Structural Steel:
 AASHTO M-183 (ASTM A-36) $f_y = 21,600$ psi
 Cold formed ASTM A-500 Grade B $f_y = 27,600$ psi

A.A.S.H.T.O. M169 steel may be used for bars in lieu of A.S.T.M. A36 steel.

Keep weld 3/8" clear of anchor bolt holes.

INFORMATION ONLY

Description	Unit	Per Lin. Ft.
Structural Steel (Galvanized)	Lb.	33.1
Concrete Class D (Bridge)	Cu.Yd.	.04
Reinforcing Steel (Epoxy Coated)	Lb.	4.0

DIVISION OF HIGHWAYS

BRIDGE RAIL TYPE 10

Designer L. Sanchez	Structure	D-16-DM
Detailer B. Lere	Numbers	
Drawing Number B 11	of 13	Drawings

Revision Dates (Preliminary Stage Only)

BRIDGE DECK ELEVATIONS

REVISIONS	

BENT LINE DESCRIPTION OR FRACTIONAL POINT DESIGNATION	STATION	OFFSET	ELEVATION	BENT LINE DESCRIPTION OR FRACTIONAL POINT DESIGNATION	STATION	OFFSET	ELEVATION	BENT LINE DESCRIPTION OR FRACTIONAL POINT DESIGNATION	STATION	OFFSET	ELEVATION	BENT LINE DESCRIPTION OR FRACTIONAL POINT DESIGNATION	STATION	OFFSET	ELEVATION	BENT LINE DESCRIPTION OR FRACTIONAL POINT DESIGNATION	STATION	OFFSET	ELEVATION
LONGITUDINAL LINE: CL GIRDER 1				LONGITUDINAL LINE: HCL & PROFILE LINE				LONGITUDINAL LINE: RIGHT OUT				LONGITUDINAL LINE: LEFT OUT				LONGITUDINAL LINE: CL GIRDER 3			
B F ABUT 1	88+07.0000	-18.5000	5105.3923	B F ABUT 1	88+07.0000	0.0000	5105.76823	B F ABUT 1	88+07.0000	23.2500	5106.2273	B F ABUT 1	88+07.0000	-23.2500	5105.2673	B F ABUT 1	88+07.0000	0.0000	5105.76823
CL BRG. A1	88+08.2500	-18.5000	5105.3950	CL BRG. A1	88+08.2500	0.0000	5105.76900	CL BRG. A1	88+08.2500	23.2500	5106.2300	CL BRG. A1	88+08.2500	-23.2500	5105.2700	CL BRG. A1	88+08.2500	0.0000	5105.76900
F-1	88+15.3250	-18.5000	5105.4101	F-1	88+15.3250	0.0000	5105.78000	F-1	88+15.3250	23.2500	5106.2350	F-1	88+15.3250	-23.2500	5105.2750	F-1	88+15.3250	0.0000	5105.78000
F-2	88+22.4000	-18.5000	5105.4253	F-2	88+22.4000	0.0000	5105.78000	X-2	88+22.4000	23.2500	5106.2500	X-2	88+22.4000	-23.2500	5105.2800	F-2	88+22.4000	0.0000	5105.78000
F-3	88+29.4750	-18.5000	5105.4404	F-3	88+29.4750	0.0000	5105.78104	X-3	88+29.4750	23.2500	5106.2754	X-3	88+29.4750	-23.2500	5105.2854	F-3	88+29.4750	0.0000	5105.78104
F-4	88+36.5500	-18.5000	5105.4556	F-4	88+36.5500	0.0000	5105.78104	X-4	88+36.5500	23.2500	5106.2906	X-4	88+36.5500	-23.2500	5105.2906	F-4	88+36.5500	0.0000	5105.78104
F-5	88+43.6250	-18.5000	5105.4707	F-5	88+43.6250	0.0000	5105.78104	X-5	88+43.6250	23.2500	5106.3058	X-5	88+43.6250	-23.2500	5105.2958	F-5	88+43.6250	0.0000	5105.78104
F-6	88+50.7000	-18.5000	5105.4858	F-6	88+50.7000	0.0000	5105.78104	X-6	88+50.7000	23.2500	5106.3210	X-6	88+50.7000	-23.2500	5105.3010	F-6	88+50.7000	0.0000	5105.78104
F-7	88+57.7750	-18.5000	5105.5010	F-7	88+57.7750	0.0000	5105.78104	X-7	88+57.7750	23.2500	5106.3362	X-7	88+57.7750	-23.2500	5105.3062	F-7	88+57.7750	0.0000	5105.78104
F-8	88+64.8500	-18.5000	5105.5161	F-8	88+64.8500	0.0000	5105.78104	X-8	88+64.8500	23.2500	5106.3514	X-8	88+64.8500	-23.2500	5105.3114	F-8	88+64.8500	0.0000	5105.78104
F-9	88+71.9250	-18.5000	5105.5313	F-9	88+71.9250	0.0000	5105.78104	X-9	88+71.9250	23.2500	5106.3666	X-9	88+71.9250	-23.2500	5105.3166	F-9	88+71.9250	0.0000	5105.78104
PK BRG.	88+79.0000	-18.5000	5105.5465	PK BRG.	88+79.0000	0.0000	5105.78104	PK BRG.	88+79.0000	23.2500	5106.3818	PK BRG.	88+79.0000	-23.2500	5105.3218	PK BRG.	88+79.0000	0.0000	5105.78104
PIER 2.	88+80.0000	-18.5000	5105.5517	PIER 2.	88+80.0000	0.0000	5105.78104	PIER 2.	88+80.0000	23.2500	5106.3970	PIER 2.	88+80.0000	-23.2500	5105.3270	PIER 2.	88+80.0000	0.0000	5105.78104
AH BRG.	88+81.0000	-18.5000	5105.5569	AH BRG.	88+81.0000	0.0000	5105.78104	AH BRG.	88+81.0000	23.2500	5106.4122	AH BRG.	88+81.0000	-23.2500	5105.3322	AH BRG.	88+81.0000	0.0000	5105.78104
F-1	88+88.0750	-18.5000	5105.5721	F-1	88+88.0750	0.0000	5105.78104	F-1	88+88.0750	23.2500	5106.4274	F-1	88+88.0750	-23.2500	5105.3374	F-1	88+88.0750	0.0000	5105.78104
F-2	88+95.1500	-18.5000	5105.5873	F-2	88+95.1500	0.0000	5105.78104	F-2	88+95.1500	23.2500	5106.4426	F-2	88+95.1500	-23.2500	5105.3426	F-2	88+95.1500	0.0000	5105.78104
F-3	88+02.2250	-18.5000	5105.6025	F-3	88+02.2250	0.0000	5105.78104	F-3	88+02.2250	23.2500	5106.4578	F-3	88+02.2250	-23.2500	5105.3478	F-3	88+02.2250	0.0000	5105.78104
F-4	88+09.3000	-18.5000	5105.6177	F-4	88+09.3000	0.0000	5105.78104	F-4	88+09.3000	23.2500	5106.4730	F-4	88+09.3000	-23.2500	5105.3530	F-4	88+09.3000	0.0000	5105.78104
F-5	88+16.3750	-18.5000	5105.6329	F-5	88+16.3750	0.0000	5105.78104	F-5	88+16.3750	23.2500	5106.4882	F-5	88+16.3750	-23.2500	5105.3582	F-5	88+16.3750	0.0000	5105.78104
F-6	88+23.4500	-18.5000	5105.6481	F-6	88+23.4500	0.0000	5105.78104	F-6	88+23.4500	23.2500	5106.5034	F-6	88+23.4500	-23.2500	5105.3634	F-6	88+23.4500	0.0000	5105.78104
F-7	88+30.5250	-18.5000	5105.6633	F-7	88+30.5250	0.0000	5105.78104	F-7	88+30.5250	23.2500	5106.5186	F-7	88+30.5250	-23.2500	5105.3686	F-7	88+30.5250	0.0000	5105.78104
F-8	88+37.6000	-18.5000	5105.6785	F-8	88+37.6000	0.0000	5105.78104	F-8	88+37.6000	23.2500	5106.5338	F-8	88+37.6000	-23.2500	5105.3738	F-8	88+37.6000	0.0000	5105.78104
F-9	88+44.6750	-18.5000	5105.6937	F-9	88+44.6750	0.0000	5105.78104	F-9	88+44.6750	23.2500	5106.5490	F-9	88+44.6750	-23.2500	5105.3790	F-9	88+44.6750	0.0000	5105.78104
BRG. A3	88+51.7500	-18.5000	5105.7089	BRG. A3	88+51.7500	0.0000	5105.78104	BRG. A3	88+51.7500	23.2500	5106.5642	BRG. A3	88+51.7500	-23.2500	5105.3842	BRG. A3	88+51.7500	0.0000	5105.78104
ABUT. 3	88+53.0000	-18.5000	5105.7241	ABUT. 3	88+53.0000	0.0000	5105.78104	ABUT. 3	88+53.0000	23.2500	5106.5794	ABUT. 3	88+53.0000	-23.2500	5105.3894	ABUT. 3	88+53.0000	0.0000	5105.78104
LONGITUDINAL LINE: CL GIRDER 2				LONGITUDINAL LINE: CL GIRDER 4				LONGITUDINAL LINE: CL GIRDER 2				LONGITUDINAL LINE: CL GIRDER 4							
B F ABUT 1	88+07.0000	-5.1667	5105.8290	B F ABUT 1	88+07.0000	18.5000	5106.1323	B F ABUT 1	88+07.0000	-5.1667	5105.8290	B F ABUT 1	88+07.0000	18.5000	5106.1323				
CL BRG. A1	88+08.2500	-5.1667	5105.8411	CL BRG. A1	88+08.2500	18.5000	5106.1050	CL BRG. A1	88+08.2500	-5.1667	5105.8411	CL BRG. A1	88+08.2500	18.5000	5106.1050				
F-1	88+15.3250	-5.1667	5105.8532	F-1	88+15.3250	18.5000	5106.1501	F-1	88+15.3250	-5.1667	5105.8532	F-1	88+15.3250	18.5000	5106.1501				
F-2	88+22.4000	-5.1667	5105.8653	F-2	88+22.4000	18.5000	5106.1653	F-2	88+22.4000	-5.1667	5105.8653	F-2	88+22.4000	18.5000	5106.1653				
F-3	88+29.4750	-5.1667	5105.8774	F-3	88+29.4750	18.5000	5106.1804	F-3	88+29.4750	-5.1667	5105.8774	F-3	88+29.4750	18.5000	5106.1804				
F-4	88+36.5500	-5.1667	5105.8895	F-4	88+36.5500	18.5000	5106.1956	F-4	88+36.5500	-5.1667	5105.8895	F-4	88+36.5500	18.5000	5106.1956				
F-5	88+43.6250	-5.1667	5105.9016	F-5	88+43.6250	18.5000	5106.2107	F-5	88+43.6250	-5.1667	5105.9016	F-5	88+43.6250	18.5000	5106.2107				
F-6	88+50.7000	-5.1667	5105.9137	F-6	88+50.7000	18.5000	5106.2259	F-6	88+50.7000	-5.1667	5105.9137	F-6	88+50.7000	18.5000	5106.2259				
F-7	88+57.7750	-5.1667	5105.9258	F-7	88+57.7750	18.5000	5106.2410	F-7	88+57.7750	-5.1667	5105.9258	F-7	88+57.7750	18.5000	5106.2410				
F-8	88+64.8500	-5.1667	5105.9379	F-8	88+64.8500	18.5000	5106.2561	F-8	88+64.8500	-5.1667	5105.9379	F-8	88+64.8500	18.5000	5106.2561				
F-9	88+71.9250	-5.1667	5105.9500	F-9	88+71.9250	18.5000	5106.2713	F-9	88+71.9250	-5.1667	5105.9500	F-9	88+71.9250	18.5000	5106.2713				
PK BRG.	88+79.0000	-5.1667	5105.9621	PK BRG.	88+79.0000	18.5000	5106.2864	PK BRG.	88+79.0000	-5.1667	5105.9621	PK BRG.	88+79.0000	18.5000	5106.2864				
PIER 2.	88+80.0000	-5.1667	5105.9742	PIER 2.	88+80.0000	18.5000	5106.2995	PIER 2.	88+80.0000	-5.1667	5105.9742	PIER 2.	88+80.0000	18.5000	5106.2995				
AH BRG.	88+81.0000	-5.1667	5105.9863	AH BRG.	88+81.0000	18.5000	5106.3126	AH BRG.	88+81.0000	-5.1667	5105.9863	AH BRG.	88+81.0000	18.5000	5106.3126				
F-1	88+88.0750	-5.1667	5105.9984	F-1	88+88.0750	18.5000	5106.3257	F-1	88+88.0750	-5.1667	5105.9984	F-1	88+88.0750	18.5000	5106.3257				
F-2	88+95.1500	-5.1667	5106.0105	F-2	88+95.1500	18.5000	5106.3388	F-2	88+95.1500	-5.1667	5106.0105	F-2	88+95.1500	18.5000	5106.3388				
F-3	88+02.2250	-5.1667	5106.0226	F-3	88+02.2250	18.5000	5106.3519	F-3	88+02.2250	-5.1667	5106.0226	F-3	88+02.2250	18.5000	5106.3519				
F-4	88+09.3000	-5.1667	5106.0347	F-4	88+09.3000	18.5000	5106.3650	F-4	88+09.3000	-5.1667	5106.0347	F-4	88+09.3000	18.5000	5106.3650				
F-5	88+16.3750	-5.1667	5106.0468	F-5	88+16.3750	18.5000	5106.3781	F-5	88+16.3750	-5.1667	5106.0468	F-5	88+16.3750	18.5000	5106.3781				
F-6	88+23.4500	-5.1667	5106.0589	F-6	88+23.4500	18.5000	5106.3912	F-6	88+23.4500	-5.1667	5106.0589	F-6	88+23.4500	18.5000	5106.3912				
F-7	88+30.5250	-5.1667	5106.0710	F-7	88+30.5250	18.5000	5106.4043	F-7	88+30.5250	-5.1667	5106.0710	F-7	88+30.5250	18.5000	5106.4043				
F-8	88+37.6000	-5.1667	5106.0831	F-8	88+37.6000	18.5000	5106.4174	F-8	88+37.6000	-5.1667	5106.0831	F-8	88+37.6000	18.5000	5106.4174				
F-9	88+44.6750	-5.166																	

AS CONSTRUCTED			FEDERAL ROAD REC'D NO.	DIVISION	PROJECT NUMBER	SHEET NUMBER
NO REVISIONS	REVISED	VOID	221	COLORADO	BRF 007-3(3)	2 /

ROADWAY APPROACHES

REVISIONS	

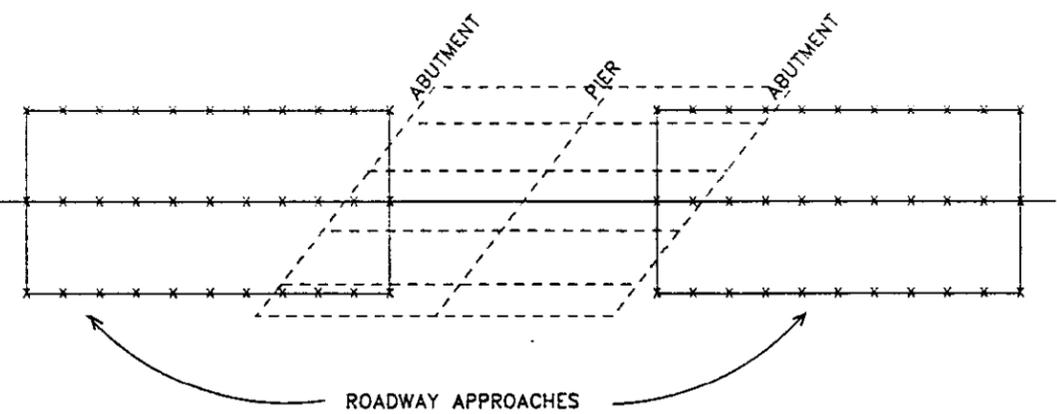
ELEVATIONS ON ROADWAY APPROACHES
(AT FINISHED GRADE)

STATION	OFFSETS		
	-22.0000	0.0000	22.0000
86+60.0	5105.51	5105.95	5106.39
+70.0	05.48	05.92	06.36
+80.0	05.45	05.89	06.33
+90.0	05.42	05.86	06.30
87+00.0	5105.40	5105.84	5106.28
+10.0	05.39	05.83	06.27
+20.0	05.38	05.82	06.26
+30.0	05.38	05.82	06.26
87+40.0	5105.38	5105.82	5106.26
+50.0	05.38	05.82	06.26
+60.0	05.39	05.83	06.27
+70.0	05.41	05.85	06.29
87+80.0	5105.43	5105.87	5106.31
+90.0	05.45	05.89	06.33
88+00.0	05.47	05.91	06.35
+10.0	05.50	05.94	06.38

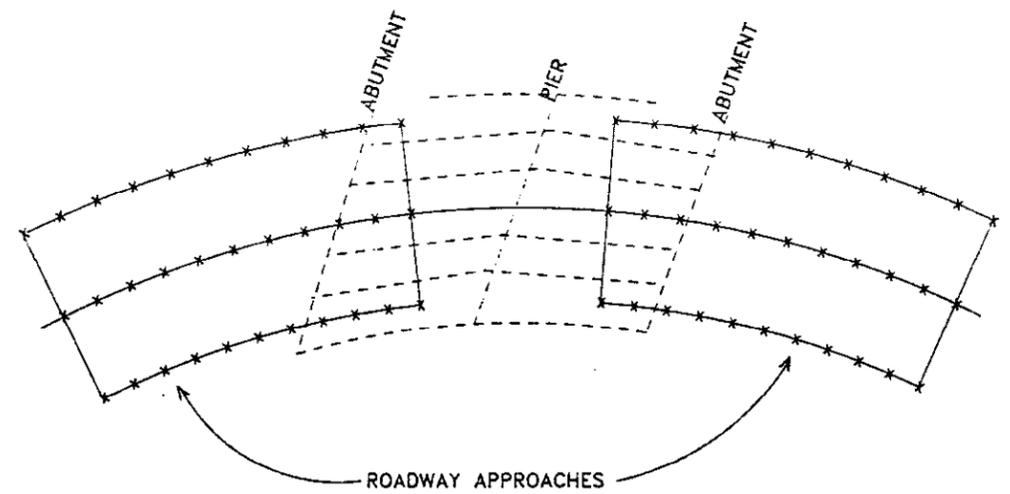
ELEVATIONS ON ROADWAY APPROACHES
(AT FINISHED GRADE)

STATION	OFFSETS		
	-22.0000	0.0000	22.0000
89+50.0	5105.80	5106.24	5106.68
+60.0	05.82	06.26	06.70
+70.0	05.84	06.28	06.72
+80.0	05.86	06.30	06.74
89+90.0	5105.88	5106.32	5106.76
90+00.0	05.90	06.34	06.78
+10.0	05.92	06.36	06.80
+20.0	05.94	06.38	06.82
90+30.0	5105.97	5106.41	5106.85
+40.0	05.99	06.43	06.87
+50.0	06.01	06.45	06.89
+60.0	06.03	06.47	06.91
90+70.0	5106.05	5106.49	5106.93
+80.0	06.07	06.51	06.95
+90.0	06.09	06.53	06.97
91+00.0	06.12	06.56	07.00

DESIGNED BY	DATE	CHECKED BY	DATE	DESIGNED BY	DATE	CHECKED BY	DATE
L. S.	10-88						
QUANTITIES BY	DATE						
L. S.	6-89						



ROADWAY APPROACHES
 * TANGENT ALIGNMENT



ROADWAY APPROACHES
 * HORIZONTAL CURVE ALIGNMENT

* THESE DRAWINGS ARE SCHEMATICS ONLY AND DO NOT
 NECESSARILY REFLECT THE ACTUAL BRIDGE LAYOUT.

DIVISION OF HIGHWAYS			
ROADWAY APPROACHES			
Designer	L. SANCHEZ	Structure	D-16-DM
Detailer	L. SANCHEZ	Numbers	
Drawing Number B13		of 13 Drawings	

Revision	Date	(Preliminary Stage Only)

NOV. 1983

AS CONSTRUCTED		FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.
NO REVISIONS	REVISED 3-2-90	VII	COLO.	BRF 007-3(3)	22

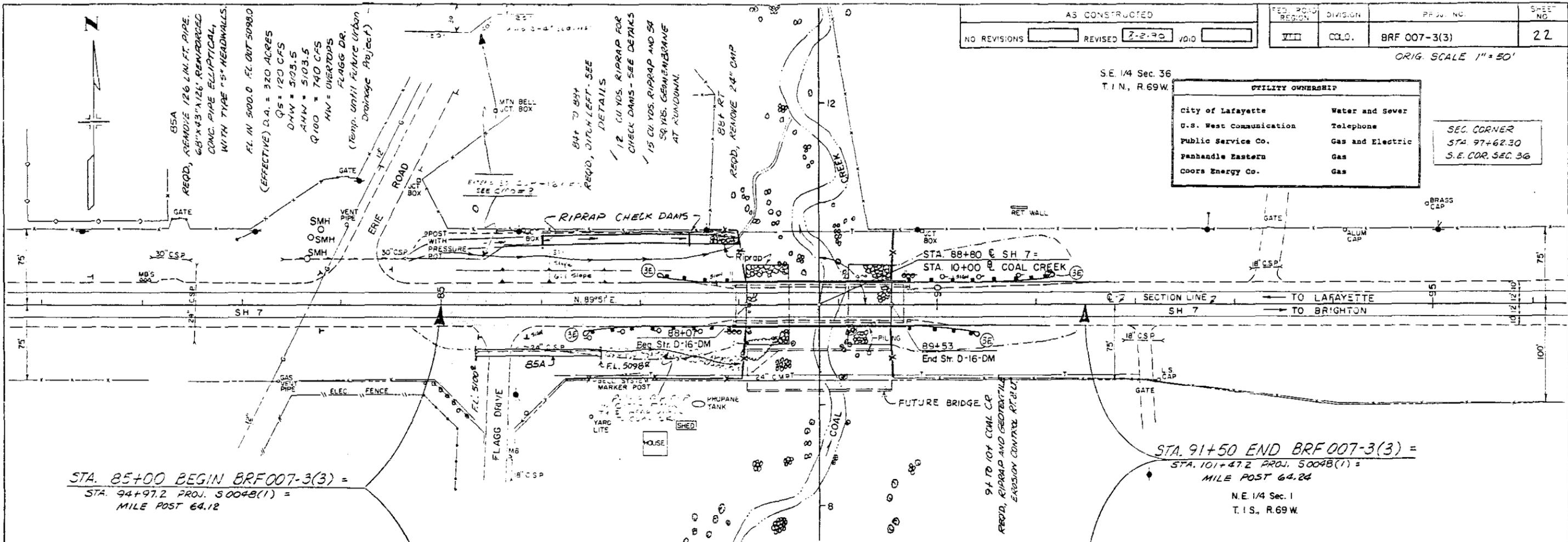
ORIG. SCALE 1" = 50'

SE 1/4 Sec. 36
T.1N., R.69W.

UTILITY OWNERSHIP	
City of Lafayette	Water and Sewer
U.S. West Communication	Telephone
Public Service Co.	Gas and Electric
Panhandle Eastern	Gas
Coors Energy Co.	Gas

SEC. CORNER
STA. 97+62.30
S.E. COR. SEC. 36

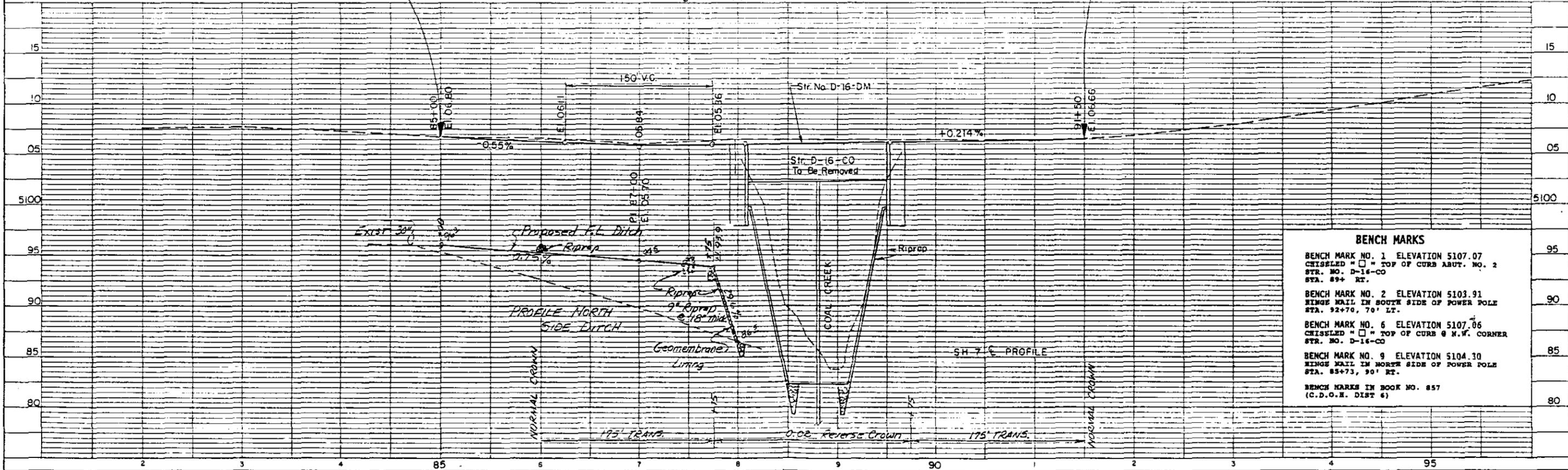
PLAN	DATE	BY
SAVED		
MODIFIED		
REVISIONS		



STA. 85+00 BEGIN BRFO07-3(3) =
STA. 94+97.2 PROJ. 50048(1) =
MILE POST 64.12

STA. 91+50 END BRFO07-3(3) =
STA. 101+47.2 PROJ. 50048(1) =
MILE POST 64.24
N.E. 1/4 Sec. 1
T.1S., R.69W.

PROFILE	DATE	BY
SAVED		
MODIFIED		
REVISIONS		



BENCH MARKS	
BENCH MARK NO. 1	ELEVATION 5107.07 CHISELED "□" TOP OF CURB ABUT. NO. 2 STR. NO. D-16-CO STA. 89+ RT.
BENCH MARK NO. 2	ELEVATION 5103.91 RINGS NAIL IN SOUTH SIDE OF POWER POLE STA. 92+70, 70' LT.
BENCH MARK NO. 6	ELEVATION 5107.06 CHISELED "□" TOP OF CURB @ N.W. CORNER STR. NO. D-16-CO
BENCH MARK NO. 9	ELEVATION 5104.30 RINGS NAIL IN NORTH SIDE OF POWER POLE STA. 85+73, 90' RT.
BENCH MARKS IN BOOK NO. 857 (C.D.O.N. DIST 6)	

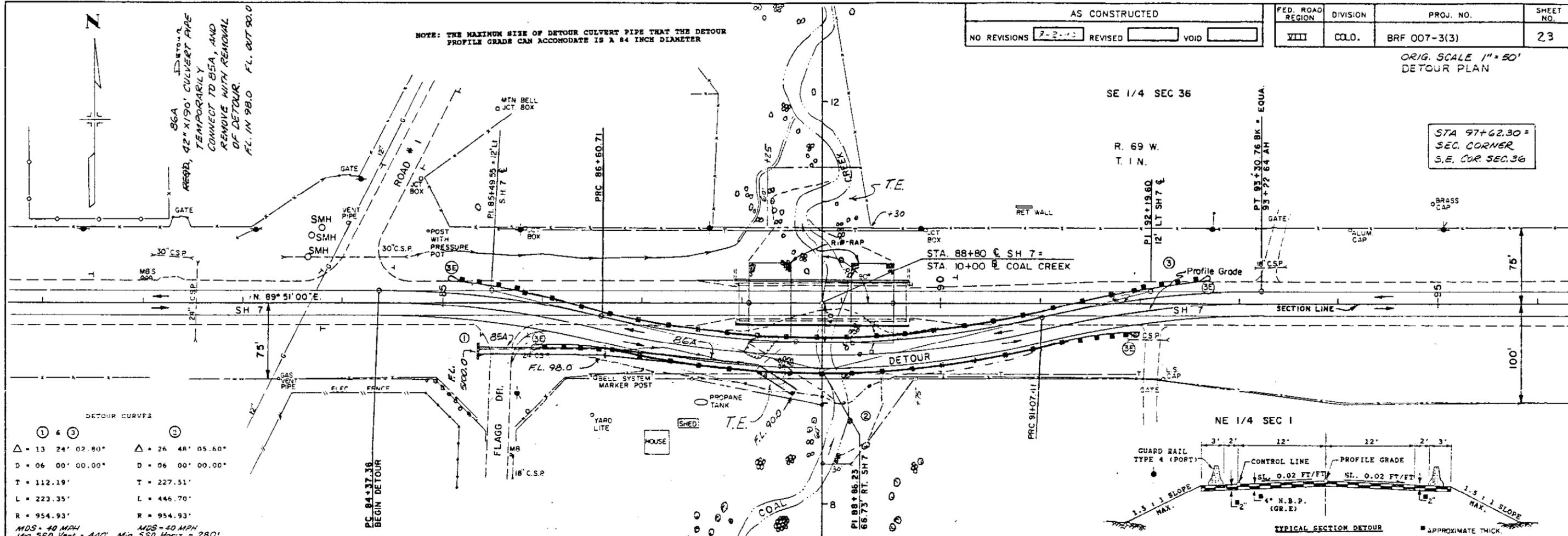
AS CONSTRUCTED		
NO REVISIONS	REVISED	VOID
2-2-00		

FED. ROAD REGION	DIVISION	PROJ. NO.	SHEET NO.
VIII	COLO.	BRF 007-3(3)	23

ORIG. SCALE 1" = 50'
 DETOUR PLAN

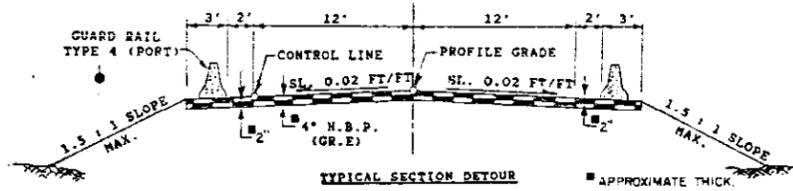
NOTE: THE MAXIMUM SIZE OF DETOUR CULVERT PIPE THAT THE DETOUR PROFILE GRADE CAN ACCOMMODATE IS A 84 INCH DIAMETER

86A
 42" X 150' CULVERT PIPE
 TEMPORARILY
 CONNECT TO B5A, AND
 REMOVE WITH REMOVAL
 OF DETOUR.
 F.L. IN 98.0 F.L. OUT 90.0



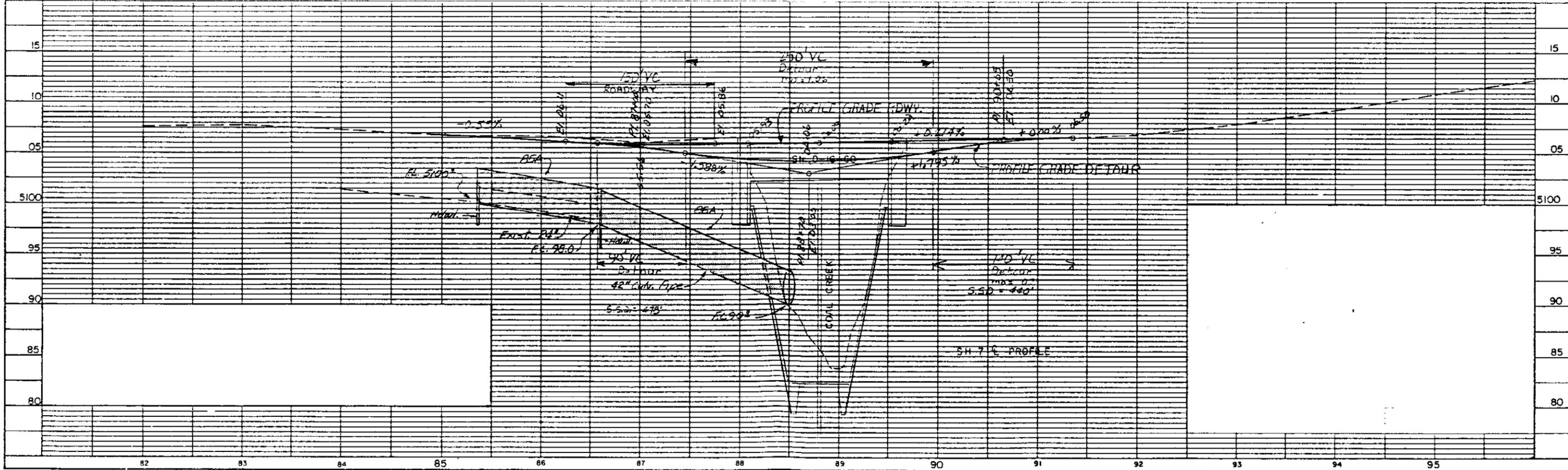
DETOUR CURVES

①	②
Δ = 13 24' 02.80"	Δ = 26 48' 05.60"
D = 06 00' 00.00"	D = 06 00' 00.00"
T = 112.19'	T = 227.51'
L = 223.35'	L = 446.70'
R = 954.93'	R = 954.93'
MDS = 40 MPH Min SSD Vert = 440' Min SSD Horiz = 280'	MDS = 40 MPH Min SSD Vert = 440' Min SSD Horiz = 280'



PLAN	BY	DATE
DESIGNED		
PLOTTED		
ALIGNED		
CHECKED		
RT. OF HWY CHECKED		
BY		
DATE		

PROFILE	BY	DATE
DESIGNED		
PLOTTED		
ALIGNED		
CHECKED		
RT. OF HWY CHECKED		
BY		
DATE		



Colorado Department of Transportation
Structure Inspection and Inventory Report (English Units)

Highway Number (ON) 5D: 007D 1

Mile Post (ON)11: 64.166 mi

Bridge Key: D-16-DM Inspection Date: 1/17/2007 Sufficiency Rating: 91.5 Not Eligible

Rgn/Sectn 2E/2M:	68	Hist Signif 37:	5	UW Inspection Date 93B:	
Trans Region 2T:	02	Posting status 41:	A	SI Date 93C:	
County Code 3:	013	Service on/un 42A/B:	1 5	Bridge Cost 94:	\$ 0
BOULDER		Main Mat/Desgn 43A/B:	6 6	Roadway Cost 95:	\$ 0
Place Code 4:	41835	Appr Mat/Desgn 44A/B:	0 0	Total Cost 96:	\$ 0
LAFAYETTE		Main Spans Unit 45:	2	Year of Cost Estimate 97:	
Rte.(On/Under)5A:	1	Approach Spans 46:	0	Brdr Brdg Code/% 98A/B:	
Signing Prefix 5B:	3	Horiz Clr 47:	44.0 ft	Border Bridge Number 99:	
Level of Service 5C:	1	Max Span 48:	70.8 ft	Defense Highway 100:	0
Directional Suffix 5E:	0	Str Length 49:	146.0 ft	Parallel Structure 101:	N
Feature Intersected 6:		Curb Wdth L/R 50A/B:	0.0 ft 0.0 ft	Direction of Traffic 102:	2
COAL CREEK		Width Curb to Curb 51:	44.0 ft	Temporary Structure 103:	
Facility Carried 7:		Width Out to Out 52:	46.5 ft	Highway System 104:	0
SH 7 ML		Deck Area:	6,792. sq. ft	Fed Lands Hiway 105:	0
Alias Str No.8A:		Min Clr Ovr Brdg 53:	99.99	Year Reconstructed 106:	0000
#D-16-CO		Min Undrclr Ref 54A:	N	Deck Type 107:	1
Prll Str No. 8P		Min Undrclr 54B:	0.0 ft	Wearing Surface 108A:	6
		Min Lat Clrnce Ref R 55A:	N	Membrane 108B:	2
Location 9:		Min Lat Undrclr R 55B:	0.0 ft	Deck Protection 108C:	1
1.7 MI E OF JCT US 287		Min Lat Undrclr L 56:	0.0 ft	Truck ADT 109:	4 %
Max Clr 10:	328.1 ft	Deck 58:	7	Trk Net 110:	1
BaseHiway Net12:	0	Super 59:	8	Pier Protection 111:	#
IrsinvRout 13A:	0000000000	Sub 60:	7	NBIS Length 112:	Y
IrssubRout No13B:	00	Channel/Protection 61:	7	Scour Critical 113:	8
Latitude 16:	40d 00' 01"	Culvert 62:	N	Scour Watch 113M:	0
Longitude 17:	105d 03' 29"	Oprrtg Rtg Method 63:	5 No rating	Future ADT 114:	28,086
Range18A:	69 W	Operating Rating 64:	79.0	Year of Future ADT 114:	2028
Township18B:	69	Inv Rtg Method 65:	5	CDOT Str Type 120A:	CBGC
Section18C:	6	Inventory Rating 66:	44.0	CDOT Constr Type 120B:	3.
Detour Length 19:	6.0 mi	Asph/Fill Thick 66T:	004 "in"	Inspection Indic 122A:	
Toll Facility 20:	3	Str. Evaluation 67:	7	Inspection Trip 122AA:	
Custodian 21:	1	Deck Geometry 68:	6	Scheduling Status 122B:	
Owner 22:	1	Undrclr Vert/Hor 69:	N	Maintenance Patrol 123:	19
Functional Class 26:	14	Posting 70:	5	Expansion Dev/Type124:	0
Year Built 27:	1990	Waterway Adequacy 7:	8	Brdg Rail Type/Mod 125A/B:	Y 0
Lanes on 28A:	2	Approach Alignment 72:	8	Posting Trucks 129A/B/C:	0 0 0
Lanes Under 28B:	0	Type of Work 75A:		Str Rating Date 130:	6/22/1998
ADT 29:	18,600	Work Done By 75B:		Special Equip 133:	
Year of ADT 30:	2008	Length of Improvment 76:	0.0 ft	Vert Clr N/E 134A/B/C:	X 99.99 0.00
Design Load 31:	6	Insp Team Indicator 90B:	White Team (Ric	Vert Clr S/W 135A/B/C:	X 99.99 0.00
Apr Rdwy Width 32:	44.0 ft	Inspector Name 90C:	CHURCHESK	Vertical Clr Date:	5/5/1905
Median 33:	0	Frequency 91:	48 months	Weight Limit Color: 134:	0
Skew 34:	0.00 °	FC Frequency 92A:	-1	Str Billing Type:	U
Structure Flared 35:	0	UW Frequency 92B:	-1	Userkey 1 - System:	ONSYS
Sfty Rail 36a/b/c/d:	1 0 1 1	SI Frequency 92C:	-1	Userkey 7-Update Indi:	
Rail ht36h:	35 "in"	FC Inspection Date 93A:			

Inspector Name: CHURCHESK

Colorado Department of Transportation
Structure Inspection and Inventory Report (English Units)

Highway Number (ON) 5D: 007D 1

Mile Post (ON)11: 64.166 mi

Element Inspection Report

Elm/Env	Description	Units	Total Qty	% in 1	CS 1	% in 2	CS 2	% in 3	CS 3	% in 4	CS 4	% in 5	CS 5
26/1	Conc Deck/Coatd Bars	(SF)	6,789	100 %	6,789	0 %	0	0 %	0	0 %	0	0 %	0
104/1	P/S Conc Box Girder	(LF)	568	100 %	568	0 %	0	0 %	0	0 %	0	0 %	0
210/1	R/Conc Pier Wall	(LF)	24	100 %	24	0 %	0	0 %	0	0 %	0	0 %	0
215/1	R/Conc Abutment	(LF)	94	100 %	94	0 %	0	0 %	0	0 %	0	0 %	0
234/1	R/Conc Cap	(LF)	44	100 %	44	0 %	0	0 %	0	0 %	0	0 %	0
325/1	Slope Prot/Berms	(EA)	2	100 %	2	0 %	0	0 %	0	0 %	0	0 %	0
326/1	Bridge Wingwalls	(EA)	4	100 %	4	0 %	0	0 %	0	0 %	0	0 %	0
334/1	Metal Rail Coated	(LF)	292	100 %	292	0 %	0	0 %	0	0 %	0	0 %	0
338/1	Conc Curbs/SW	(LF)	292	100 %	292	0 %	0	0 %	0	0 %	0	0 %	0
340/1	Superstr Cnc Coating	(EA)	1	100 %	1	0 %	0	0 %	0	0 %	0	0 %	0
341/1	Substr Conc Coating	(EA)	1	100 %	1	0 %	0	0 %	0	0 %	0	0 %	0
359/1	Soffit Smart Flag	(EA)	1	0 %	0	100 %	1	0 %	0	0 %	0	0 %	0
501/1	Channel Cond	(EA)	1	100 %	1	0 %	0	0 %	0	0 %	0	0 %	0
504/1	BankCond	(EA)	1	100 %	1	0 %	0	0 %	0	0 %	0	0 %	0

Elem/Env	Description	Element Notes
26/1	Conc Deck/Coatd Bars	3 Inches to 4 inches asphalt. New overlay since last inspection. Looks good.
104/1	P/S Conc Box Girder	Girder 2D has been patched about 20 ft. from Abutment 3.
210/1	R/Conc Pier Wall	Looks good.
215/1	R/Conc Abutment	Look good.
234/1	R/Conc Cap	Looks good.
325/1	Slope Prot/Berms	Up to 2 ft. diam. angular rock riprap on both abutment slopes. Looks good.
326/1	Bridge Wingwalls	Light vertical/diag. cracks with efflor. in #1 Rt. and #3 Right. Some minor washing alongside #1 Left wing.
334/1	Metal Rail Coated	Galvanized Type Y rail with concrete end posts. Looks good.
338/1	Conc Curbs/SW	Several trans. cracks in both.
340/1	Superstr Cnc Coating	On exterior girders, overhangs, and curbs. Looks OK.
341/1	Substr Conc Coating	On abutments, wingwalls, pier cap, and pier wall. Some graffiti on pier wall.
359/1	Soffit Smart Flag	Few light trans. cracks without efflor. scattered about. Light trans. & diag. cracks in Bays 2A and 2C near Abut. 3 with light efflor., and one with rust stains in Bay 2C.
501/1	Channel Cond	Coal Creek. Silt bottom, alignment OK. Flow is through both spans and around Pier 2 wall. Channel has degradation and cutting (See 2003 Photos).
504/1	BankCond	Steep cut, covered with grass and a few trees.

Colorado Department of Transportation
Structure Inspection and Inventory Report (English Units)

Highway Number (ON) 5D: 007D 1

Mile Post (ON)11: 64.166 mi

Maintenance Activity Summary

MMS Activity	Description	Recommended	Status	Target Year	Est Cost
353.08	Br Dk Rpr	1/29/2003	-1	2003	-1

Seal cracks in apshalt surfacing.

A-DOT001	Repl Super	1/29/2003	-1	2003	-1
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Generated by pontis on 01/30/2003

Bridge Notes

Colorado Department of Transportation
Structure Inspection and Inventory Report (English Units)

Highway Number (ON) 5D: 007D 1

Mile Post (ON)11: 64.166 mi

Inspection Notes

Temperature: 18 Degrees
Time: 8:50
Weather: Clear

Scope:

NBI: Element: Underwater: Fracture Critical: Other: Type: Regular NBI

Inspector: CHURCHESK

Inspection Team:

Inspection Date: 01/17/2007

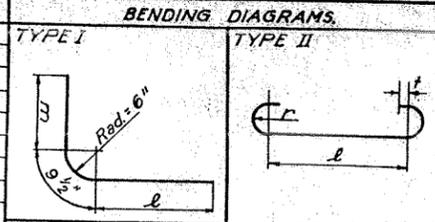
Inspector

Inspector

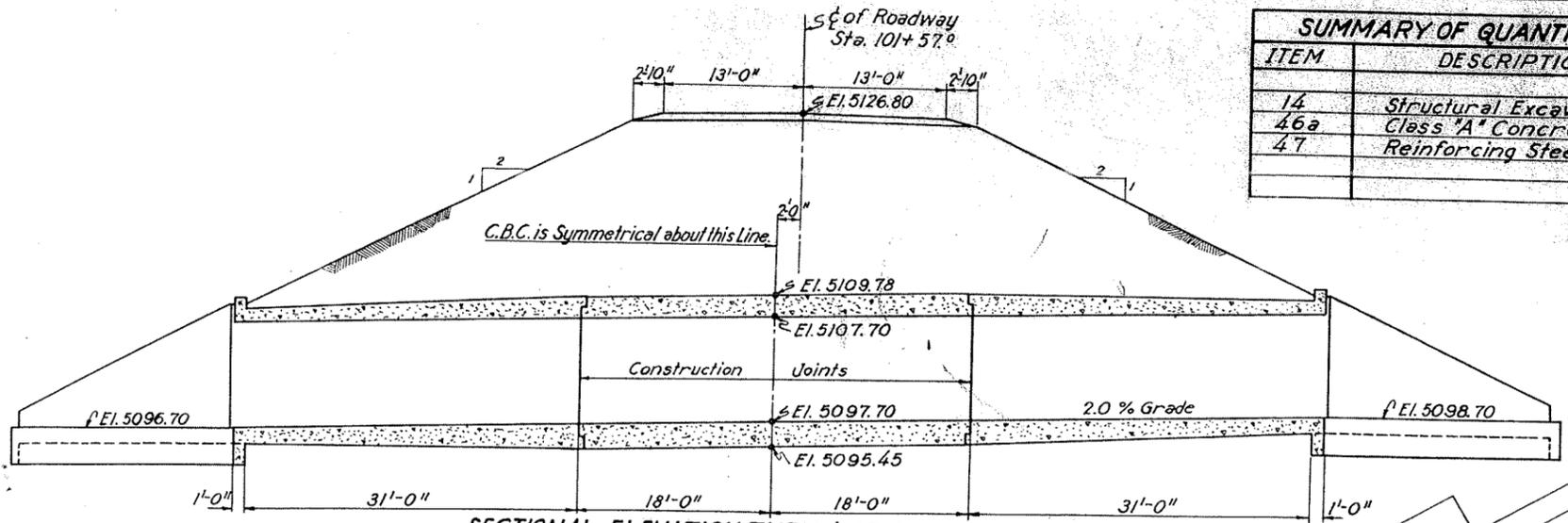
SUMMARY OF QUANTITIES FOR ENTIRE STRUCTURE			
ITEM	DESCRIPTION	UNIT	TOTAL
14	Structural Excavation	Cu.Yd.	390
46a	Class "A" Concrete	Cu.Yd.	306
47	Reinforcing Steel (incl. 1% overrun)	Lb.	27,600

FED. ROAD DIST. NO.	STATE	F.A.S.	SHEET NO.	TOTAL SHEETS
3	COLO.	4-A (1)	9	

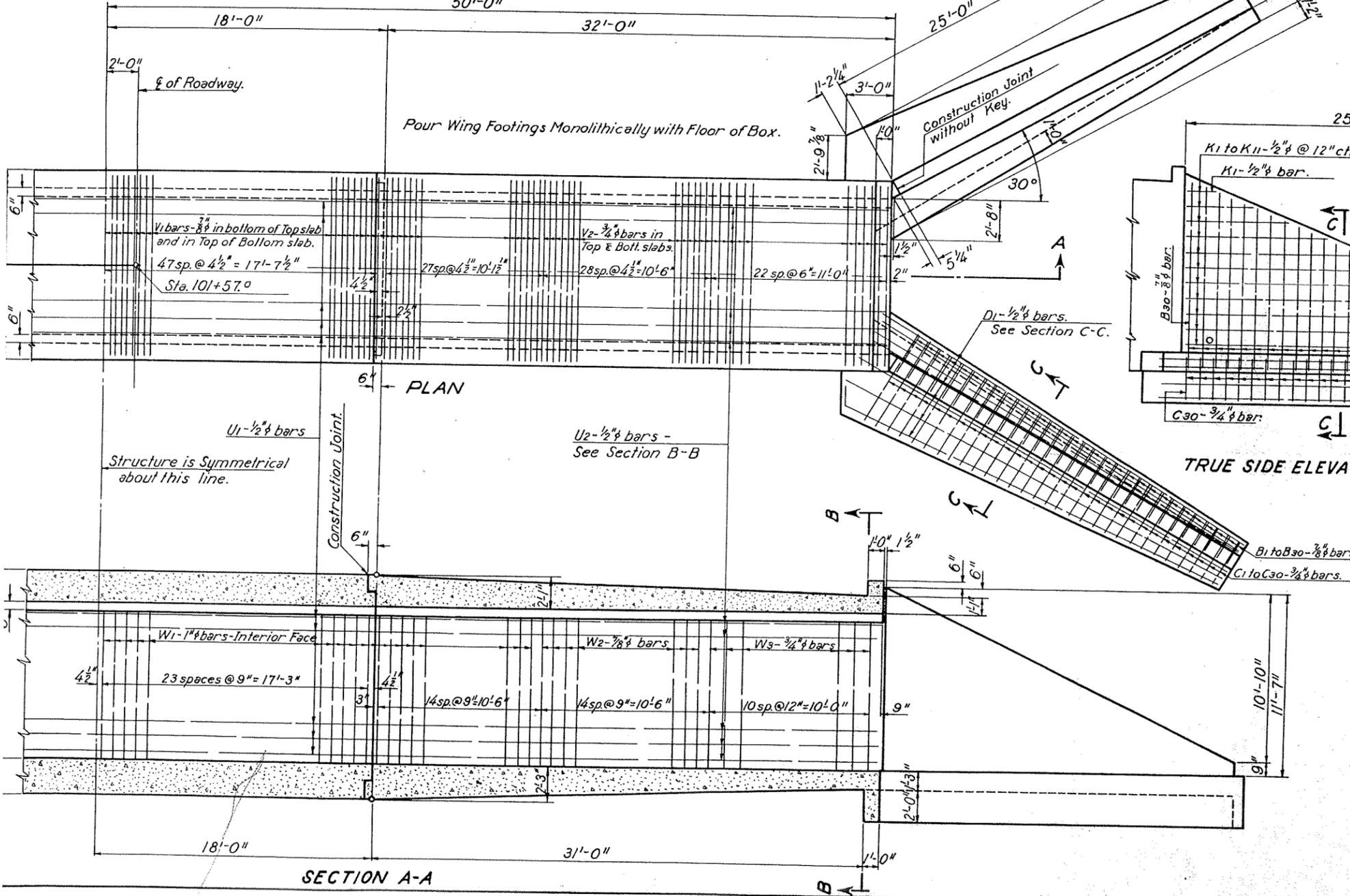
BAR LIST FOR ENTIRE STRUCTURE									
Mark	Size	Type	No. Req'd	Length	l	m	r	t	
B1				21'-8"	0'-10 1/2"	1'-0"			
B10	7/8"	I	4 Each	by 5' to 14'-9"	by 4 1/2' to 11'-9"	by 2' to 2'-2 1/2"			
B30				14'-9"	11'-9"	2'-2 1/2"			
C1				5'-5 1/2"	2'-4 1/2"				
C10	3/4"	I	4 Each	by 1 1/2' to 9'-1"	by 1 1/2' to 6'-0"	2'-3 1/2"			
C30				9'-1"	6'-0"				
D1	1/2"		36	27'-0"					
K1				11'-9"					
K10	1/2"		4 Each	by 2 3/4' to 24'-8"					
K11				24'-8"					
U1	1/2"		40	35'-8"					
U2	1/2"		80	31'-8"					
V1	7/8"	II	298	13'-6"	11'-2"	3 1/2"	3"		
V2	3/4"	II	204	13'-2"	11'-2"	3"	2 1/2"		
W1	1"	II	152	13'-8"	11'-0"	4"	3 1/2"		
W2	7/8"	II	56	13'-4"	11'-0"	3 1/2"	3"		
W3	3/4"	II	44	13'-0"	11'-0"	3"	2 1/2"		



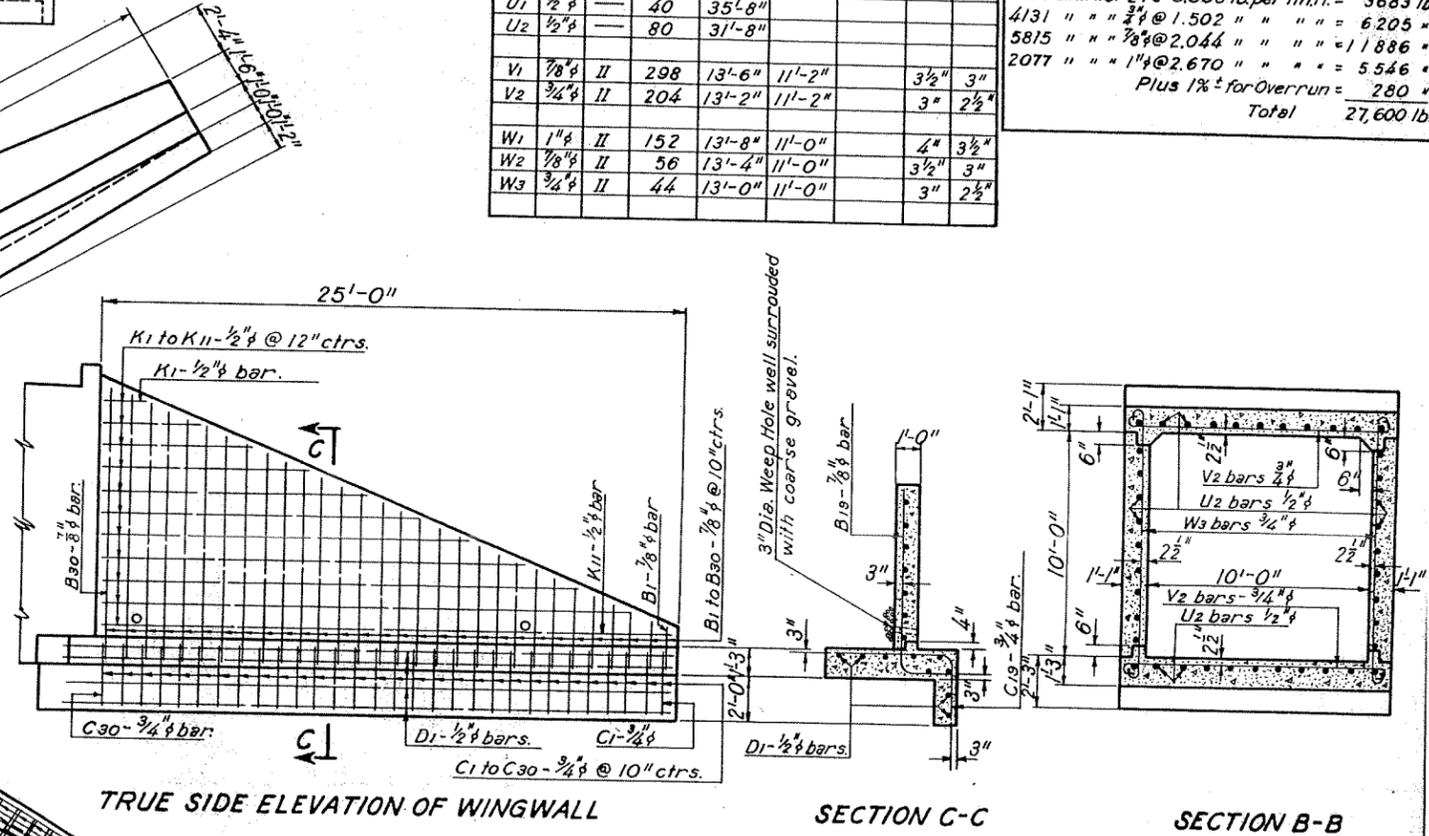
BAR SUMMARY	
5513 Lin. ft. of 3/4" @ 0.668 lb. per lin. ft. =	3683 lb
4131 " " " 1/2" @ 1.502 " " " =	6205 "
5815 " " " 7/8" @ 2.044 " " " =	11886 "
2077 " " " 1" @ 2.670 " " " =	5546 "
Plus 1% for Overrun = 280 "	
Total	27,600 lb



SECTIONAL ELEVATION THRU & OF C.B.C. AT STA. 101+57.0



SECTION A-A



TRUE SIDE ELEVATION OF WINGWALL

SECTION C-C

SECTION B-B

GENERAL NOTES

All work shall be done in accordance with the Standard Specifications of the Colorado State Highway Dept. adopted June 1st, 1937.

All concrete to be Class "A".

All walls shall have forms on both sides.

All construction joints shall be thoroughly cleaned before fresh concrete is poured.

All bars to be deformed.

Minimum distance from centerline of bars, to edge of concrete, to be 3".

Long bars when spliced shall be lapped two feet.

All reinforcing bars shall be tagged with station number, and letter designation.

This design is not to be used if fill is greater than that shown in above section.

Pour wing footings monolithically with floor of Box.

Pour wings independent of sides of Box.

The use of additional construction joints undesirable, should field conditions require, construction joints may be made on a vertical plane perpendicular to centerline of culvert, and shall have keys as shown.

Supporting soils for culvert must be composed of firm and uniform material throughout.

All backfilling to and over culvert shall be laid in layers not to exceed six inches in depth, and each layer shall be rolled or hand tamped where inaccessible, also moistened when necessary.

COLORADO STATE HIGHWAY DEPARTMENT

10'x10' C.B.C.

Across Gulch

Sta. 101+57.0

Near Lafayette Sec. 37 T. 11 S. R. 62 W.

Designed by R.D.W. Approved by _____

Made by D.M.D. Bridge Engineer

Checked by _____ Date: _____ 1938

Bridge Inspection Report (English Units)

Structure No: 007D064470BR

Abbr. Str. No: 007D064470BR

Inspection Date : 3/5/2007

IDENTIFICATION

1. State: 08 Colorado
 2. District: Reg 6 MSec 8
 3. County: BROOMFIELD
 4. City: BROOMFIELD
 5A. Route On/Under: Route On Str
 5B. Route Signing Prefix: 3 State Hwy
 5C. Level of Service: 1 Mainline
 5D. Route Number: 0007D
 5E. Directional Suffix: 0 N/A (NBI)
 6. Feature Intersected: UNNAMED DRAINAGE
 7. Facility Carried On: SH 7 ML
 9. Location: 1.9 MI E OF JCT US 287
 11. Mile Post: 64.47 miles
 16. Latitude: 40 d 0 ' 1.5 "
 17. Longitude: 105 d 3 ' 15.2 "
 18A. Survey Range: -
 18B. Survey Township: -
 18C. Survey Section: -
 98. Border Bridge Code: (N/A)
 99. Neighboring State Code: (N/A)
 % of Responsibility: 0%

CLASSIFICATION

20. Toll Facility: 3 On free road
 21. Custodian: State Highway Agency
 22. Owner: State Highway Agency
 26. Function Class: 14 Urban Other Princ
 37. Historical Sign.: 5 Not eligible for NRHP
 100. Defense Hwy: 0 Not a STRAHNET hwy
 101. Parallel Str: No II Bridge Exists
 102. Dir of Traffic: 2 2-way traffic
 103. Temporary Str: Not a Temporary Structure
 104. Highway Sys: 0 Not on NHS
 112. NBIS Length: Too Short

GEOMETRIC DATA

10. Max. Ver Clr : 99.99 ft.
 32. Appr. Roadway Width: 44 ft.
 33. Bridge Median: 0 No median
 34. Skew: 0 degree(s)
 35. Structure Flared: Structure Not Flared
 47. Horizontal Clr 44 ft.
 48. Length of Max. Span: 10 ft.
 49. Structure Length: 10 ft.
 50A. Curb/Sdwk Width (lt): 0 ft.
 50B. Curb/Sdwk Width (rt): 0 ft.
 51. Curb to Curb Width: 0 ft.
 52. Out to Out Width: 98 ft.
 53. Min Vert Clr Over: 99.9 ft.
 54A. Ref Min Ver Clr Und: Not a Highway or RR
 54B. Min Vert Clr Und: 0 ft.
 55A. Ref Min Lat Clr Und: Not a Highway or RR
 55B. Min Lat Clr Under (rt): 0 ft.
 56. Min Lat Clr Under (lt): 0 ft.
 Deck Area: 980 sq. ft.

STRUCTURE TYPE AND MATERIALS

43A. Main Span Material: 1 Concrete
 43B. Main Span Design: 19 Culvert
 44A. Approach Material: 0 Other
 44B. Approach Design: 00 Other (NBI)
 45. No of Main Spans: 1
 46. No of App. Spans: 0
 107. Deck Type: N N/A (NBI)
 108A. Wearing Surface: N N/A (no deck (NBI))
 108B. Membrane: N N/A (no deck (NBI))
 108C. Deck Protection: N N/A (no deck (NBI))
 120A. Structure Type: CBC
 120B. Construction Type: 02

INSPECTION

91. Frequency: 24	90. Inspection Date: 3/5/2007	Next Inspection: 3/5/2009
92A. FC Frequency: N	93A. FC Inspection Date: -	Next FC Inspection: -
92B. UW Frequency: N	93B. UW Inspection Date: -	Next UW Inspection: -
92C. Sp. Frequency: N	93C. Sp. Inspection Date: -	Next Sp. Inspection: -
Elem Insp Freq: 24	Elem Insp Date: 3/5/2007	Next Elem Inspection: 3/5/2009
	Inspector: TRIPLETTJ	Signature: _____

Bridge Inspection Report (English Units)

Structure No: 007D064470BR

Abbr. Str. No: 007D064470BR

Inspection Date : 3/5/2007

NAVIGATION DATA

38. Navigation Control: Permit Not Required
39. Vertical Clearance: 0 ft.
40. Horizontal Clearance: 0 ft.
111. Pier Protection: Not Applicable (P)
116. Lift Bridge Ver. Clr: 0 ft.

AGE AND SERVICE

19. Detour Length: 12 miles.
27. Year Built: 1964
28A. Lanes On: 3
28B. Lanes Under: 0
29. Avg. Daily Traffic: 17400
30. Year of ADT: 2005
42A. Service Type On: 1 Highway
42B. Service Type Under: 5 Waterway
106. Year Reconstructed: 0
109. Truck ADT: 3 %
114. Future ADT: 26300
115. Year of Future ADT: 2025

PROPOSED IMPROVEMENTS

75. Type of Work: -
76. Length of Improvement: 0 ft.
94. Bridge Cost: 0
95. Roadway Cost: 0
96. Total Cost: 0
97. Year of Cost Estimate: 0

Sufficiency Rating: 70
SD/FO Indicator: Not Applicable

APPRAISAL

36A. Bridge Rail: 1 Meets Standards
36B. Transition: 1 Meets Standards
36C. Approach Rail: 1 Meets Standards
36D. Appr. Rail on End : 1 Meets Standards
36H. Rail Height : 150.0 ft.
66T. Asphalt Thickness : 120.0 ft.
67. Structure Evaluation : 7 Above Min Criteria
68. Deck Geometry : N Not applicable (NBI)
69. Und. Clr. Ver. & Hor.: N Not applicable (NBI)
71. Waterway Adequacy: 7 Above Minimum
72. Approach Alignment: 8 Equal Desirable Crit
113. Scour Critical: 8 Stable Above Footing

CONDITION

58. Deck: N N/A (NBI)
59. Superstructure: N N/A (NBI)
60. Substructure: N N/A (NBI)
61. Channel/ Channel Prot.: 7 Minor Damage
62. Culvert: 7 Minor Deterioration

LOAD RATING AND POSTING

31. Design Load: 5 MS 18 (HS 20)
41. Posting Status: A Open, no restriction
63. Operating Rating Mthd: 5 No rating
64. Operating Rating: 40 tons.
65. Inventory Rating Mthd: 5 No rating
66. Inventory Rating: 36 tons.
70. Posting: 5 At/Above Legal Loads
129. Truck Load: 0 0 0 tons.
130. Rating Date: 3/5/2007

Bridge Notes:

GPS RT: LAT N 40 Degrees 00 Minutes 00.5 Seconds LONG W 105 Degrees 03 Minutes 15.2 Seconds

Inspection Notes:

Date - 3/5/2007

Temp: 50 Degrees Time: 11:30 AM Weather: Cloudy, breeze

Bridge Inspection Report (English Units)

Structure No: 007D064470BR

Abbr. Str. No: 007D064470BR

Inspection Date : 3/5/2007

ELEMENT DATA COLLECTION

Element Number	Element Name	Env.	Total Qty	Unit	Quantity by State				
					1	2	3	4	5
Substructure									
241 (4)	Concrete Culvert	1	98	(LF)	98	0	0	0	0
10 ft W x 10 ft H poured-in-place concrete box culvert with 13 ft cover - Alkali crust along waterline on walls.									
Other Elements									
327 (3)	Culvert Wingwalls	1	4	(EA)	4	0	0	0	0
Concrete, flared, at inlet and outlet - Separated 1/4 inch and leaning in 1 inch at top at left wingwalls.									
335 (3)	Culvert Headwalls	1	2	(EA)	2	0	0	0	0
Concrete at inlet and outlet - Minor scaling on top of left headwall.									

Rounds, Jesse

From: Portia Pearson [portia0217@yahoo.com]
Sent: Friday, February 19, 2010 8:12 PM
To: Rounds, Jesse
Subject: Coal Creek trail opening
Follow Up Flag: Follow up
Flag Status: Completed

Do you have a map of where the trail will be and the existing trails in the area? Also, where is the meeting on Mar. 1 ?

Looking forward to the opening,

Portia Pearson

Rounds, Jesse

From: John Dawson [john.dawson@sopheon.com]
Sent: Thursday, February 25, 2010 11:39 AM
To: Boulder County Mountain View Egg Farm
Subject: Add a map to your web page

Hello Jesse,

Your Egg Farm Trail Master Plan web page would be much improved if you included a map of the proposed link trail or of the various options.

I ride my bike out to the "end" of the Coal Creek trail a lot & am really looking forward to having it continued on to the east.

Thanks very much,

John Dawson

This message and any attachments are intended only for the addressee. If you are not the intended recipient, please delete the material from your system immediately, and notify the sender by email. If you are not the intended recipient, you may not copy, disclose, or distribute this message or its contents to any other person, and any such actions may be unlawful. Any views or opinions presented are solely those of the author and do not necessarily represent those of Sopheon.

This message has been checked for all known viruses by the MessageLabs Virus Scanning Service.

3/3/2010

Rounds, Jesse

From: Wendy Phillips [wendancer@gmail.com]
Sent: Monday, March 01, 2010 2:41 PM
To: Boulder County Mountain View Egg Farm
Subject: Egg Farm Trial

I am out of town and cannot attend the Lafayette meeting this evening. I do have some thoughts that I would like considered in this process.

I would encourage the following logic:

Keep the trail system to the East side of the property where there has already been development. This does a number of things.

It keeps a larger portion of the virgin open space just that ... virgin.

It uses existing roads to bring people onto the property in a safe manner. There is room to add a turn lane from Highway 7 onto the property at this juncture.

A trail head could be built on the property that uses currently developed areas and could accommodate parking for the future market that was discussed at the first meeting.

Aligning the trail, close or adjacent to the Broomfield's Anthem trail system accomplishes one of the goals of the GOCO Grant in that it promotes co-operation between neighboring communities. It would be a wonderful symbiotic match. Using the diversity of a more natural path aligned with one that is hardened. This would enhance the user availability many fold. Just from a revenue perspective from Erie and Lafayette's point of view this brings the opportunity to bring literally thousands of potential users into their retail community in the most "Green" possible way.

There is an existing underpass adjacent to the east side of the Egg Farm that was used as a passage for cattle. It appears that this could be redeveloped into a human passageway. This would allow many positive things to take place; First and foremost it would allow for the safe passage immediately from the Egg Farm property. This would allow limited access to Highway 7.(Boulders Department of

Transportation was shocked recently with the numbers regarding traffic when they studied the current Flagg Dr. - East Boulder County Rd 1 project.) Limiting access to the highway is a very ... very good thing; This alignment with the current cattle underpass brings the trail up out of the Floodway. This means a couple of things. The alignment could be carried out with less disruption of its surrounds. It most likely would be cheaper, allowing funding of other trail improvements. There would be less seasonal flooding cutting down on use. The maintenance issues would be more simple. Additionally the County's current thought process about not allowing development in areas next to Coal Creek has been loud and clear. If we hold to the County's current logic, there is something fundamentally wrong with the idea of bringing thousands of users to an area they have felt a need to protect.

Let's limit the impact of altering what hasn't been altered. Let's improve the areas that have already been impacted. Let's promote community and intergovernmental co-operation. Let's use common sense with design criteria that uses and enhances what is in place and promotes the health and safety of the users.

Thank You David Phillips

Rounds, Jesse

From: Bob Snow [rdsnow2@gmail.com]
Sent: Monday, March 01, 2010 9:21 AM
To: Boulder County Mountain View Egg Farm
Cc: kpritz@broomfield.org
Subject: comments on egg farm trail
Follow Up Flag: Follow up
Flag Status: Flagged

On behalf of many Anthem Ranch residents I would like to congratulate and thank Boulder County and Lafayette on acquiring the Mountain View Egg Farm as open space. The potential for development of this gem is significant.

This is a wonderful complement to the recently completed Broomfield Park/open space that abuts on the eastern Boulder County boundary. The Broomfield Park is approximately 1000 ft wide and runs south from SH 7 to the Northwest Parkway.

The completion of the Coal Creek trail between Lafayette and Erie and connections with existing Broomfield trails will create a valuable contiguous open space / park area that will serve both Boulder and Broomfield equally well. The walkers, joggers and bikers will be very supportive.

Many of us in Anthem Ranch do have a concern regarding the abandoned yellow chicken sheds. They have not been used for many years and appear to be rapidly deteriorating with the ravages of weather, especially wind. They present a threat to any housing downwind of future Chinook winds.

The sheds also present an attractive nuisance for users of the trails and open space, especially since they will be much more accessible. They also present visual pollution to users of the trails.

The sheds may create an opportunity for recycling the buildings or at least the materials that could be reclaimed.

We are requesting that their removal be considered at this time since it would benefit all future users. Thank you.

3/3/2010

Bob Snow

Anthem ranch resident

Rounds, Jesse

From: Tom Merrigan [tomer26@comcast.net]
Sent: Monday, March 01, 2010 7:15 AM
To: Boulder County Mountain View Egg Farm
Cc: Barb
Subject: Egg Farm and trail
Follow Up Flag: Follow up
Flag Status: Flagged

What's the status of plans to tear down the egg farm buildings and remove the junk vehicles, etc. from the site? Have you considered just offering the material for scrap, ie., just give it away on Craig's List to anyone who will tear it down and haul it away?

Do yo have funding to take the trail under Highway 7? Where will it go on the Erie side?

Thanks for your response.

Tom and Barb Merrigan

Rounds, Jesse

From: ojibwelw [ojibwelw@yahoo.com]
Sent: Monday, March 01, 2010 11:56 AM
To: Boulder County Mountain View Egg Farm
Subject: Public meeting tonight - website

Greetings Jesse-

I was looking on your website to see if I could find a map or other visual representation of different proposed routes for this part of the trail, but nothing is there. I would like to be prepared for the meeting tonight, but without a map how can I know what you are thinking? Perhaps I was not seeing where it was posted. I looked under documents and it just said "information to be posted at a later date".

Please post a map on your website -
http://www.bouldercounty.org/openspace/management_plans/eggfarmtrail/connect.htm
ASAP today so people may see your proposals before the meeting.

Thank you for your immediate attention to this oversight.

Regards,

Lori Windle

" The Nation shall be strong, so long as the hearts of the women are not on the ground."

Instruction to the Tsistsistas (Cheyenne) Nation People

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 01, 2010 11:37 AM
To: Boulder County Mountain View Egg Farm
Subject: questions about Egg Farm project

Hi Jesse,

Some questions that have come up at the meeting we had in the neighborhood this week:

- 1) When is the deconstruction on the Egg Farm buildings going to take place?
- 2) Who gets to use the oil/gas roads that are on the property?
- 3) Who specifically decided on the three choices for the trails that you showed on the map at the advisory meeting last week?
- 4) And can we present our own option for a trail tonight?
- 5) Where is BC department of transportation planning on a possible pedestrian bridge crossing the highway?

Thank you, Wendy Phillips

Rounds, Jesse

From: Mike Jinnette [mike.jinnette@gmail.com]
Sent: Monday, March 01, 2010 2:26 PM
To: Boulder County Mountain View Egg Farm
Subject: Mountain View Egg Farm Trail

Hi,

I'm Mike Jinnette and I live at 12549 Flagg Drive, Lafayette, CO.

For years my wife, children and I have used and appreciated the Coal Creek trail between Lafayette and Louisville. We have long anticipated its extension out to Flagg Park and beyond. Unfortunately, I'm out of town on business and can't attend tonight's meeting, so I'd like to submit my input via this e-mail.

Having lived across the street from Coal Creek for many years, we have had the pleasure to observe from a distance this rich riparian habitat. During this time, we have seen Great Horned Owls, Red-tailed Hawks and even a Bald Eagle pair nesting in the cottonwoods along the creek. I want to see the trail extended to Erie, but I do hope that you will keep the trail up high on the egg farm property away from this section of the creek.

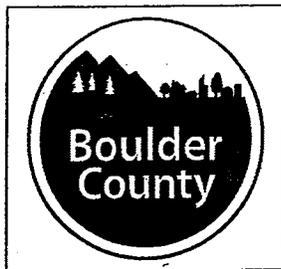
Protecting this section of the creek and keeping it wild will benefit all Boulder County citizens by giving them the opportunity to see these magnificent birds soaring overhead.

Thank you for the wonderful Boulder County trail system. Keep up the good work.

Truly,

Mike Jinnette

3/3/2010



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

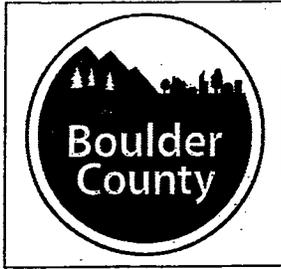
Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

I'D LIKE TO SEE THE N S TRAIL RUN
ALONG THE EAST SIDE OF THE FARM
CLOSER TO THE DEVELOPEMENT AT THE TOP
OF THE HILL WHERE RIGHT OF WAY IS
ALREDY DEVELOPED AND AWAY FROM
THE HAWKS - OWLS - HERONS - KINGFISHERS
THAT LIVE IN MY BACK YARD - SOME
ACCOMODATION FOR TRAY CROSSING?



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

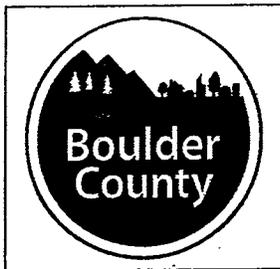
Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

Include into the planning a "path" to connect to Broomfield County trails.

Please keep the option open for this trail to connect to Broomfield.

In future plans to include sidewalks along Highway 7 and underpass/overpass for access to the coal creek trail in consideration for safety as well as accessibility by foot, bike, and parking for vehicles.

Please do not be take action based on Broomfield's "non purchase" participation b/c it may change in the near future. Thanks



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

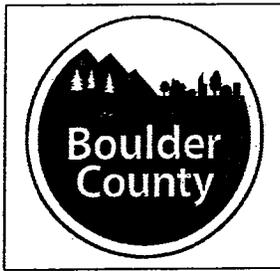
Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

This is a great project. I live in Erie and connection to Anthem, Louisville and Lafayette are critical for providing alternative transportation options.



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: _____

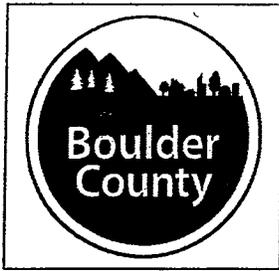
Organization (if applicable): _____

Contact Information Address/E-mail: _____

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Please consider alignment or planning to connect trails (to Broomfield) E-W for this section of the trail in the master plan; to protect wildlife, and for concerns of public safety (trespassers!!) via hwy 7. Thank you!

Please consider alignment a bit further E. of the proposed master plan to maximize views, accessible land and to inter-connect communities.



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

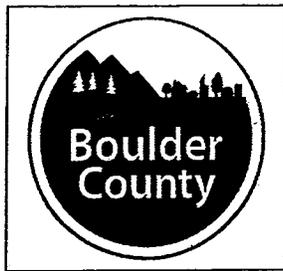
Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

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We do not want the
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Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

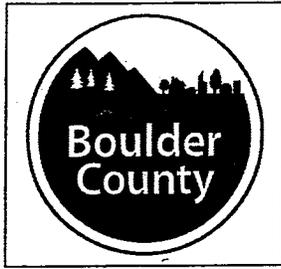
Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

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Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: MALCOLM PHILLIPS

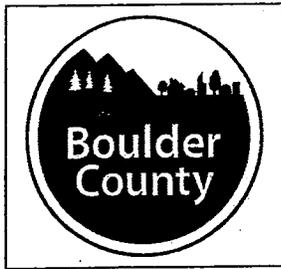
Organization (if applicable): _____

Contact Information Address/E-mail:

724-931-9230

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THAT LIVE IN MY BACK YARD - SOME
ACCOMODATION FOR HWY CROSSING?



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: Sunnie T. Glaister

Organization (if applicable): self (Anthem resident)
(former Erie resident)

Contact Information Address/E-mail:
glaister@netzero.net

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

Include into the planning a "path" to connect to Broomfield County trails.

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Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: Peter Loris

Organization (if applicable): Loris And Associates

Contact Information Address/E-mail:

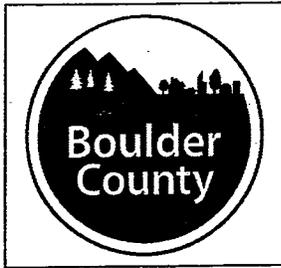
2585 Trailridge Dr. E.

Lafayette, CO 80026

p.loris@LorisAndAssociates.com

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

This is a great project! I live in Erie and connection to Anthem, Louisville and Lafayette are critical for providing alternative transportation options.



Public Comment Form
Mountain View Egg Farm
Trail Master Plan

Public Meeting
March 1, 2010

Name: Pamela Miller-Chavez

Organization (if applicable): _____

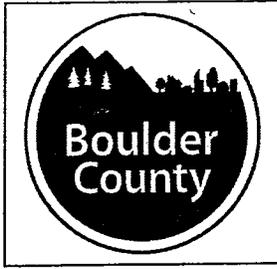
Contact Information Address/E-mail:

3315 Chapin Pl
Broomfield Co
80023

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

Please consider alignment or planning to connect trails (to Broomfield) E-W for this section of the trail in the master plan; to protect wildlife, and for concerns of public safety (trespassers!!) via hwy 7. Thank you!

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Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name:

Cheryl Beck

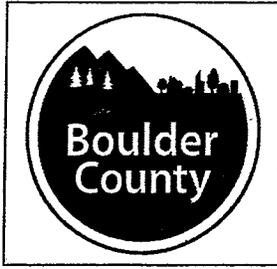
Organization (if applicable):

Contact Information Address/E-mail:

Cherylabeck@
mesanetworks.net

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

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supposed to be protecting.



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: Dave Belin

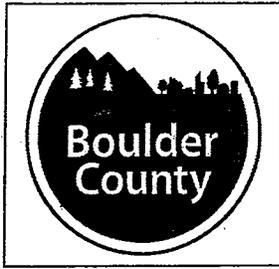
Organization (if applicable): LOSAC

Contact Information Address/E-mail:

174 High Country Dr.
Lafayette CO 80026
davebelin@mindspring.com

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

I support the continuation of the Rock Creek / Coal Creek trail system to the north and east to Flagg Park trailhead, to Hwy 7, and north to Erie. I support the trail uses of walking, dog walking, biking, and horseback riding. I support an 8 to 10 foot wide path for the duration of the path. I would like to see Broomfield / Anthem's interest kept as secondary until they are willing to participate in the funding.



Public Comment Form

Mountain View Egg Farm Trail Master Plan

Public Meeting

March 1, 2010

Name: Wendy Phillip's

Organization (if applicable): personal statement for our own properties.

Contact Information Address/E-mail:
wendancer@gmail.com
12626 Flagg Dr, Lafayette

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

— ~~the~~ Our family owns the three properties along Coal Creek bordered by the Hwy 7 bridge / overpass at the creek.

— We have had nesting + mating G.H. Owls, Redtail Hawk and a host of other birds on these properties + across the creek on the BC Creek open space for 20 years.

— We have also watched the Hwy 7 traffic flow grow + continue to grow. We have seen hundreds of accidents at and around the bridge, including a little girl riding her bicycle into the Hwy traffic + being killed.

— We have watched the City of Lafayette

plant 50-100 trees in the floodway just downstream from us... Hello!

- We have been told we can not build even a fence on our properties, and in fact we have been told by BC that our houses "should be deconstructed" in official County ~~Public~~ ^{commission} hearings.

- We have been told by the City of Lafayette staff (at least 4x) over 15 years that they do not need to work with us because they have E.D. over us + can just take what they want when they need it.

So. ① you might understand our concerns over Lafayettes, Erie's + BC's "master plan" to build a trail connection under the Hwy 7 bridge. We DO NOT WANT THAT TO HAPPEN for so many reasons.. ~~although~~

SAFETY, WILDLIFE, PRIVACY, Floodway issues, E. Domain, ... etc.

② We would prefer you put the trail higher up on the Egg Farm because of the nesting + mating bird population

2 continued And because the traffic flow of a trail like that (connecting SO MANY communities) will be used far more than you can imagine right now. (TAKE A LOOK at traffic flow increase on Hwy 7 in the last 10 years) IT IS NOT A GOOD IDEA TO BRING ALL THAT FUTURE GROWTH + TRAFFIC CLOSE TO THE CREEK/Hwy 7 bridge.

3 We want you all to put aside your anger ~~at Lafayette~~ and posturing at Broomfield and work with them since their citizens will HUGELY IMPACT this open space - fencing or not.

WPP

Rounds, Jesse

From: Tom Merrigan [tomer26@comcast.net]
Sent: Monday, March 01, 2010 7:15 AM
To: Boulder County Mountain View Egg Farm
Cc: Barb
Subject: Egg Farm and trail
Follow Up Flag: Follow up
Flag Status: Flagged

What's the status of plans to tear down the egg farm buildings and remove the junk vehicles, etc. from the site? Have you considered just offering the material for scrap, ie., just give it away on Craig's List to anyone who will tear it down and haul it away?

Do yo have funding to take the trail under Highway 7? Where will it go on the Erie side?

Thanks for your response.

Tom and Barb Merrigan

Rounds, Jesse

From: Bob Snow [rdsnow2@gmail.com]
Sent: Monday, March 01, 2010 9:21 AM
To: Boulder County Mountain View Egg Farm
Cc: kpritz@broomfield.org
Subject: comments on egg farm trail
Follow Up Flag: Follow up
Flag Status: Flagged

On behalf of many Anthem Ranch residents I would like to congratulate and thank Boulder County and Lafayette on acquiring the Mountain View Egg Farm as open space. The potential for development of this gem is significant.

This is a wonderful complement to the recently completed Broomfield Park/open space that abuts on the eastern Boulder County boundary. The Broomfield Park is approximately 1000 ft wide and runs south from SH 7 to the Northwest Parkway.

The completion of the Coal Creek trail between Lafayette and Erie and connections with existing Broomfield trails will create a valuable contiguous open space / park area that will serve both Boulder and Broomfield equally well. The walkers, joggers and bikers will be very supportive.

Many of us in Anthem Ranch do have a concern regarding the abandoned yellow chicken sheds. They have not been used for many years and appear to be rapidly deteriorating with the ravages of weather, especially wind. They present a threat to any housing downwind of future Chinook winds.

The sheds also present an attractive nuisance for users of the trails and open space, especially since they will be much more accessible. They also present visual pollution to users of the trails.

The sheds may create an opportunity for recycling the buildings or at least the materials that could be reclaimed.

We are requesting that their removal be considered at this time since it would benefit all future users. Thank you.

7/7/2010

Bob Snow

Anthem ranch resident

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 01, 2010 11:37 AM
To: Boulder County Mountain View Egg Farm
Subject: questions about Egg Farm project

Hi Jesse,

Some questions that have come up at the meeting we had in the neighborhood this week:

- 1) When is the deconstruction on the Egg Farm buildings going to take place?
- 2) Who gets to use the oil/gas roads that are on the property?
- 3) Who specifically decided on the three choices for the trails that you showed on the map at the advisory meeting last week?
- 4) And can we present our own option for a trail tonight?
- 5) Where is BC department of transportation planning on a possible pedestrian bridge crossing the highway?

Thank you, Wendy Phillips

Rounds, Jesse

From: tomer26@comcast.net
Sent: Monday, March 01, 2010 11:46 AM
To: Rounds, Jesse
Subject: Re: Egg Farm and trail

Thanks.

----- Original Message -----

From: "Jesse Rounds" <jrounds@bouldercounty.org>
To: "Tom Merrigan" <tommer26@comcast.net>
Sent: Monday, March 1, 2010 10:00:01 AM GMT -07:00 US/Canada Mountain
Subject: RE: Egg Farm and trail

Mr. Merrigan,

Thank you for your email. The primary reason the buildings have remained on site is a lack of funds for removal. Declining tax revenues have significantly reduced our ability to contract out demolition services. Beyond funding concerns, no management plan has been developed for the site as a whole. Without that plan making decisions about structures on site might be premature.

Your questions regarding the alignment of the trail outside of the Egg Farm property are best answered by Kristine Nelson, a Boulder County Transportation Planner and the project manager for the larger trail project. I've forwarded your email to her.

Thanks again and if you have any questions, please do not hesitate to contact me.

Jesse

-----Original Message-----

From: Tom Merrigan [mailto:tommer26@comcast.net]
Sent: Monday, March 01, 2010 7:15 AM
To: Boulder County Mountain View Egg Farm
Cc: Barb
Subject: Egg Farm and trail

What's the status of plans to tear down the egg farm buildings and remove the junk vehicles, etc. from the site? Have you considered just offering the material for scrap, ie., just give it away on Craig's List to anyone who will tear it down and haul it away?

Do yo have funding to take the trail under Highway 7? Where will it go on the Erie side?

Thanks for your response.

Tom and Barb Merrigan

Rounds, Jesse

From: ojibwelw [ojibwelw@yahoo.com]
Sent: Monday, March 01, 2010 11:56 AM
To: Boulder County Mountain View Egg Farm
Subject: Public meeting tonight - website

Greetings Jesse-

I was looking on your website to see if I could find a map or other visual representation of different proposed routes for this part of the trail, but nothing is there. I would like to be prepared for the meeting tonight, but without a map how can I know what you are thinking? Perhaps I was not seeing where it was posted. I looked under documents and it just said "information to be posted at a later date".

Please post a map on your website -
http://www.bouldercounty.org/openspace/management_plans/eggfarmtrail/connect.htm
ASAP today so people may see your proposals before the meeting.

Thank you for your immediate attention to this oversight.

Regards,

Lori Windle

" The Nation shall be strong, so long as the hearts of the women are
not on the ground."

Instruction to the Tsistsistas (Cheyenne) Nation People

Rounds, Jesse

From: Mike Jinnette [mike.jinnette@gmail.com]
Sent: Monday, March 01, 2010 2:26 PM
To: Boulder County Mountain View Egg Farm
Subject: Mountain View Egg Farm Trail

Hi,

I'm Mike Jinnette and I live at 12549 Flagg Drive, Lafayette, CO.

For years my wife, children and I have used and appreciated the Coal Creek trail between Lafayette and Louisville. We have long anticipated its extension out to Flagg Park and beyond. Unfortunately, I'm out of town on business and can't attend tonight's meeting, so I'd like to submit my input via this e-mail.

Having lived across the street from Coal Creek for many years, we have had the pleasure to observe from a distance this rich riparian habitat. During this time, we have seen Great Horned Owls, Red-tailed Hawks and even a Bald Eagle pair nesting in the cottonwoods along the creek. I want to see the trail extended to Erie, but I do hope that you will keep the trail up high on the egg farm property away from this section of the creek.

Protecting this section of the creek and keeping it wild will benefit all Boulder County citizens by giving them the opportunity to see these magnificent birds soaring overhead.

Thank you for the wonderful Boulder County trail system. Keep up the good work.

Truly,

Mike Jinnette

Rounds, Jesse

From: Wendy Phillips [wendancer@gmail.com]
Sent: Monday, March 01, 2010 2:41 PM
To: Boulder County Mountain View Egg Farm
Subject: Egg Farm Trial

I am out of town and cannot attend the Lafayette meeting this evening. I do have some thoughts that I would like considered in this process.

I would encourage the following logic:

Keep the trail system to the East side of the property where there has already been development. This does a number of things.

It keeps a larger portion of the virgin open space just that ... virgin.

It uses existing roads to bring people onto the property in a safe manner. There is room to add a turn lane from Highway 7 onto the property at this juncture.

A trail head could be built on the property that uses currently developed areas and could accommodate parking for the future market that was discussed at the first meeting.

Aligning the trail, close or adjacent to the Broomfield's Anthem trail system accomplishes one of the goals of the GOCO Grant in that it promotes co-operation between neighboring communities. It would be a wonderful symbiotic match. Using the diversity of a more natural path aligned with one that is hardened. This would enhance the user availability many fold. Just from a revenue perspective from Erie and Lafayette's point of view this brings the opportunity to bring literally thousands of potential users into their retail community in the most "Green" possible way.

There is an existing underpass adjacent to the east side of the Egg Farm that was used as a passage for cattle. It appears that this could be redeveloped into a human passageway. This would allow many positive things to take place; First and foremost it would allow for the safe passage immediately from the Egg Farm property. This would allow limited access to Highway 7.(Boulders Department of

Transportation was shocked recently with the numbers regarding traffic when they studied the current Flagg Dr. - East Boulder County Rd 1 project.) Limiting access to the highway is a very ... very good thing; This alignment with the current cattle underpass brings the trail up out of the Floodway. This means a couple of things. The alignment could be carried out with less disruption of its surrounds. It most likely would be cheaper, allowing funding of other trail improvements. There would be less seasonal flooding cutting down on use. The maintenance issues would be more simple. Additionally the County's current thought process about not allowing development in areas next to Coal Creek has been loud and clear. If we hold to the County's current logic, there is something fundamentally wrong with the idea of bringing thousands of users to an area they have felt a need to protect.

Let's limit the impact of altering what hasn't been altered. Let's improve the areas that have already been impacted. Let's promote community and intergovernmental co-operation. Let's use common sense with design criteria that uses and enhances what is in place and promotes the health and safety of the users.

Thank You David Phillips

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Tuesday, March 02, 2010 12:08 PM
To: Rounds, Jesse
Subject: RE: questions about Egg Farm project

Thank you Jesse.

One clarification on #3...what are the names of the people on the 'Planning Team' and who do they work for? We are interested in knowing who the "we" is that have been making such important recommendations.

And I missed the answer to the oil/gas roads...are these available for use by the public?

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 02, 2010 10:33 AM
To: Wendy and David Phillips
Subject: RE: questions about Egg Farm project

Certainly,

3) Who specifically decided on the three choices for the trails that you showed on the map at the advisory meeting last week?

The three options you saw on the map at the POSAC meeting were developed in response to conditions on the ground on the Egg Farm. The Planning Team discussed the best location for the trail based on existing uses and resources and decided that we should stick to the northwestern area of the property for possible trail alignments. Then we used existing railroad grades to layout the four alignments on the map. This was done to reduce disturbance. Railroad grades, as man-made features not only are already disturbed areas, but lend themselves to trails as they are stable enough to top with a trail.

4) And can we present our own option for a trail tonight?

The questions at the meeting went long and I didn't have an opportunity to provide a group exercise in which we could layout trails. However, during the meeting and after the meeting many people took the opportunity to propose alternative alignments.

5) Where is BC department of transportation planning on a possible pedestrian bridge crossing the highway?

As Kristine Nelson stated last night, the options for crossing Colorado Route 7 need to be studied through a feasibility study. At this point, no such study would be conducted until GOCO decides how it will spend its grant funds. As I stated at POSAC and again last night any discussion of how the trail would cross 7 is premature and not pertinent to this process.

That said, I do completely understand your concerns with regard to trail routing and I hope that seeing the team last night and listening to some of the process we plan to go through assuages some of your fears that we plan to do things without consulting with the public. While we cannot guarantee that everyone is happy at the end of a public process, we work to ensure that everyone is heard and their questions and concerns are addressed.

Thanks again and I look forward to working with you as the project goes forward.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Tuesday, March 02, 2010 10:07 AM
To: Rounds, Jesse
Subject: RE: questions about Egg Farm project

Hi Jesse,

We would appreciate you answering the questions 2, 3, 4 and 5. Thank you. Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 02, 2010 9:43 AM
To: Wendy and David Phillips; Nelson, Kristine
Subject: RE: questions about Egg Farm project

Ms. Phillips,

I had a draft response to your email ready and then last night's meeting snuck up on me. Please let me know if any of these questions went unanswered last night and I'll happily respond.

Thanks,

Jesse

7/7/2010

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]

Sent: Monday, March 01, 2010 11:37 AM

To: Boulder County Mountain View Egg Farm

Subject: questions about Egg Farm project

Hi Jesse,

Some questions that have come up at the meeting we had in the neighborhood this week:

- 1) When is the deconstruction on the Egg Farm buildings going to take place?
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Thank you, Wendy Phillips

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Tuesday, March 02, 2010 10:07 AM
To: Rounds, Jesse
Subject: RE: questions about Egg Farm project

Hi Jesse,

We would appreciate you answering the questions 2, 3, 4 and 5. Thank you. Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 02, 2010 9:43 AM
To: Wendy and David Phillips; Nelson, Kristine
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Ms. Phillips,

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Jesse

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Sent: Monday, March 01, 2010 11:37 AM
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- 5) Where is BC department of transportation planning on a possible pedestrian bridge crossing the highway?

Thank you, Wendy Phillips

Rounds, Jesse

From: Sunnie Glaister [sunnieglaister@q.com]
Sent: Thursday, March 04, 2010 8:19 PM
To: Rounds, Jesse
Subject: clarifications on the Mtn View Egg project

Hi Jesse,

While talking with Pam, a neighbor who attended the March 1 meeting at the Lafayette Library, we were unsure of a copy of things and wanted to ask you to make some clarification.

The GOCO grant is already submitted, correct? Awards will be announced this month or next?

The Master Plan is being submitted on April 25, correct? Who will review the plan?

You mentioned that the project is budgeted for \$1.5 million. Is it possible to view a cost breakdown of the project? Curious to understand the reason for the high expense for a mile length trail.

Sunnie Glaister.

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Tuesday, March 02, 2010 12:30 PM
To: Rounds, Jesse
Subject: RE: questions about Egg Farm project

Thank you for all your help here.

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 02, 2010 12:26 PM
To: Wendy and David Phillips
Subject: RE: questions about Egg Farm project

Wendy,

The oil and gas roads are built and maintained by the owner of the mineral rights, Noble Energy. As the landowner we often use these trails for access to our sites. In terms of public access, these are not public roads. In rare instances we use a segment of an oil and gas road for trail purposes. This is considered a last resort as the roads are not designed for foot traffic, heavy foot traffic can interfere with gas well operations, and the roads can be removed or relocated with very little notice. For example when we discuss trails crossing these roads with any mineral right owner, we often have to spend a significant amount of time designing the crossing of the road, we reinforce the road and change the surface type in order to facilitate movement.

As for the planning team:

There are 7 staff members from Boulder County Parks and Open Space. Their position titles are:

Plant Ecology Supervisor

Senior Wildlife Specialist

Trails Supervisor

Agricultural Resources Manager

Lead Ranger

Natural Resource Planner

7/7/2010

Weeds Coordinator

There is one team member from the City of Lafayette:

Superintendent of Open Space and Trails

There is one member of the team from Boulder County Transportation:

Regional Trails Planner

I hope that helps,

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]

Sent: Tuesday, March 02, 2010 12:08 PM

To: Rounds, Jesse

Subject: RE: questions about Egg Farm project

Thank you Jesse.

One clarification on #3...what are the names of the people on the 'Planning Team' and who do they work for? We are interested in knowing who the "we" is that have been making such important recommendations.

And I missed the answer to the oil/gas roads...are these available for use by the public?

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]

Sent: Tuesday, March 02, 2010 10:33 AM

To: Wendy and David Phillips

Subject: RE: questions about Egg Farm project

7/7/2010

Certainly,

3) Who specifically decided on the three choices for the trails that you showed on the map at the advisory meeting last week?

The three options you saw on the map at the POSAC meeting were developed in response to conditions on the ground on the Egg Farm. The Planning Team discussed the best location for the trail based on existing uses and resources and decided that we should stick to the northwestern area of the property for possible trail alignments. Then we used existing railroad grades to layout the four alignments on the map. This was done to reduce disturbance. Railroad grades, as man-made features not only are already disturbed areas, but lend themselves to trails as they are stable enough to top with a trail.

4) And can we present our own option for a trail tonight?

The questions at the meeting went long and I didn't have an opportunity to provide a group exercise in which we could layout trails. However, during the meeting and after the meeting many people took the opportunity to propose alternative alignments.

5) Where is BC department of transportation planning on a possible pedestrian bridge crossing the highway?

As Kristine Nelson stated last night, the options for crossing Colorado Route 7 need to be studied through a feasibility study. At this point, no such study would be conducted until GOCO decides how it will spend its grant funds. As I stated at POSAC and again last night any discussion of how the trail would cross 7 is premature and not pertinent to this process.

That said, I do completely understand your concerns with regard to trail routing and I hope that seeing the team last night and listening to some of the process we plan to go through assuages some of your fears that we plan to do things without consulting with the public. While we cannot guarantee that everyone is happy at the end of a public process, we work to ensure that everyone is heard and their questions and concerns are addressed.

Thanks again and I look forward to working with you as the project goes forward.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]

Sent: Tuesday, March 02, 2010 10:07 AM

To: Rounds, Jesse

Subject: RE: questions about Egg Farm project

Hi Jesse,

We would appreciate you answering the questions 2, 3, 4 and 5. Thank you.

7/7/2010

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 02, 2010 9:43 AM
To: Wendy and David Phillips; Nelson, Kristine
Subject: RE: questions about Egg Farm project

Ms. Phillips,

I had a draft response to your email ready and then last night's meeting snuck up on me. Please let me know if any of these questions went unanswered last night and I'll happily respond.

Thanks,

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Monday, March 01, 2010 11:37 AM
To: Boulder County Mountain View Egg Farm
Subject: questions about Egg Farm project

Hi Jesse,

Some questions that have come up at the meeting we had in the neighborhood this week:

- 1) When is the deconstruction on the Egg Farm buildings going to take place?
- 2) Who gets to use the oil/gas roads that are on the property?
- 3) Who specifically decided on the three choices for the trails that you showed on the map at the advisory meeting last week?
- 4) And can we present our own option for a trail tonight?
- 5) Where is BC department of transportation planning on a

possible pedestrian bridge crossing the highway?

Thank you, Wendy Phillips

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 08, 2010 9:38 AM
To: Rounds, Jesse
Subject: Flagg neighborhood meeting

Hi Jesse,

Hope you had a great weekend! Spring is in the air. I've attached a photo from yesterday of the two Great Horned Owls in the tree across the creek from our house on Hwy 7 and Baseline. I wish I had a better camera.

We are having a couple meetings this week on Flagg Drive.

- 1) Would it be possible for me to borrow the posters you have of the trail plans for our meetings?
- 2) Who specifically do I speak to about the plans for Flagg Park on the Flagg Drive side of the creek?
- 3) Is there anything in writing about the Flagg Park plans for the future?
- 4) Am I correct in hearing that the 120th to Flagg Park will be completed this year? If not, when?

Thank you, Wendy



Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 08, 2010 11:24 AM
To: Rounds, Jesse
Subject: change of schedule

Hi Jesse,

Can you explain how the cancellation of the March POSAC meeting will change the schedule of the Egg Farm process?

Wendy

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 08, 2010 12:15 PM
To: Rounds, Jesse
Subject: RE: change of schedule

Thank you, Jesse. Can we get some trail maps for our meeting this week?

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Monday, March 08, 2010 11:29 AM
To: Wendy and David Phillips
Subject: RE: change of schedule

Wendy,

We'll push back the Egg Farm Trail Master Plan presentation to April. So, assume that everything will push back a month. Other than that, nothing will change. If anything, this will afford us some time to hold a public meeting outside the official POSAC, LOSAC, and Commissioner's meetings to discuss the trail plan draft.

Jesse

-----Original Message-----

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Wendy

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From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 08, 2010 12:38 PM
To: Rounds, Jesse
Subject: RE: change of schedule

I was hoping I could show the neighborhood the poster board map you had had the Lafayette library meeting...the one that shows the whole system. I could return it on Friday morning to you.

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Monday, March 08, 2010 12:24 PM
To: Wendy and David Phillips
Subject: RE: change of schedule

Wendy,

Are you speaking of trail maps for the Egg Farm property? If so, there are no trail maps. The staff team decided that it was important to look at trail options again without limiting them to 4 or 5 options but instead to focus on a trail study area as I presented on the 1st. I will post the trail study area on the web today.

Jesse

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Sent: Monday, March 08, 2010 11:24 AM

To: Rounds, Jesse

Subject: change of schedule

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Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, March 08, 2010 12:56 PM
To: Rounds, Jesse
Subject: RE: change of schedule

Thanks Jesse,

Many of the people out here do not get internet because of availability and cost. It would really help to have some hard copy to show or pass out.

W

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Monday, March 08, 2010 12:48 PM
To: Wendy and David Phillips
Subject: RE: change of schedule

Wendy,

Unfortunately, that isn't my poster. I'll ask Kristine about lending it out. If not, I plan to put both the Egg Farm and larger trail map on the website.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Monday, March 08, 2010 12:38 PM
To: Rounds, Jesse
Subject: RE: change of schedule

I was hoping I could show the neighborhood the poster board map you had had the Lafayette library meeting...the one that shows the whole system. I could return it on Friday morning to you.

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7/7/2010

Sent: Monday, March 08, 2010 12:24 PM
To: Wendy and David Phillips
Subject: RE: change of schedule

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Sent: Monday, March 08, 2010 11:24 AM

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Subject: change of schedule

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Can you explain how the cancellation of the March POSAC meeting will change the schedule of the Egg Farm process?

Wendy

Rounds, Jesse

From: David Phillips [ravenphillips@gmail.com]
Sent: Wednesday, March 10, 2010 9:12 AM
To: Boulder County Mountain View Egg Farm
Subject: Resend of thoughts on Egg Farm Trail

My wife made me aware that this email was forwarded using her email address. I would like to be put on record that these are my individual thoughts and should have come to you using my email at ravenphillips@gmail.com. Sorry for the misunderstanding.

I am out of town and cannot attend the Lafayette meeting this evening. I do have some thoughts that I would like considered in this process.

I would encourage the following logic:

Keep the trail system to the East side of the property where there has already been development. This does a number of things.

It keeps a larger portion of the virgin open space just that ... virgin.

It uses existing roads to bring people onto the property in a safe manner. There is room to add a turn lane from Highway 7 onto the property at this juncture.

A trail head could be built on the property that uses currently developed areas and could accommodate parking for the future market that was discussed at the first meeting.

Aligning the trail, close or adjacent to the Broomfield's Anthem trail system accomplishes one of the goals of the GOCO Grant in that it promotes co-operation between neighboring communities. It would be a wonderful symbiotic match. Using the diversity of a more natural path aligned with one that is

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hardened. This would enhance the user availability many fold. Just from a revenue perspective from Erie and Lafayette's point of view this brings the opportunity to bring literally thousands of potential users into their retail community in the most "Green" possible way.

There is an existing underpass adjacent to the east side of the Egg Farm that was used as a passage for cattle. It appears that this could be redeveloped into a human passageway. This would allow many positive things to take place; First and foremost it would allow for the safe passage immediately from the Egg Farm property. This would allow limited access to Highway 7. (Boulders Department of Transportation was shocked recently with the numbers regarding traffic when they studied the current Flagg Dr. - East Boulder County Rd 1 project.) Limiting access to the highway is a very ... very good thing; This alignment with the current cattle underpass brings the trail up out of the Floodway. This means a couple of things. The alignment could be carried out with less disruption of its surrounds. It most likely would be cheaper, allowing funding of other trail improvements. There would be less seasonal flooding cutting down on use. The maintenance issues would be more simple. Additionally the County's current thought process about not allowing development in areas next to Coal Creek has been loud and clear. If we hold to the County's current logic, there is something fundamentally wrong with the idea of bringing thousands of users to an area they have felt a need to protect.

Let's limit the impact of altering what hasn't been altered. Let's improve the areas that have already been impacted. Let's promote community and intergovernmental co-operation. Let's use common sense with design criteria that uses and enhances what is in place and promotes the health and safety of the users.

Thank You David Phillips

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Thursday, March 11, 2010 1:09 PM
To: Rounds, Jesse
Subject: tonights/THURSmeeting

Hi Jesse,

Two clarifications for our meeting tonight.

- 1) When is the section from 120th to Flagg Park going to be underconstruction and finished?
- 2) Who is the person in charge of Flagg Park Trailhead...Lafayette, BC?

Good news from Dir.Gerstle about the GOCO grant preliminary approval...J!

Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Thursday, March 11, 2010 4:29 PM
To: Rounds, Jesse
Subject: RE: tonights/THURSmeeting

Thank you Jesse. We appreciate the info.

The FDCA will be drafting a letter to POS and Lafayette this week.

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Thursday, March 11, 2010 4:07 PM
To: Wendy and David Phillips
Subject: RE: tonights/THURSmeeting

Wendy,

I asked Kristine Nelson for help on both questions:

1) The transportation dept. working with BCPOS hopes to have the trail from 120th to Flagg complete by either the end of this year or spring next year.

2) Parks and Open Space is managing the improvements for Flagg Park in collaboration with the Department of Transportation and under the guidance of the City of Lafayette. Lafayette owns Flagg Park and will have ultimate decision-making authority.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Thursday, March 11, 2010 1:09 PM
To: Rounds, Jesse
Subject: tonights/THURSmeeting

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7/7/2010

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- 2) Who is the person in charge of Flagg Park Trailhead...Lafayette, BC?

Good news from Dir.Gerstle about the GOCO grant preliminary approval...J!

Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]

Sent: Thursday, March 18, 2010 9:01 AM

To: Rounds, Jesse

Subject: Egg Farm process

Hi Jesse,

I'd like to read the file, including public comments on the Egg Farm process. Do I need to come in to do that, or do you have it on the internet?

Thanks, Wendy Phillips

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Friday, March 19, 2010 9:34 AM
To: Rounds, Jesse
Subject: RE: Egg Farm process

Thanks Jesse.

What I mean by the file, is that with most other public projects, the public is allowed access to the meeting notes for the discussions taking place outside of the public meetings. Parks and Open Space has a lot of staff working on this Egg Farm plan, GOCO and the connections between communities. We are interested in hearing about and participating in these conversations. It has always been as simple as me coming in and reading through the file to catch up on the activities of the process.

Thanks, Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Thursday, March 18, 2010 11:23 AM
To: Wendy and David Phillips
Subject: RE: Egg Farm process

Wendy,

I'll be posting the comments online soon. As for files on the process, I am not sure what you mean. I have notes taken by Kristan Pritz from the first public meeting and I have a schedule which is posted online.

Our next public outreach will be April 1st at the Lafayette Open Space Advisory Committee.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Thursday, March 18, 2010 9:01 AM
To: Rounds, Jesse
Subject: Egg Farm process

7/7/2010

Hi Jesse,

I'd like to read the file, including public comments on the Egg Farm process. Do I need to come in to do that, or do you have it on the internet?

Thanks, Wendy Phillips

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Friday, March 26, 2010 8:30 AM
To: Frye, Renata; 'Monte Stevenson'
Cc: Rounds, Jesse
Subject: Flagg Drive Community comments on Egg Farm

Dear City of Lafayette and Boulder County Parks and Open Space,

Attached is a letter from the Flagg Drive Community Association to the Parks and Open Space Advisory Committees. We would appreciate each member of the committees receiving a copy of this letter from our association. Additionally, we are asking Jesse Rounds to submit this letter to the public files for the Egg Farm Master Plan and GOCO process.

Thank you,

Wendy Phillips

7/7/2010

March 22, 2010

Boulder County Parks and Open Space Advisory Committee
Lafayette Open Space Advisory Committee

Dear Boulder County and City of Lafayette Parks and Open Space Advisory Committees,

The Flagg Drive Community Association (FDCA) has a membership of all 34 residences along Flagg Drive between 120th and Hwy 7. At our meeting on March 11 we discussed and formalized our group statement to you relating to the public processes of the Egg Farm Open Space Master Plan and the GOCO Grant. Additionally we addressed the potential future connections of the Egg Farm trail system with Erie across Hwy 7 and the proximity to the Broomfield trail systems as an important part of these discussions. We voted unanimously to write letters to all the Parks and Open Space Advisory Committees that may be involved in the future of the trail systems connecting to the Egg Farm and it's Open Space Master Plan.

The Rapid Resource Assessment and the GOCO grant are hurriedly influencing the Egg Farm Master Plan, and although this is understandable given the great opportunity to win the grant and to connect our communities, we feel it just as important at this time to include discussions of other pertinent considerations that will be outlined in this letter.

Firstly, we cannot state strongly enough that we support the efforts of Parks and Open Space in Boulder County and the City of Lafayette. Our residents are long time supporters of Open Space and Wildlife issues and have supported staff many times in the past with our time, efforts and contributions of private property. We ask again that as plans move forward that we be included individually and as a group in the discussions and planning that surround and impact our neighborhood, our rural lifestyle, the wildlife and the safety of people on our roadways.

Following are the points that our neighborhood supported unanimously by vote at our meeting. Although Jesse Rounds has received additional perspective from many of us individually, this letter includes the points that we support as a community presented to the Advisory Committees of the governments surrounding our neighborhood.

(1) It is the wish of the Flagg Drive Community Association that the Egg Farm Open Space Property have equestrian use. The City of Lafayette has supported equestrian use in our areas in the past and we would hope this could continue to all connecting Open Spaces along these eastside corridors.

That said we do not believe Flagg Park could support a viable equestrian trailhead and trailer parking area for several significant reasons. 1) We think the financial investment to stabilize an equestrian trailhead would be wiser and more maintainable at the Egg Farm or on 120th.

2) Flagg Park is a park where children and families come to picnic and hang out. Horses and children do not go together safely. Taking horses out of trailers in and around a family picnic and parking area has safety concerns. 3)The traffic flow along Flagg Drive is dangerous and many animals have been killed there, including horses. The road bend at the Park is a very dangerous corner and has a long history of accidents. 4) Flagg residents continue to have conversations

with Boulder County about how dangerous Flagg Drive commuter and cut-through traffic has been over the years and there have been no consistent solutions so far. 5) The size of the area and erosion around Flagg Park and along Coal Creek is a consistent problem. Stressing this area further is not a good idea for the Park. Please keep it a park and not a trailhead.

We would ask that consideration be granted for equestrian parking at the Egg Farm where there is significantly more room and resources for these activities. Or maybe an equestrian trailhead and parking area could be developed in the industrial area along 120th.

- (2)** The FDCA discussed the plans for the Egg Farm chicken barns and other related outbuildings. Meeting attendees' agreement was unanimous that the deconstruction of the existing chicken barns and related outbuildings be a priority and take place as soon as possible. The Director of BCPOS pointed out that funding for this deconstruction project is currently not available (stated at the public meeting on the Egg Farm Master Plan held at the Lafayette Library March 8, 2010). But it may be feasible to complete the deconstruction project for little or no additional cost to the City and County by actively searching for a buyer who would take down the structures at no cost to the City and County in return for the structures, a point which was proposed by one of the meeting participants present at the public meeting. Even if a "no cost" solution is not feasible, we ask that the City of Lafayette and Boulder County make it a priority to find and allocate the necessary funds to ensure compliance with the original stated plan for the property and to facilitate future development. As long as those barns and outbuildings are in place, we believe it will inhibit achieving the best overall use of the property, could inevitably slow the re-development process, and distract from the best trail options because of safety issues.

We believe removal of these structures will improve the overall natural look and feel of the Mountain View Egg Farm, which is consistent with the goals of the Open Space Program, and which will remove the unsightly structures from being such a prominent eyesore visible on the County's horizon today.

We further believe that removal of these structures will eliminate future maintenance and liability costs that will exist as long as they are left in place. By removing these existing structures as soon as possible, another major benefit the City and County will realize is the increased flexibility in planning the location of the future trail system and any other improvements desired on the property by having opened up a significant portion of the existing land to the most optimum use. Today, with those large structures in place, it significantly limits the land use and trail location options, which we believe would result in a compromised plan. We encourage those in charge of the planning process to set a goal to achieve the best long-term solutions for this unique and excellent property.

- (3)** *FDCA would like to acknowledge the hard work and challenges the City of Lafayette and Boulder County staff, along with the support of its residents and leadership, faced in order to succeed in the purchase of the Egg Farm property. This purchase significantly impacts in surprisingly and positive ways the limitations of the dated Comprehensive Master Plans for connecting Boulder County, Lafayette and Erie. Thanks to this purchase it will be much easier now to complete the trail connections while protecting the creek and riparian corridors along Coal Creek and keeping people safe along Highway 7. In consideration of this...*

We ask that Lafayette and County staff, the Advisory Councils and our elected Leaders take into consideration how this purchase will allow for more inclusive and forward thinking options to the dated Master Plans of each area.

We ask that our leaders live up to the intention of the GOCO Grant and work together to connect the people of all three counties - Boulder, Weld and Broomfield. Connecting ALL the peoples of this area will significantly impact commuting, shopping and recreational alternatives in these communities. Connecting the citizens and visitors of the entire area can be a boon for business and cross recreational options. Citizens from all three counties impact the roads in Boulder County and shopping in Lafayette...residents and businesses in downtown Lafayette will all benefit from the connections offered by the Egg Farm.

We ask that our leaders think beyond the moment and into the near future and realize the benefits of working together with all three counties. The residents want to share the resources and maintenance with ALL our neighbors. It can help with financially caring for and following through on our open space maintenance commitments to wildlife and grass lands and create much safer connections for our children and animals.

We ask that our political leaders and staff forgive Broomfield's inability to help purchase the Egg Farm property and allow for new and creative ways that Broomfield can also contribute to creating connections for ALL trail users in the future. In consideration of the long term future and that nonautomotive connectivity IS THE FUTURE...political posturing should not get in the way of connecting the communities that surround the Egg Farm. This is a very high priority to the people of Boulder County. (If our leaders don't believe that, we can help them become aware.)

Citizens and businesses of Lafayette and Boulder County ask that both the City of Erie and the City and County of Broomfield be included in the planning of the Egg Farm extension during the earliest phases of planning and implementation. Coordinating the planning of the interconnection of these three counties and their trail systems on the Egg Farm property will permit the GoCo grant funds to be used most effectively thus offering the greatest future "bang for the buck", while significantly improving safety and the quality of life for both wildlife and trail users of Lafayette, Boulder County, Erie and the subdivisions of Broomfield.

- (4)** Lastly, FDCA unanimously supports that the trail connection from the Egg Farm on the south side of Hwy 7 to Weld County and Erie on the north side of Highway 7 MUST NOT go under the highway at the Coal Creek overpass. There are many reasons for our stance.

We are told that Boulder County Department of Transportation will deal with this discussion later. We ask that it be discussed now and in relation to the Master Plan at the Egg Farm since that property is the perfect connector to Weld and Broomfield County trail systems.

Although the Comprehensive Plans for Boulder County, City of Lafayette and Erie have always shown that the trail will follow the creek and cross Hwy 7 at the creek overpass, the purchase of the Egg Farm property (unforeseen when the Comprehensive Plans were developed) allows for safer, less expensive and more dynamic options and connections. We also know so much more about the vulnerability of wildlife around the creek corridors and our neighborhood wants the wildlife and

rural character protected. This is the time for staff to rethink its desire to use the creek as a connector to Erie.

The safety issues, or lack thereof, around the County Line Rd/Flagg and Hwy 7 intersections are well documented. Many lives have been damaged and lost at this intersection and around the Coal Creek overpass and intersection. It is not the place to bring more people and distractions to driving.

The concrete cattle underpass under Hwy 7 would make a great and very safe connection to Erie. It is higher up the hill so vehicle visibility is significantly improved and less a distraction to drivers. We believe it would save significantly on the budget to correct its recent problems with drainage...mostly due to Broomfield development, and make the crossing there. We also believe it will save lives.

Some staff at Parks and Open Space have responded to our concerns by saying that the birds and wildlife around the Coal Creek overpass are acclimated to people because of the highway. This is not the case...they are acclimated to moving vehicles. Bringing people, bikes, dogs and horses to this fragile area is NOT ACCEPTABLE. Human and domestic animal traffic needs to be kept away from the Coal Creek area. This is a mating and nesting area and Boulder County residents do not want this disturbed. The people who live along the creek respect the timing of the wildlife and remain supportive and quiet during the mating, nesting and hunting periods. This would not be manageable if the public had access to this part of the county.

Floodway issues and erosion are of great concern to Boulder County, the State of Colorado and the residents along the creek. We do not want any more negative impact to this area and although the people who live here seem to know about how to preserve the wildlife and trees, we are rarely invited to the development conversations. In the past we have tried to be good neighbors to the City of Lafayette and let them make their own decisions about their properties and boundaries. But the County of Boulder has not allowed us the same standards or privilege. We now ask to be involved in anything that impacts the Coal Creek system up and down stream from this corridor. Local knowledge is invaluable.

Thank you for hearing our thoughts. We appreciate being part of the public process and would ask that we be included in future conversations. We can be reached through wendancer@gmail.com.

Respectfully,

Flagg Drive Community Association

**Cc: Jesse Rounds, BCPOS
Erie Parks and Open Space
Broomfield Parks and Open Space
Kristine Nelson, BCDOT**

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]

Sent: Tuesday, March 30, 2010 11:03 AM

To: Rounds, Jesse

Subject: Egg Farm info

Hi Jesse,

A question has come up and I thought you may know the answer. What % of the Egg Farm purchase was funded by Boulder County, and what % was funded by the City of Lafayette?

Thanks, Wendy

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Tuesday, March 30, 2010 11:14 AM
To: Rounds, Jesse
Subject: RE: Egg Farm info

Thanks Jesse, I'll see you there. W

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, March 30, 2010 11:13 AM
To: Wendy and David Phillips
Subject: RE: Egg Farm info

Wendy,

That's a great question. We split the cost with Lafayette 60-40. They will end up paying 60% of the cost and we will end up paying 40%.

Just a reminder, I am going to present the Trail Master Plan at LOSAC on April 1st. I hope you and your neighbors can make it out.

Jesse

-----Original Message-----

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Sent: Tuesday, March 30, 2010 11:03 AM
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A question has come up and I thought you may know the answer. What % of the Egg Farm purchase was funded by Boulder County, and what % was funded by the City of Lafayette?

Thanks, Wendy

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Thursday, April 01, 2010 12:29 PM
To: Boulder County Mountain View Egg Farm
Subject: Egg Farm

Hi Jesse,

I will be attending tonight. I thought a couple weeks ago you said you'd put the public comments on the internet. I do think it a real disservice to the public part of the process to not have allowed the public to read and respond to comments about the Egg Farm Master Plan on the internet. Especially because you plan to get approval for the plan tonight.

Wendy Phillips

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]

Sent: Friday, April 02, 2010 7:01 AM

To: Rounds, Jesse

Subject: Egg Farm

Jesse,

Can we set a time before the 9th of April to walk the property? I would like you to show me the 'yellow' path that you decided was the best route.

Wendy

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, April 05, 2010 7:41 AM
To: Rounds, Jesse
Subject: Egg Farm questions
Follow Up Flag: Follow up
Flag Status: Flagged

Hi Jesse,

Hope you had a nice holiday weekend. We are hoping we can walk the property with you on Wed, Thurs or Fri morning. At this point, Thurs or Fri has the best weather forecast. When will you be available, as I have several neighborhood folks who want to try to arrange their schedules.

Meanwhile, a couple questions:

- 1) How long in mileage is the trail from the connecting trail by Flagg Park to the north end of the Egg Farm?
- 2) Would you email me the photos you used in the presentation to LOSAC?
- 3) Who is the wildlife staff person for this project? Can that person come with us on the trail walk this week? Contact info?
- 4) Where do I get a copy of the GOCO grant?

Thank you, Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, April 05, 2010 7:50 AM
To: Rounds, Jesse
Subject: another question
Follow Up Flag: Follow up
Flag Status: Flagged

I have two more questions:

- 5) What is the distance from 120th to Flagg Park.
- 6) What is the planned budget for that section.
- 7) Proportionately, how will the GOGO grant \$ be split between the north side of Hwy 7 and the southside...and is there a proportion of the grant delegated to south of Flagg Park and a separate portion through the Egg Farm to Hwy 7.

Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, April 05, 2010 10:08 AM
To: Rounds, Jesse
Subject: RE: Egg Farm questions

Thanks. Can we walk the proposed trail this week?

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Monday, April 05, 2010 9:20 AM
To: Wendy and David Phillips
Subject: RE: Egg Farm questions

Wendy,

Just wanted to say that I recieved your emails and I'm working on a response. Lots of good questions and I want to take the time to answer them thoroughly.

Thanks,

Jesse

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Sent: Monday, April 05, 2010 7:41 AM
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Thank you, Wendy

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To: Rounds, Jesse; sunnieglaister@q.com; kmccourt@comcast.com; jdbesse@aol.com; cherylabeck@mesanetworks.net; steve@stevegandy.com; msajban@geosociety.org; p.loris@LorisandAssociates.com; ojibwelw@yahoo.com; jrsajban@aol.com; pdchavez3@msn.com; djb@alumni.rice.edu; mike.jinnette@gmail.com; rdsnow2@gmail.com; tomer26@comcast.net; john.dawson@sopheon.com; portia0217@yahoo.com
Subject: RE: Board of County Commissioners to Review GOCO IGA

Jesse, it is our understanding from the Lafayette City Council Meeting last night that Erie has been given all GOCO grant money to design and implement the trail from and including the Hwy 7 underpass northward through Lafayette owned property to its Vista Parkway. It is interesting that you and Ron planned the Egg Farm public hearing the same night this IGA was discussed at the Erie meeting and this information was not shared publicly.

Please send me the full IGA between Erie and Boulder County this morning so we may have time to review it.

Wendy Phillips

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
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7/7/2010

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Jesse Rounds
Resource Planner
Boulder County Parks and Open Space
(o) 303.678.6271

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Jesse Rounds
Resource Planner
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(o) 303.678.6271

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Wednesday, April 07, 2010 8:53 AM
To: Rounds, Jesse
Subject: RE: Board of County Commissioners to Review GOCO IGA

Jesse...you do not need to check on my statements...

GOCO Awards \$500,000 Grant for Erie/Regional Trail Project**Eastern Link of Coal Creek Trail to Connect to Rock Creek Trail**

Governor Bill Ritter today announced the award of a \$500,000 Great Outdoors Colorado (GOCO) grant for the Eastern Link of the Coal Creek / Rock Creek Trail Project. The project is a multi-jurisdictional collaborative effort to connect the communities of Erie and Lafayette with public open space properties in unincorporated Boulder County, Broomfield, Louisville, Superior and the city of Boulder. The project will construct a new 4-mile long trail that will extend from the existing Rock Creek Trail, near 120th Street in the City of Lafayette, to the existing trail at Vista Pointe Parkway and Coal Creek on Erie's eastern edge. The 4-mile section will complete one of the last remaining links of a 24-mile loop trail that connects these communities.

BACKGROUND: In December 2009, Town of Erie Board of Trustees approved a GOCO Special Opportunity grant application in partnership with Boulder County and the City of Lafayette for construction of various segments of the Coal Creek/Rock Creek Regional Trail. Erie has appropriated \$350,000 in matching funds and is responsible for the segment from Vista Parkway to State Highway 7 which will include approximately 2.5 miles of 8 foot crusher fines trail, two bridges and an underpass at State Highway 7. It was determined that submitting one application on behalf of all three agencies provided the best opportunity for success in this highly competitive Special Opportunity grant cycle. Boulder County submitted the grant application to GOCO and each agency agreed to provide financial assistance to enable the entire project to be completed as follows:

- Erie - \$350,000 (from the Erie Trails and Natural Areas Fund)
- Boulder County - \$300,000
- City of Lafayette - \$250,000

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Wednesday, April 07, 2010 8:41 AM
To: Wendy and David Phillips
Subject: RE: Board of County Commissioners to Review GOCO IGA

7/7/2010

Wendy,

I am working on checking in on your statements in this email. I will get you a copy of the funding mechanism IGA as soon as possible. However, I am not sure I understand your statement regarding the Egg Farm public hearing and the IGA discussion.

Thanks,

Jesse

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Please send me the full IGA between Erie and Boulder County this morning so we may have time to review it.

Wendy Phillips

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Jesse Rounds
Resource Planner
Boulder County Parks and Open Space
(o) 303.678.6271

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Wednesday, April 07, 2010 9:37 AM
To: Rounds, Jesse
Subject: RE: Board of County Commissioners to Review GOCO IGA

An example of a mixed signal...

Erie tells us repeatedly that it has no attachment to connecting their trail under the Hwy 7 bridge, that in fact it is up to Lafayette. Now, without ever involving us in their "Master Plans" or the IGA discussions...all of a sudden, tomorrow they are asking the Commissioners to approve the fact that they exclusively are in charge of the design of the bridge area and the trail across the Lafayette property.

Jesse...Erie could care less about Flagg Drive needs, and the BC Riparian habitat. Lafayette and Erie can barely speak to each other...Lafayette is livid about the idea of connecting to Broomfield...how are they going to protect owls and bald eagles and hawks.

The County Commissioners have no idea what is going on...they depend on staff and either staff is not speaking the truth to us in the public hearings, or staff doesn't know what's going on so how can staff educate the Commissioners.

Jesse...we have been very clear about our concerns. We do not want the Riparian Corridor at Hwy 7 disturbed. I am not getting mixed signals...you all just don't know what is going on...Ron is driving this connection and the birds are going to be lost...THAT IS NOT AN OPTION.

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Wednesday, April 07, 2010 9:05 AM
To: Wendy and David Phillips
Subject: RE: Board of County Commissioners to Review GOCO IGA

Wendy,

Thank you for both emails. What I meant in my email was that I want to make sure by checking with Transportation, Lafayette, and Erie that I give you the right information. You have stated in the past that you feel that you are getting mixed signals from the different parties. I want that to stop. I think a lot of it comes from each department using different terminology, however, that isn't okay when it comes to talking to affected residents.

7/7/2010

As for the IGA, I do not have a copy of the draft IGA or I would send it to you now. I am chasing down a copy and will get it to you as soon as I can.

On an related note, trying to organize a tour of the Egg Farm for this Friday is not possible. I have previous commitments that I cannot break. However, I appreciate the idea and I'd like to try to organize something in the near future. What we could do is walk the proposed trail alignment and then visit specific sites that members of the tour are interested in seeing.

Jesse

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(o) 303.678.6271

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Wednesday, April 07, 2010 10:22 AM
To: Rounds, Jesse
Subject: Egg Farm tour

Jesse,

I have been trying to get you to organize a local group gathering since the POSAC meeting in Feb, as they told us you would work with the local folks.

I am out of town next week. Let's set something up **now** for the week of the 19th. I would like to invite our County Commissioner, Cindy since she is a Lafayette resident...and I think it very important that the Commissioners at least consider the local knowledge.

Wendy Phillips

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Wednesday, April 07, 2010 9:05 AM
To: Wendy and David Phillips
Subject: RE: Board of County Commissioners to Review GOCO IGA

Wendy,

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Resource Planner
Boulder County Parks and Open Space
(o) 303.678.6271

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Friday, April 09, 2010 7:34 AM
To: Rounds, Jesse
Subject: Egg Farm Master Plan

Jesse,

We want to speak only to the Egg Farm "Master Plan" in this email.

Ron clearly said yesterday to the Commissioners that now the process will be to look at the entire project in coming up with a plan.

The Egg Farm Master Plan process from Feb 28th until now has not been a 'robust' public process thus far, infact it has not even happened yet.

Are you going to continue to try to have this "Master Plan" passed at POSAC April 29 and by the Commissioners in May?

Wendy

7/7/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Friday, April 09, 2010 1:44 PM
To: Rounds, Jesse
Subject: RE: Egg Farm Master Plan

Thanks, Jesse.

I don't like having to be so hard on you...but a couple questions need to be addressed as we move on...

The way the Egg Farm process has gone from your first presentation to POSAC on Feb. 28th until even now, we don't feel you (and Ron) have been honest with the public. Nor have you fully presented the public comments and issues, or used the internet commenting appropriately. You have not answered the questions presented to you or nor have you responded in a timely manner to the public process. Overall, this process so far has not had integrity.

- 1) How can we, the public and locals, believe that now you will be able to do the job better?
- 2) Do you want me to resubmit all our questions and requests again...or are you still working on them?

Wendy Phillips, Flagg Drive Community Association

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Friday, April 09, 2010 7:37 AM
To: Wendy and David Phillips
Subject: RE: Egg Farm Master Plan

Wendy,

The management plan is going to be run on a different schedule. I'll be putting up the new schedule in the next couple days. So, we will not be taking anything to POSAC this month or to the commissioners in May.

Jesse

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Subject: Egg Farm Master Plan

Jesse,

We want to speak only to the Egg Farm "Master Plan" in this email.

Ron clearly said yesterday to the Commissioners that now the process will be to look at the entire project in coming up with a plan.

The Egg Farm Master Plan process from Feb 28th until now has not been a 'robust' public process thus far, infact it has not even happened yet.

Are you going to continue to try to have this "Master Plan" passed at POSAC April 29 and by the Commissioners in May?

Wendy

Rounds, Jesse

From: Steve Gandy [steve@stevegandy.com]
Sent: Sunday, April 18, 2010 7:04 AM
To: Rounds, Jesse
Subject: Re: Board of County Commissioners to Review GOCO IGA

You have apparently lost of decided not to answer my previous question. So, I will ask it again.

When, specifically, are we allowed to give input into the Hwy 7 crossing part of the trail project.

--

Steve Gandy
TeachTech Inc. - Consulting, Support, Training
Steve.Gandy@TeachTechColorado.com
www.TeachTechColorado.com

Photography
www.SteveGandy.com/photography

ph: 303.359.9836 fax: 303.942.3667

On Apr 6, 2010, at 4:37 PM, Rounds, Jesse wrote:

> All,
> You're receiving this email because you added your name and email address to the mailing list at the Egg Farm Trail Master Plan meeting on March 1st. On April 1st, we received word that Great Outdoors Colorado (GOCO) had approved the grant request for the completion of the Coal Creek Trail. Now the partners in the grant: Erie, Lafayette, and Boulder County are working out the funding structure for the grant through inter-governmental agreements (IGA).
>
> This Thursday, the Boulder County Commissioners will be discussing and deliberating over the funding mechanism IGA with the Town of Erie. This IGA establishes the funding framework for the grant with the Town of Erie. A separate IGA will be signed with the City of Lafayette.
>
> The Commissioners will meet at the Boulder County Courthouse at 10:30 a.m. on Thursday, April 8th to discuss the IGA. The meeting is open to the public.
> Jesse Rounds
> Resource Planner
> Boulder County Parks and Open Space
> (o) 303.678.6271
>
>

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]

Sent: Wednesday, April 21, 2010 9:08 AM

To: Rounds, Jesse

Subject: POSAC public meeting on Egg Farm

Hi Jesse,

The Egg Farm web site still has April 29th as a public meeting time for POSAC. I thought POSAC was this Thursday. People are calling me asking if they should come...

Is this still correct and if NOT, please post the correct dates and places and process.

Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Friday, April 23, 2010 9:03 AM
To: Rounds, Jesse; 'Jill Wait, Parks & Rec Director'; Nelson, Kristine; jeffm@cityoflafayette.com
Subject: Trail along Coal Creek

Jesse, Jill and Jeff and Kristine,

Our neighborhood is preparing a report related to the design options available to connect Lafayette, Erie, Boulder County and Broomfield by trail. We need the same information you all have gathered to substantiate your past positions on the trail crossing Hwy 7 at the Coal Creek Bridge. Please provide us with:

- 1) The wildlife studies and monitoring data for the open space area along the creek between Vista Ridge in Erie and Flagg Park in BC.
- 2) The engineering and other studies used to determine that the Cattle Underpass was not a viable crossing for the 2007 Amended Comp Plan.
- 3) The company Erie has hired to do the design and engineering studies for the options on the trail crossing to the south side of Hwy 7.
- 4) Jill Wait has stated in a previous email to me that the cattle crossing is not an option. And after two requests as to 'why', she has not responded. Could you please provide an answer to what information her statement is based on.

When could we expect this information be available to us?

Wendy Phillips, Flagg Drive Community Association

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, April 26, 2010 8:43 AM
To: Rounds, Jesse
Subject: RE: Trail walk

Thanks Jesse, for all the info this morning. It is really great to get information that makes sense.

- When is the feasibility study scheduled for completion?
- When do you expect the public meetings related to the feasibility study?
- Could Wed May 12th work if the 5th in the afternoon won't?

Wendy

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Monday, April 26, 2010 8:21 AM
To: Wendy and David Phillips
Subject: RE: Trail walk

Wendy,

And there I go promising too much. Our staff will be in our seasonal all-staff meeting on the 5th. While we're only in the meeting for half a day, it's impact on our work schedule might make it hard to make the 5th. I will ask though.

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Monday, April 26, 2010 7:47 AM
To: Rounds, Jesse
Subject: Trail walk

Hi Jesse,

Quite the rain and snow this weekend! Got some great pictures for you on how high the creek gets this time of year.

7/7/2010

Our neighbors came up with Wednesday May 5th. Any time after 10:30, but more people could come if it is afternoon.

Will that work?

Wendy

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, May 10, 2010 9:11 AM
To: Rounds, Jesse; Nelson, Kristine
Cc: Gerstle, George
Subject: Trail Walk on Friday

Hi Jesse,

Our neighborhood came up with this list for the trail walk, all keeping with the desire for safety along Hwy 7, connectivity to all communities including Broomfield and wildlife habitat preservation. We are also interested in the Counties commitments to the Egg Farm buildings demolition, weed control and grass land restoration, along with long term vision for area population impact and connection to open space south of the Egg Farm.

- 1) Walk the anticipated trail from Flagg Park to Erie property.
- 2) Walk the egg farm boundary east of the yellow buildings and discussion of connectivity to Broomfield at Anthem and south to Pony Estates.
- 3) Walk the Hwy 7 corridor from Anthem down to the creek with a discussion of safety issues.
- 4) Walk the north side of Hwy 7, the Lafayette WW and Solar Facility area to show the trail vision through that area.
- 5) Walk the area along the east side of the Vigil property to discuss wildlife habitats, including burrowing owls and Coal Creek.

Can the time be later in the afternoon? Where will we be meeting?

Thanks, Wendy Phillips

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Tuesday, May 04, 2010 11:22 AM
To: Rounds, Jesse
Subject: RE: Coal Creek Trail at Hwy 7

Hi Jesse,

Some folks are disappointed with the 'noon' time...hoping they could join us late afternoon or after 4:30-5...

But I know it is impossible to come up with something that works for everyone.

Later the better...hopefully we will have a good showing no matter what on the 14th.

Thanks, W

From: Rounds, Jesse [mailto:jrounds@bouldercounty.org]
Sent: Tuesday, May 04, 2010 11:15 AM
To: Wendy and David Phillips
Subject: RE: Coal Creek Trail at Hwy 7

Wendy,

I understand through Ron that you don't think the time for our tour on the 14th will work for the Flagg Drive community. If that's so, please let me know and I will try to work out another time.

Thanks,

Jesse

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Friday, April 30, 2010 12:15 PM
To: Nelson, Kristine
Cc: Rounds, Jesse
Subject: Coal Creek Trail at Hwy 7

7/7/2010

Kristine,

Thank you for your time today and for putting together a meeting with George and Ron.

I will put together some points of discussion. Jesse asked me to put together some questions for the public meetings, so I will copy him also. I'll try to get that to you by Monday.

We appreciate having the opportunity to be heard. Our desire is to contribute to well researched (viable) options and an informative public meeting presentation in June.

Wendy



Public Comment Form
Two Creeks Open Space
Management Plan Amendment

Public Open House
June 10, 2010

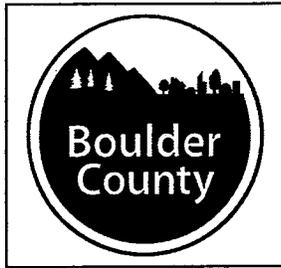
Name: Frank Phillips

Organization (if applicable): City of Lafayette

Contact Information Address/E-mail:
Frank.Phillips@cityoflafayette.com

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

INSTEAD of tearing down BARRS, convert to
FLIGHT CAGES for BOP.



Public Comment Form
Two Creeks Open Space
Management Plan Amendment

Public Open House

June 10, 2010

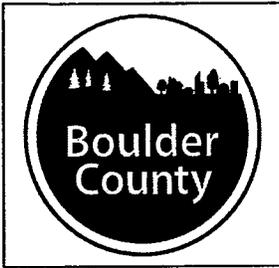
Name: Andrew Moore

Organization (if applicable): _____

Contact Information Address/E-mail:
Andrew@mooreinfo.us

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

- Great that this highly important trail extension and connection to the north is getting done.
- Crossing should be economical + safe. with this in mind crossing under the overpass makes the most sense. It is also most responsible to the taxpayers.
- The closer the trail is to the creek - like in other parts of Lafayette + Louisville the better.



Public Comment Form
Two Creeks Open Space
Management Plan Amendment

Public Open House
June 10, 2010

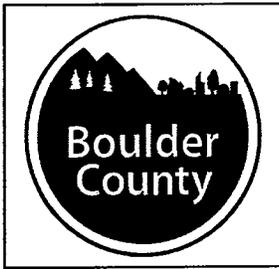
Name: MONICA KASH (OSTAB)
Organization (if applicable): TOWN OF ERIE OPEN SPACE & TRAILS
ADVISORY BOARD,
Contact Information Address/E-mail: CHAIRPERSON

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

REGARDING TRAIL PLANNING, OSTAB PREFERENCES ROUTING TO CROSS UNDER HIGHWAY 7 AT COAL CREEK, UNDER THE EXISTING BRIDGE, WITHOUT WALKING THE PROPERTY, ALIGNMENT A LOOKS BEST. ERIE PLANS TRAIL ALONG COAL CREEK IN THE VICINITY OF LAFAYETTE WATER TREATMENT FACILITY IN TOWN-OWNED OPEN SPACE TO EXISTING TRAIL AT VISTA ~~PARKWAY~~ PARKWAY TO GRIG PARKWAY (~ 2.2 MILES EXISTING TRAIL). FURTHERMORE, APPROX 1 MORE MILE OF TRAIL NORTH OF GRIG PARKWAY IS EITHER UNDER CONSTRUCTION OR HAS A GRANT APPLICATION FROM GRIG PARKWAY TO RELIANCE PARK IN GRIG.

IN THE RARE CASES OF FLOODING, THE TRAIL SHOULD BE CLOSED UNTIL THE WATER SAFELY RECEDES.

THE DESIGNS OF ~~THE~~ TRAIL NEAR COAL CREEK REFLECTS OTHER SEGMENTS OF COAL CREEK TRAIL ELSEWHERE IN GRIG, LAFAYETTE, LOUISVILLE, + SUPERIOR.



Public Comment Form

Two Creeks Open Space Management Plan Amendment

Public Open House

June 10, 2010

Name: Jamie Moore

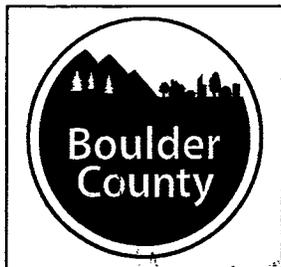
Organization (if applicable): _____

Contact Information Address/E-mail:

jamie@mooreinfo.us

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

- as a parent I would not want my children to go inside a ^{dark} box culvert. The price difference of building a passage way, ~~using~~ the box culvert idea, is much less economical too. I like the underpass proposal more. Yes, there is a major safety concern as it is close to the road but the underpass has better visibility.
- I like the idea of a trail being close to a creek - it's very pleasant to walk or
- I run in areas where the underpass ^{run} there, is sometimes closed due to flooding, and ~~these~~ signs ^{are posted. They} are clear & obvious that there is water ^{it is} on the trail (underpass area), ~~so one turns~~ ~~around~~ around



Public Comment Form

Two Creeks Open Space Management Plan Amendment

Public Open House

June 10, 2010

Name: _____

Organization (if applicable): _____

Contact Information Address/E-mail: _____

Comments (please provide comments regarding your interests, values, needs, and concerns about the future alignment and management of the Coal Creek Trail on the Mountain View Egg Farm.)

The crossing under the #17
Bridge ~~near~~ near Flag Drive
is the very best and safest
crossing. It is similar to many
crossings on the Coal Creek trail
in Boulder County (HW 42 in
Louisville and many others).
Horses and motorized vehicles
should be prohibited because
they damage the trail and make
it difficult for pedestrians to use it.

Rounds, Jesse

From: Cheryl Beck [cherylabeck@skybeam.com]
Sent: Thursday, June 24, 2010 2:18 PM
To: Rounds, Jesse
Subject: The Egg Farm Trail
Follow Up Flag: Follow up
Flag Status: Flagged

To Jesse Rounds: As residents on Flagg Drive, we as well as the neighborhood are extremely concerned about the impact of the new trail going under Hwy 7, especially since there are other beautiful and safe alternatives. We saw these alternatives when we walked the area several weeks ago. We do not think the trail coming into this rich Riparian Corridor is a well thought out plan, and would like to have more studies done both on the safety of crossing at this location and on the presence of burrowing owls there. Longtime Flagg residents have stated seeing them at times on both sides of the bridge, but Boulder County doesn't seem to hear this. This would mean that the current feasibility study is biased, and it is against what the people here want. Connectivity to Broomfield is also desired to preserve social trailing and property rights issues on both sides of the highway.

Please acknowledge that you have received and read this email.

Thank you.

Cheryl Beck
12584 Flagg Drive
Lafayette, CO 80026

6/29/2010

Rounds, Jesse

From: Elizabeth Upper [zuppere@gmail.com]
Sent: Thursday, June 24, 2010 11:11 PM
To: Rounds, Jesse
Subject: Flagg Drive & Hwy 7

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Jesse,

I am in full support of the residents in the vicinity of Flagg Drive and Highway 7 regarding the routing of the trail crossing at Coal Creek. They are the best informed, and have gone to great efforts to ensure that this project is done in the most responsible way possible. I urge you to take their input under serious consideration, avoiding a potentially dangerous and destructive situation.

Please reply and acknowledge receipt of this message.

Sincerely,

Elizabeth Upper
1347 Alpine Avenue
Boulder, CO 80304
303-444-5331
zuppere@gmail.com

Rounds, Jesse

From: D681952@aol.com
Sent: Thursday, June 24, 2010 4:46 PM
To: wendancer@gmail.com; alancheryl@juno.com; bobaloo1@viawest.net;
cherylabeck@mesanetworks.net; dueces_wild@netzero.net; plumleyonestop17@netscape.com;
jrsajban@aol.com; kmcrehan@gmail.com; ipl.lana@yahoo.com; ojobwelw@yahoo.com;
mike.jinnette@lifespringstaff.com; mserrano@westernsummit.com; nj@lifespringstaff.com;
pag@bhgrlaw.com; Wendancer@gmail.com; bobeebah@aol.com;
freespiritfarm80026@yahoo.com; fouraces@idcomm.com; salbirds@juno.com;
steve@stevegandy.com; alohabeachbum@msn.com; laymantamaral@aol.com;
gleaning@juno.com; vgcrow@mesanetworks.net; aérofirewp@yahoo.com; Rounds, Jesse
Subject: The trail
Follow Up Flag: Follow up
Flag Status: Flagged

To All,

As far as we are concerned we want the trails to go forward. It will be nice to ride out of here and hit the trails. And personally, I don't care how they build them. Sorry. We have issues with the impact and don't think it will change the rapers behavior, slightly if anything. I have friends with owls and hawks nesting in a much more urban setting and their doing just fine.

This is a battle we don't want any part of. Our government has heard enough negative crap from Flagg drive assoc.. Yes, it worked, we got the road closed and the Church has decided not to build. Those where great accomplishments. I'm not sure if the Church decided on their own or if our objection had anything to do with it, I'm assuming it did.

I would like to hear what everyone else thinks.

But the trail thing, your on your own.

John DiDomenico

Hi neighbors...I sent this to all my friends. Please forward this to as many people has you can and ask your friends to do the same.

6/29/2010

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Thursday, June 24, 2010 3:18 PM
To:
Subject: Wendy is asking for your help

Hi everyone...PLEASE SEND THIS ON TO YOUR FRIENDS WHO CARE ABOUT WILDLIFE, BIRDS AND SAFETY IN BOULDER COUNTY, LAFAYETTE, ERIE AND BROOMFIELD...public comments end this weekend. The attached photo is in front of our house...this is what we are trying to save.

I don't often ask for help on my community activism projects...but, **our neighborhood is trying desperately to safe some wildlife and riparian habitat and protect pedestrians on Hwy 7.** Please read my neighbor, Lori's email to Boulder County and my scathing email on attached document.

Send Jessie an email immediately (comments close on Monday) and ask him to put **your email supporting the Flagg Drive Neighborhood in public comments and ask for a response that he received and read your email.**

The public comments to Jessie Rounds jrounds@bouldercounty.org ends this weekend. We are a very small neighborhood and the only neighborhood in the immediate area of the habitat...your help will help tremendously! This habitat needs protection.

If you want more reading, attached is my email today to Jessie, the Manager for the BC trail project and Director Ron Stewart, POS. Also the web site for the actual project is http://www.bouldercounty.org/openspace/management_plans/eggfarmtrail/docs.htm,

We are not trying to stop the trail...we only want the crossing to be safe and a little further from the riparian corridor.!!

Wendy Raven Phillips

Subject: Re: Egg Farm
To: "JesseRounds" <jrounds@bouldercounty.org>
Date: Thursday, June 24, 2010, 2:26 PM

Dear Jesse,

Please understand that most of the people in our neighborhood are very concerned about the committee favoring to cross Highway 7 at Coal Creek. Not only is this option endangering the fragile riparian habitat for many of the creatures who live and nest there, but it is a dangerous hazard to hikers. It will only take one fatality for all the planning and the

6/29/2010

money going into this trail system to go down the tubes and you will have to find an alternate route anyway. Stop it before it starts, and put the crossing further to the east at the safer area. Hikers going under 7 at Coal Creek will have to contend with eroding banks, high water and no safe outlet on the other side, not to mention the close proximity to the highway to get from the trail westward to the crossing. It makes NO SENSE to put it there. A plan is just that, a plan, it is not set in stone and was made by humans, therefore can be amended by humans who in a different time see a different need, have better ideas and options. As your document says, it is a working document, which means it is subject to revisions. Please incorporate our concerns.

My understanding is that the grant is supposed to assist in connecting the communities of Broomfield, Erie and Boulder County. The Coal Creek crossing option does none of that, but the easternmost option would do that handily. I do not understand why the County does not seem to get this; it is like you do not hear us or respect our experience in living here and knowing what is best for our own communities. Seems like you are public servants, and should consult with and listen to the public. I work for the Federal government, and if my agency treated public comments like you folks do, our funding would be cut.

I direct you to the Lafayette 2005 Open Space & Trails Master Plan, objectives that are of particular relevance to jointly-owned Boulder County-Lafayette owned Open Space that includes this verbiage:

! Objective 5.1. Good Neighbor Approach: Manage existing open space lands and pursue additional open space land in a manner that is cognizant and respectful of the privacy and rights of neighboring landowners.

! Objective 5.3. Security: Design and manage trails and other open space facilities in a manner that protects the safety and security of open space users and adjacent landowners.

! Objective 5.4. Wildlife Conflicts: Cooperate with adjacent landowners and trail users to minimize wildlife conflicts.

(on page 85 of your document)

Please acknowledge receipt of this email upon reading. Thank you very much for your consideration and entering my comments into the record.

Lori Windle
12558 Flagg Drive, Unincorporated Boulder County
" The Nation shall be strong, so long as the hearts of the women are
not on the ground."

6/29/2010

Instruction to the Tsistsistas (Cheyenne) Nation People

Rounds, Jesse

From: Bob Luethje [bobaloo1@viawest.net]
Sent: Thursday, June 24, 2010 9:20 PM
To: Rounds, Jesse
Subject: Habitat
Follow Up Flag: Follow up
Flag Status: Flagged

Jessie

My name is Bob Luethje and I live at 12372 Flagg Drive.

Please submit my name supporting the Flagg Drive Neighborhood.

I have lived here for 16 years now, and the reason why is because to me it is like the Last Frontier with all the wildlife ect. that is around here. WE DO NEED TO SAVE OUR HABITAT OUT HERE.

Although I work many Hours and am not able to attend the meetings, I do receive e-mails concerning our neighborhood.

Thank you,

Bob Luethje

12372 Flagg Drive

Lafayette, Colorado 80026

6/29/2010

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Thursday, June 24, 2010 1:21 PM
To: Rounds, Jesse
Cc: Stewart, Ron; Gerstle, George; Boulder County Board of Commissioners; Frye, Renata
Subject: For Public Record Mtn View Egg Farm
Follow Up Flag: Follow up
Flag Status: Flagged

Hi Jesse,

Please submit this email along with the attached FDCA response to the public meeting June 10th as part of public record for the Mtn View Egg Farm Project. Also attached which I'd like to submit for public record on this project is my statement (with some corrections to typos) to Erie Board of Trustees on June 22, 2010.

We believe with conviction that you, as manager of this project, have been nonresponsive to our needs and input to the public process. You, either on your own or under the direction of Ron Stewart, put off answering questions, have evaded helping us to get information, held back public input and knowledge so people can work together, offer little and distracting help in understanding procedure, and have avoided working out solutions related to the disagreements between public and BCPOS staff and the three communities involved in this project.

The public meetings on this project have been uninformed, lacking of information for the public to understand the complexities of issues, unresponsive to public input and requests. You and Ron have held back information, restricted discussions when perspectives differ from Ron's desires for the project, you both have created separation between the connecting communities and ultimately distracting the important issues...and yet continue to call this a public process.

Since February, the Web site information and management of this project has been thoroughly incompetent, untimely, inaccurate and misleading to people trying to figure out how to show up, get accurate information and respond to the 'rigorous public process' the Commissioners requested.

The feasibility study is biased and flawed and you have indicated no desire to know why...only to continue to use it as proof that your proposed trail should switch back through fragile Riparian Corridor. The study was done by SEH which is contracted with BC on many projects and is

6/29/2010

vested in pleasing County Staff. At the last public meeting and on the web site your proposed trail is identical to the one you, Ron and Kristine Nelson from BC Dept of Transportation proposed at the first public meeting in March....it hasn't changed at all and did not address any of the public concerns to safety and protection of people, animals, birds and land.

As a member of the public, I've been minimized as a nimby...shunned as a pest...been the object of angry staff people who don't want to deal with our neighborhood, who think I'm "not nice"...it goes on and on. It is insulting to try to discuss property rights with people who have never owned a home! It is so obvious that you just want us to continue running around trying to figure out how to make our points in a respectful, timely, organized way. Being sent from one staffer to another to a different town meeting, to an advisory meeting to a council meeting to another staffer to another meeting...OH AND BY THE WAY...WE JUST CHANGED TO RULES AGAIN...until we few who can keep going are worn out. Meanwhile you pass these management plans as if everyone supports them. I do not have a staff of hundreds, or a public budget to distract your pet projects. This system is not working for the public. In the end the Commissioners and Council People and Trustees all say the same thing..."We depend on our staff to inform us..."

Never mind the public who won't speak up because they think BC will punish them further with land use, floodway and property rights in subdivision requests. Never mind the people who have strong opinions but don't have the ability to speak articulately. Never mind the public who is too old or uneducated or nontechnical to keep up with the systems way of informing people. Never mind the people who are desperately trying to pay their bills or find a job...or work a job and care for their family to commit to running around burning gas and resources trying to keep up with your game plan.

HOW IS IT POSSIBLE THAT THE PEOPLE VOTE FOR WILDLIFE HABITAT PRESERVATION AND PROTECTING OPEN SPACE AND A PROJECT LIKE SUSTAINABLE AGRICULTURE WHICH IS YET UNDEFINED AND UNAPPROVED CAN PREEMPT THE DECISIONS ON A TRAIL REGARDING HUMAN AND ANIMAL SAFETY AND CONNECTING ALL COMMUNITIES...FOR EXAMPLE THE NEIGHBORHOODS RON STEWART AND SOME LAFAYETTE COUNCIL PEOPLE ARE POLITICALLY MAD AT... LIKE BROOMFIELD.

Outrageous, Jesse. Is this the way Boulder County treats the people and communities that pay their salaries? Has Boulder County Department of Open Space become so big and self contained that is can treat its citizens like this in its "rigorous public processes"?

We have recently heard the same type of issues on the North side of Hwy 7 in the Town of Erie. Their citizens are having some similar feelings and asked me to speak to their Trustees, which I did last week. We all have also approached GOCO management at state level to be heard and

anticipate more conversation with you in the future with many more parties...stakeholders involved.

Because of your management of this project, the lack of integrity of the process and your choices to impact habitat and safety negatively, a number of us are beginning the process of organizing a web site for Boulder County residents who want to begin to challenge Parks and Open Space, its methods and funding.

I am obviously angry. I apologize for my current lack of formality and professionalism. For ten years I've asked that this corridor area be studied as an important habitat area and have been put off by and bounced around by all the agencies involved in this project and their staff. I had been told by Lafayette to talk to Boulder County...Boulder County points at Lafayette. I go back to Lafayette and the City Engineer and the City Administrator tells me that they don't need to talk to Flagg Drive folk because when the time comes they can just take our land. So I went back to Boulder County and they say there is no budget, not to worry there is plenty of time. Now it is 2010 and I have been donating my time to our neighborhoods issues since you hurriedly put this on everyone's agenda...I have no patience left. AND you have been incredibly unresponsive to our needs as shown at the last public meeting.

Wendy Phillips, Flagg Drive Community Association

Rounds, Jesse

From: ojibwelw [ojibwelw@yahoo.com]
Sent: Thursday, June 24, 2010 2:26 PM
To: Rounds, Jesse
Subject: Re: Egg Farm
Follow Up Flag: Follow up
Flag Status: Flagged

Dear Jesse,

Please understand that most of the people in our neighborhood are very concerned about the committee favoring to cross Highway 7 at Coal Creek. Not only is this option endangering the fragile riparian habitat for many of the creatures who live and nest there, but it is a dangerous hazard to hikers. It will only take one fatality for all the planning and the money going into this trail system to go down the tubes and you will have to find an alternate route anyway. Stop it before it starts, and put the crossing further to the east at the safer area. Hikers going under 7 at Coal Creek will have to contend with eroding banks, high water and no safe outlet on the other side, not to mention the close proximity to the highway to get from the trail westward to the crossing. It makes NO SENSE to put it there. A plan is just that, a plan, it is not set in stone and was made by humans, therefore can be amended by humans who in a different time see a different need, have better ideas and options. As your document says, it is a working document, which means it is subject to revisions. Please incorporate our concerns.

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! Objective 5.4. Wildlife Conflicts: Cooperate with adjacent landowners and trail users to minimize wildlife conflicts. (on page 85 of your document)

Please acknowledge receipt of this email upon reading. Thank you very much for your consideration and entering my comments into the record.

Lori Windle
 12558 Flagg Drive, Unincorporated Boulder County
 " The Nation shall be strong, so long as the hearts of the women are

6/29/2010

not on the ground."

Instruction to the Tsistsistas (Cheyenne) Nation People

--- On **Thu, 6/17/10, Rounds, Jesse** <jrounds@bouldercounty.org> wrote:

From: Rounds, Jesse <jrounds@bouldercounty.org>

Subject:

To: "Wendy and David Phillips" <wendancer@gmail.com>, christi.walsh@gmail.com, rdsnow@comcast.net, steve@stevegandy.com, rschillawski@earthlink.net, Bmused7@comcast.net, kmcrehan@gmail.com, freespiritfarm80026@yahoo.com, rgcrow@mesanetworks.net, nj@lifespringstaff.com, cherlabeck@mesanetworks.net, ojibwelw@yahoo.com, frost@comcast.net, rfmetz@ieee.org, dphillips@windowinthesky.net, salbride@skybeam.com, lindsey_rosso@yahoo.com, alfredandvictoria@yahoo.com, sunnieglaister@q.com, kmccourt@comcast.com, jdbesse@aol.com, msajban@geosociety.org, p.loris@LorisandAssociates.com, jrsajban@aol.com, pdchavez3@msn.com, djb@alumni.rice.edu, tomer26@comcast.net, portia0217@yahoo.com, john.dawson@sopheon.com, mike.jinnette@gmail.com

Date: Thursday, June 17, 2010, 4:14 PM

Stakeholders,

The Two Creeks Management Plan Mountain View Egg Farm Amendment is working it's way toward more opportunities for public involvement.

The draft plan is currently on the website:

http://www.bouldercounty.org/openspace/management_plans/eggfarmtrail/docs.htm, along with the draft feasibility study for the crossing of State Highway 7. I ask that public comments on the draft be submitted by the 28th of June so that I can prepare the document for review by both Lafayette and Boulder County.

Speaking of that review, we are taking the plan amendment before both the Lafayette and Boulder County Open Space Advisory Committees as well as before the Lafayette City Council and the Boulder County Commissioners. We will present the plans on the following dates:

Lafayette Open Space Advisory Committee: July 8th, 5:30 p.m. at the Lafayette City Council Chambers

Lafayette City Council: July 20th, 6:30 p.m. at the Lafayette City Council Chambers

Boulder County Parks and Open Space Advisory Committee: July 22nd, 6:30 p.m. at the Boulder County Courthouse

Boulder County Commissioners Public Hearing: July 26th, 4 p.m. at the Boulder County Courthouse

The Lafayette City Council Chambers are located at: 1290 South Public Road in Lafayette. The Boulder County Courthouse is located at 1325 Pearl Street on the Pearl Street Mall.

All of these meetings are open to the public and I encourage you to attend. If you have any further questions, please do not hesitate to contact me.

Sincerely,

Jesse Rounds
Jesse Rounds
Resource Planner
Boulder County Parks and Open Space
(o) 303.678.6271

Rounds, Jesse

From: Debra Whitehead [debrawhitehead1@gmail.com]
Sent: Friday, June 25, 2010 8:55 AM
To: Rounds, Jesse
Subject: Flagg Dr Community & nearby trail

Dear Mr. Rounds,

Please send an acknowledgement that you have read my comments and please enter them into the public record.

I am deeply concerned about the information I've just received that details Orwellian-type tactics here in Boulder County: A smile and expression of concern for wildlife and citizen safety and a smile and expressed desire for open communication on the face of our county yet the realities of this situation appear to bode otherwise. I am speaking about the concern about the committee favoring to cross Highway 7 at Coal Creek. Not only is this option endangering the fragile riparian habitat for many of the creatures who live and nest there, but it is a dangerous hazard to hikers.

Really? Are these behaviors actually happening here?

- 1- You are not responding to expressed concerns about the location of a trail even as comments are being invited.
- 2- You are disregarding concern for the safety and well being of natural habitat around the trail site and people using the proposed trail.
- 3- You are disregarding that lack of safety to pedestrians can, at minimum, lead to a tremendous waste of taxpayer dollars?

As I read the information sent to me, I just kept thinking, WHY? Why is this communication with you so difficult? What interest is met by not listening to the people in the Flagg Dr neighborhood who are telling you that there is another safer and better way to build this trail? What interest is not met if you made the proposed modification?

The people who informed me of this situation are not alarmists and certainly have other issues in their lives to attend to. I really can't understand why there is a lack of quality interaction. As a voting member of our Boulder County Community I will be watching this more closely now and hope that you and our elected officials will listen to all stakeholders and make better decisions about communicating with others and about locating the trail as this process moves forward.

Thank you for your time. Again, I would appreciate a response that you have at least read this email and that you have entered my comments into the public record about this issue.

Sincerely,
Debra Whitehead
Boulder 80304

6/29/2010

Rounds, Jesse

From: Ruth Hartman [ruthhartman1@yahoo.com]
Sent: Saturday, June 26, 2010 2:37 PM
To: Rounds, Jesse
Subject: Flagg Drive
Follow Up Flag: Follow up
Flag Status: Completed

I am amazed that you are not listening to the input of the neighborhood about changing the location of the crossing. This is an important wildlife area. Government at it's worst. Please put this in the public record.

Thank you, Ruth Hartman, Boulder CO

Rounds, Jesse

From: Lynnjoywalk@aol.com
Sent: Sunday, June 27, 2010 11:15 PM
To: Rounds, Jesse
Subject: Regarding the trail at Flagg Drive
Follow Up Flag: Follow up
Flag Status: Completed

Dear Jessie,

I have been informed of your responsibility to listen and respond to neighborhood concerns regarding the proposed trail crossing near Flagg Drive in Lafayette. I believe that you have the capacity to respond favorably to the request that this trail not interfere with the riparian corridor.

I also believe that you have the capacity in general to be sensitive and listen and **RESPOND** to local residents about their clear preferences and concerns. Please remember who foots the bill for open space. The public needs to be responded to.

Thank you,
Lynn Israel

Lynn Israel
Boulder, CO 80303
(303) 819-0621

Rounds, Jesse

From: Richard Schillawski [rschillawski@earthlink.net]
Sent: Monday, June 28, 2010 12:39 PM
To: Rounds, Jesse
Subject: Re: Two Creeks Draft Management Plan

Please see the attached comments, submitted today per your request. I can be reached by email if you have any questions.

Dick Schillawski

----- Original Message -----

From: Rounds, Jesse
To: Wendy and David Phillips ; christi.walsh@gmail.com ; rdsnow@comcast.net ; steve@stevegandy.com ; rschillawski@earthlink.net ; Bmused7@comcast.net ; kmcrehan@gmail.com ; freespiritfarm80026@yahoo.com ; rgcrow@mesanetworks.net ; nj@lifespringstaff.com ; cherlabeck@mesanetworks.net ; ojibwelw@yahoo.com ; frost@comcast.net ; rfmetz@ieee.org ; dphillips@windowinthesky.net ; salbride@skybeam.com ; lindsey_rosso@yahoo.com ; alfredandvictoria@yahoo.com ; sunnieglaister@q.com ; kmccourt@comcast.com ; jdbesse@aol.com ; msajban@geosociety.org ; p.loris@LorisandAssociates.com ; jrsajban@aol.com ; pdchavez3@msn.com ; djb@alumni.rice.edu ; tomer26@comcast.net ; portia0217@yahoo.com ; john.dawson@sopheon.com ; mike.jinnette@gmail.com
Sent: Thursday, June 17, 2010 4:14 PM

Stakeholders,

The Two Creeks Management Plan Mountain View Egg Farm Amendment is working it's way toward more opportunities for public involvement.

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All of these meetings are open to the public and I encourage you to attend. If you have any further questions, please do not hesitate to contact me.

Sincerely,

Jesse Rounds

Jesse Rounds
Resource Planner
Boulder County Parks and Open Space
(o) 303.678.6271

1160 N. 119th Street
Lafayette, Colorado 80026

June 28, 2010

Jesse Rounds, Resource Planner
Boulder County Parks and Open Space Dept.
(via email)

RE: (Draft) "Two Creeks" Management Plan Update

I have the following comments and suggestions regarding the draft language:

- (p. 6, Summary, last paragraph) – Both the City of Lafayette and the County of Boulder are having problems with relations with rural residents near Lafayette. Constant reference to a “good neighbor policy” is meaningless, if not counterproductive, without consistent manifestations of such a policy by these governmental entities.
- (p. 7, Introduction, first paragraph) - “agricultural lifestyle” isn't a very meaningful term. In the particular case of these properties, most are being converted to habitat preservation from marginal agriculture, so use of the term here seems inappropriate.
- (p. 12, Hydrology, third paragraph) – Mention is made of two permitted water wells on the Stephenson-Nelson property, but this seems to be the only reference made to these wells in the Plan. Are these wells being put to some useful purpose?
- (p. 16, Goals) -
 - Vegetation – add “control weeds”.
 - Agricultural – change to “Preserve and enhance agricultural resources and practices as possible and appropriate.”
- (p. 35, “Significant Agricultural Lands and ...”) – Stephenson-Nelson wells?
- (p. 40, Wetlands, last paragraph) - “possible” wetland on the Egg Farm's northeast corner also appears to be impacted at this point by a flow restriction (dam) of some sort in its drainage channel; this dam appears to be located downstream on Lafayette's sewage plant property.
- (p. 44, Wildlife, various paragraphs):
 - common mammals are dominated by the smaller species, few deer are present.
 - include the extensive data on wildlife, particularly birds, on neighboring properties, not just data on Open Space properties, collected by staff. Data have been collected by a number of environmental and wildlife organizations as well as local residents.
- (p. 48, Water Resources, second paragraph) – Comment regarding “use to extinction” is problematical. Originally the streams were probably much more seasonal (tended to dry up in mid-summer). With the practice of storage of water for later agricultural irrigation following settlement of the area, plus the more recent trends of lawn watering throughout the summers and more-or-less continuous discharges from sewage plants, the streams are probably more likely to maintain a minimum flow year-around than was the case naturally.
- (p. 49, Cultural Resources, first paragraph) – Appears to be incomplete – the four sites aren't listed although it appears that was the intent.
- (p. 49, Cultural Resources, third paragraph) – Although it's quite likely it existed, I haven't seen any definite documentation of a connection between Jim Baker and the Baker Mine (which was active after he left the area). Other mining history of interest includes the Baker Land and Coal Co. (later Irvington) which owned most of the “Two Creeks” properties for many years, the railroads through the properties associated with the mining, the town of Irvington which

predated Lafayette, Edward B. Light and the "Erie Wars" of the 1870s (the first of the many owner-labor conflicts in Colorado's coal industry). Coal mining on these properties was among the earliest in the entire state of Colorado – not just in Boulder County.

- (p. 62, Market Farm Operations) – The soils on the Egg Farm property are marginal for a market farm operation and highly subject to erosion given its exposure to the prevailing winds; permanent cover should be considered as an alternative to a market farm. The proposed access from Highway 7 is also marginal and potentially dangerous, which would also suggest another location for a market farm operation. Some mechanism to effectively transfer the water well resources from this property (plus Stephenson-Nelson, if available) to another, more suitable location for market farm operations should be pursued.
- (p. 63, Feasibility Study and related paragraphs, Figure 13) – **Unacceptable.** The summary presented in the text and the route indicated in Figure 13 do not match the material in the Feasibility Study performed by SEH (dated June 10, 2010), and do not meet the design criteria used in the study, in particular the requirement of a 55-foot minimum radius for turns in the trail. This discrepancy calls into serious question the competence and/or motivations of those preparing this draft Plan. The Feasibility Study also has issues which need to be addressed, assuming staff wishes to incorporate it properly into this draft Plan. In addition to deficiencies in the engineering, these include inadequate attention to safety issues and user experience issues, especially the failure to consider at all the odor and other problems associated with Lafayette's sewage plant. Adoption of any trail route based on the recommendation as presented here would be highly improper.
- (p. 66, Flagg Park) – Additions to this park, for horse trailers in particular, are vigorously opposed by the neighbors and present very serious traffic concerns; jamming these unsuitable additions into Flagg Park would be another contradiction to the supposed "good neighbor" policy, as mentioned above. In addition, restrooms may be contrary to the prohibition on "indoor facilities" mentioned on p. 21.

Sincerely,

Richard D. Schillawski

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, June 28, 2010 6:18 PM
To: Rounds, Jesse
Cc: Gerstle, George; Nelson, Kristine; 'Aimee Wesley'
Subject: mountain view egg farm

For the Public Record:

Dear LOSAC and POSAC, Erie and BC Commissioners,

The Feasibility Study done by the BC Department of Transportation is flawed, biased and based on incomplete data. This is not a study that should be used to make a decision about which trail is best suited to cross under Hwy 7. We ask that a new study take place by an independent organization and include wildlife and environmental data and opinions from the Nature Conservancy, Colorado Department of Wildlife, Birds of Prey Foundation and an engineering firm that is not employed by Boulder County.

This study did not take into consideration that the north side of the Hwy 7 crossing and the connections to Lafayette properties and the Town of Erie will have a major impact on feasibility issues. It also did not consult with the Town of Erie related to many property rights issues along their creek. This is troublesome since the IGA indicates Erie is responsible for design and implementation of the trail from Vista Parkway through and under Hwy 7 to the south side of the highway.

This Study did not take into consideration that the property owners on the north side of the cattle crossing/box culvert are willing to allow an easement from the culvert to the Erie trail to keep people safely off the highway.

There was no hydrology or erosion studies done...these are incredibly important considerations in this decision.

No consideration was made to keeping people off the highway. No consideration was made to keeping Broomfield and East Erie folks off the highway and airport when trying to reach the trail. No data was gathered about the accident and repair statistics of this highway corridor.

6/29/2010

The BC Parks and Open Space staff including the people commenting in the Feasibility Study have indicated at the public meetings that they have no wildlife data on the area. How can they make accurate commentary.

We are still preparing a more detailed list of why this Feasibility Study should not be a tool for this decision process. We ask for time at all the July Public Hearings to discuss this with greater consideration to all the available data on the area.

Wendy Phillips, Flagg Drive Community Association

TRAIL COMPLETION AND GOCO GRANT:

These are some of the issues that are yet unresolved related to the connecting trail from Flagg Park to Vista Parkway as proposed by the Town of Erie, the City of Lafayette and Boulder County Parks and Open Space.

Since you are using public money and lands to complete this trail system, we ask that you become aware of the concerns of your citizens and the citizens funding this project. At this point your leadership and staff are making decisions that are **NOT YET** supported by the people who live, work and who are directly impacted by the areas that are proposed by the current trail plan.

SOME POINTS OF CONCERN:

- 1) THE PUBLIC PROCESS HAS BEEN OUT OF INTEGRITY ON THIS TRAIL CONNECTION FOR MANY YEARS. LEADERS IN ERIE, LAFAYETTE AND RON STEWART, DIRECTOR OF BCPOS HAVE FOLLOWED AND ARE FOLLOWING THEIR OWN PET DREAMS AND PRIORITIES FOR THIS PROJECT...AT THE EXCLUSION OF THE NEEDS, PRIORITIES AND DESIRES OF THE PUBLIC.**
- 2) LEADING PEOPLE TO THE HWY IS DANGEROUS...THERE HAS ALREADY BEEN LOSS OF LIFE AT THE COAL CREEK BRIDGE WHEN A LITTLE GIRL RODE HER BIKE OUT ONTO THE HIGHWAY AND WAS HIT BY A TRUCK. This corridor is very dangerous with 30-50,000 cars a day crossing that bridge.**
- 3) USING RIPARIAN CORRIDOR, CREEK CORRIDOR AND WILDLIFE HABITAT AREAS FOR TRAILS NEGATIVELY IMPACTS THE BIRDS AND ANIMALS. THERE ARE PLENTY OF CREEK PATHWAYS AVAILABLE...THE ANIMALS AND BIRDS ARE RUNNING OUT OF SPACE. THESE PRECIOUS SHADED AREAS MUST REMAIN PROTECTED.**
- 4) SERIOUS PROPERTY RIGHTS ARE AT ISSUE WITH THE PROPOSED TRAIL BOTH IN ERIE AND UNINCORPORATED BOULDER COUNTY. GOCO MONEY SHOULD NOT BE USED WHEN PROPERTY RIGHTS ARE MANIPULATED FOR THE TRAIL PROJECT.**
- 5) NONE OF THE AGENCIES INVOLVED IN THESE DECISIONS HAVE DOCUMENTED THE BIRDS ALONG THE CREEK AREAS IN QUESTION. LOCAL KNOWLEDGE SAYS THE BURROWING OWLS HAVE BEEN ALL ALONG THE CREEK AREA FOR MANY DECADES.**
- 6) THE PUBLIC PROCESS HAS BEEN DISREGARDING PUBLIC INPUT. THE SEH FEASIBILITY STUDY IS FLAWED AND BIASED TO THE PROPOSED PLAN WITH NO CONSIDERATION TAKEN TO PUBLIC INPUT LIKE SAFETY AND WILDLIFE PROTECTION. SEH IS EMPLOYED BY THE COUNTY ON MANY PROJECTS, IT SHOULD NOT BE THE AGENCY GIVING A FEASIBILITY STUDY FOR THIS PROJECT.**
- 7) LAFAYETTE HAS CREATED A SMELLY, MOSQUITO INFESTED MESS AT THE WASTE WATER TREATMENT PLANT. IT TOTALLY WIPED OUT THE BURROWING OWL POPULATION AT ITS LOCATION WHEN IT TURNED ITS CREEK BED INTO A SWAMP. THE RESIDENTS LIVING AROUND THE WWPLANT HAVE BEEN IN CONSTANT CONTACT WITH MICK FORRESTER ON AND OFF OVER**

THE YEARS TRACKING THE AIR QUALITY, OR LACK THERE OF, IN THE AREA. PROPERTY DEALS HAVE FALLEN THROUGH IN THE AREA, RESIDENTIALLY AND COMMERCIALY BECAUSE OF THE STENCH ISSUES.

- 8) **EROSION ISSUES ALONG THE STEEP BANKS OF THE CREEK ARE AT ISSUE BOTH FOR PROPERTY OWNERS AND SAFETY AND MAINTENANCE OF A TRAIL SYSTEM.**

Although everyone participating in this public process appears to agree that the trail should be completed and the spending of public funds to complete this section of trail is supported locally.....

SINCE THERE ARE OPTIONS AS TO WHERE THE TRAIL CAN GO, we will not continue to support this proposed trail location at the risk of **loss of** human life, wildlife habitat, riparian mating and training grounds, and individual property rights. As "pretty" as some Erie and Lafayette leaders like to call the creek corridor area..."pretty" is no excuse for sacrificing property rights, people's safety, wildlife habitat and our grand Riparian bird habitat (not to mention the state protected burrowing owls).

Luckily for all the hard work everyone has put in to this project, there is a win win for all on this great trail connection. Follow the will of the public who has been showing up and giving input on the location of the trail. DO NOT take away property rights to accomplish the trail system. Completion of the trail does and can serve everyone. Sustainable Ag, pretty views, safety along the highway, and protection of our last remaining wildlife corridors can all be accomplished on this project...BUT NOT as you are currently proposing.

LASTLY...BROOMFIELD SHOULD CONNECT TO THIS TRAIL AND THEIR CITIZENS SHOULD NOT BE FORCED ONTO THE HIGHWAY TO REACH ANOTHER TRAIL. GET OVER YOUR POSTURING...TAKE THE LONG TERM VIEW AND CONNECT THE PEOPLE...THAT IS WHAT GOCO IS ABOUT....AND KEEP THE PEOPLE AND ANIMALS SAFE.

Wendy Phillips, Flagg Drive Community Association
June 22, 2010, some typos and corrections added June 24, 2010

Initial Response to Public Meeting on trail connection from Flagg Park to Erie June 10, 2010

Last night I attended the Public Meeting displaying the proposed trail alignment from Flagg Park through the Mountain View Egg Farm under Hwy 7 to the Erie boundary.

Even after all our neighborhoods clarity in writing, at meetings, before the Commissioners and before your Committee, there was still no consideration taken at the public presentation to educate the public on dealing with Riparian Corridor impact, Hwy safety issues, Wildlife habitat protection and connectivity to Broomfield. The proposed trail alignment displayed last night hasn't changed since the original public meeting in March. Additionally there was absolutely no discussion to trail specifics and impact on the Riparian Corridor.

With the SEH Engineer, I reviewed and discussed what Director Stewart calls a feasibility study looking at the options for underpasses for the trail system. The Engineer had no knowledge of important components contributing to protecting the area on impacts and it was very clear to me (both from my discussion with the Engineer and the biased language in the report) that his estimates of cost were guesses based not on researched facts about the area, but by biases presented to him by Parks and Open Space. Is it really possible that Parks and Open Space wasted our limited resources on a report with no basis in reality just to appease a concerned neighborhood? (I will forward you my letter to Director George Gerstle outlining my reasoning for this statement once I get it completed.)

Loosing forever a protected section of Riparian Corridor is unacceptable especially when there are viable choices. If POS thinks even a portion of Riparian Corridor is expendable for their other priorities...perhaps they need to stop claiming and marketing to the public their need to purchase more open space to protect the birds (Channel 8). It is doubtful that people will continue to finance Parks and Open Space in the future for Sustainable Agriculture...The birds, wildlife and protected open space is what the public votes for...not Boulder County competing with our local farmers. If you sacrifice this important mating and nesting area...there will be organized public relations impact against passing more open space requests beginning now. Loosing this Riparian area will never go away as an issue to this neighborhood.

Safety is another neighborhood concern that in several public meetings our leaders have stated that people need to be responsible for their own safety. Within 75 ft of this same location in the 1990's a little girl riding her bicycle in front of her dad didn't stop and went into the traffic on Hwy 7 and was killed when a semi truck couldn't stop. The accident statistics on this section of highway are astronomical. There have been many major accidents, constant minor accidents and several deaths at this bridge location. Loosing another child's life on Hwy 7 because Boulder County and the City of Lafayette leads people by trail onto the Hwy Right-of-Way and within 15-20 feet of the highway pavement and then claiming not responsibility is ... unspeakable.

Lastly, the plan last night shows POS building a public restroom right next door to a residence...this shows the lack of respect for the area residents. Our neighborhood will review this Flagg Park Plan as a group and respond.

It is no secret that Director Stewart has other priorities for the Mountain View Egg Farm (i.e. Sustainable Agriculture) and that he and other Lafayette leaders have 'issues' with Broomfield that keep them all from wanting to make protecting the Riparian Corridor, wildlife and people on a dangerous highway their priority. I hope that your committee and the Boulder County Commissioners will take our concerns more seriously.

Although this letter is my personal response to the meeting last night, the Flagg Drive Community Association which includes all residences along Flagg Drive have unanimously voted that the issues of Riparian Corridor and Wildlife protection, Safety along Hwy 7 and Flagg Park design and safety are of their greatest concern. Wendy Phillips 12626 Flagg Drive, Lafayette, CO

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Wednesday, June 30, 2010 3:15 PM
To: Rounds, Jesse
Subject: FW: Burrowing owls and other wildlife in the Coal Creek waterway west of the Erie airport

For public record regarding the north side of the Hwy.

From: Scott deLuise [mailto:SdeLuise@matrixbusinessconsulting.com]
Sent: Wednesday, June 30, 2010 2:48 PM
To: kristin.cannon@state.co.us
Cc: Robb James; Wendy and David Phillips
Subject: Burrowing owls and other wildlife in the Coal Creek waterway west of the Erie airport

Kristin:

I just got off the phone with Stan, and he reminded me that, not only have there been burrowing owls, but blue heron in the deeper water at the north end of the airport, as well as coyotes, many other species of birds of prey, all located in the riparian waterway. Stan Wawrzyniak's number is: (303) 809-1971.

All the parks people need to do to fix many of the problems we're having with the trail is move it out of the creek right of way, and up to higher ground where it will yield better views and wildlife management!

R. Scott deLuise, CCIM, SPPA

340 E. 1st. Ave. Suite 300

Broomfield, CO 80020

303 298-1711 Office

303 298-9108 Fax

303 710-5005 Cell

7/7/2010

888 321-5200 Toll Free

scott@eaedc.com

www.eaedc.com



Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, July 05, 2010 9:13 AM
To: Rounds, Jesse
Subject: FW: wildlife information for Egg Farm

Follow Up Flag: Follow up
Flag Status: Flagged

Jesse, for the public record.
Wendy

-----Original Message-----

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Monday, July 05, 2010 8:55 AM
To: 'Hoerath, Dave'
Subject: RE: wildlife information for Egg Farm

Hi Dave,
After reviewing the feasibility study, you made came to some interesting conclusions about the wildlife at the box culvert vs the Coal Creek bridge and Riparian Corridor. I'm interested in hearing on what you base those conclusions. Could you review your findings and educate us on the facts that lead you to your and the staff that contributed to the conclusions opinions?
Wendy Phillips

-----Original Message-----

From: Hoerath, Dave [mailto:dhoerath@bouldercounty.org]
Sent: Monday, May 17, 2010 6:06 PM
To: wendancer@gmail.com
Subject: wildlife information for Egg Farm

Wendy - I wanted to get back to you quickly after the tour. As I said, I am new to this process since our initial (long-term) biologist took a job in New Mexico. Everyone is new to the Egg Farm since we only purchased it several months ago.

I want to be straight with you up front. As the wildlife biologist I am not a decision-maker, but an information provider. As Jesse said, we assemble the information and come up with recommendations to bring forward to both the Advisory Committee (who may accept/reject/massage our recommendations) and then to the ultimate decisionmakers, the County Commissioners. If you don't want the trail or want us to consider other options that we are not currently considering, I am not the person to be dealing with. My sideboards are that there will be a trail through the Egg Farm and that it will cross Hwy 7 at either the east livestock tunnel or under the Coal Creek bridge. It is my job to use the information I have to help the planning team craft the best alternative, alignment, and mitigation so that the resulting trail has the least impact on wildlife and their habitat. Then I apply the same to what actually gets decided.

If you have wildlife information or sources relative to the Egg Farm or Haselwood (Flagg Park/Coal Creek channel), I want that. You (and another gentleman) mentioned having information about burrowing owls in your area (Egg Farm, Flagg Park, private land, areas along the creek, and even areas north of Hwy 7). You also thought that the Birds of Prey Foundation might have information or survey records for the area. Surveys from/by them is a new source as they are in the rehabilitation business, but I'll follow it up. I will take any information for this process, as our information on the Egg Farm is limited due to its recent entry into our system. Maps, surveys, dates/seasons/years, etc. would all be greatly appreciated.

I also want to capture your concerns about wildlife/habitat, so that they are effectively brought into the discussion of alternatives, alignments, and mitigation. How do your concerns combine to arrive at a desired outcome for the trail alignment? We have done scoping and held the tour, but we don't have the market cornered on ideas or reasoning.

thanks
dave3

Dave Hoerath
Wildlife Specialist
Boulder County Parks + Open Space
dhoerath@bouldercounty.org
303.678.6204

Rounds, Jesse

From: Wendy and David Phillips [wendancer@gmail.com]
Sent: Monday, July 05, 2010 9:12 AM
To: Rounds, Jesse
Subject: FW: Safety on Hwy 7
Follow Up Flag: Follow up
Flag Status: Flagged

Jesse, for the public record.

Wendy

From: Wendy and David Phillips [mailto:wendancer@gmail.com]
Sent: Monday, July 05, 2010 9:11 AM
To: 'Hersey, Steven'
Cc: 'ggerstle@bouldercounty.org'
Subject: Safety on Hwy 7

Hi Steven,

I am wondering if you can give me the Highway traffic study numbers on traffic flow and accidents on Hwy 7 between 119th in Lafayette and Bonanza by Erie/Anthem? Also, how many times a year you have to fix the metal guard rails that cars and trucks keep bouncing into....(by the way, there is one bashed in again right now).

Our neighborhood is very concerned about the safety issues that will arise from Boulder County putting a trail onto the Hwy easement at the Vigil driveway property close to the Coal Creek bridge. Thousands of users, including dogs and children on bicycles should not be lead to the highway, especially when there is an alternative at the box culvert under the highway just east of the creek. We have discussed an easement for the trail on the north side of the hwy with the property owners and they have agreed to support that crossing.

What can we do with CDOT to further ask for your support in making sure all safety concerns are addressed? In 1997 a 10 year old girl riding her bicycle in front of her dad rode out onto Hwy 7 at Flagg Drive and was killed by a truck...we do not want to set this up to happen again.

We are concerned that building out a trail under the bridge will force deer onto the highway increasing

7/7/2010

safety concerns for drivers, that people and activity at this part of the highway will be incredibly distracting to the drivers, and that bringing people onto the highway at a merge lane is incredibly negligent. We are also concerned about causing major erosion issues to private property by decreasing the flow width under the bridge.

Wendy Phillips, Flagg Drive Community Association